

MEETING

PLANNING COMMITTEE

DATE AND TIME

THURSDAY 10TH SEPTEMBER, 2015

AT 6.30 PM

VENUE

HENDON TOWN HALL, THE BURROUGHS, LONDON NW4 4BQ

TO: MEMBERS OF PLANNING COMMITTEE (Quorum 3)

Chairman: Melvin Cohen

Vice Chairman: Wendy Prentice

Councillors

Maureen Braun
Barry Rawlings
Stephen Sowerby

Eva Greenspan
Claire Farrier
Jim Tierney

Tim Roberts
Agnes Slocombe
Mark Shooter

Substitute Members

Richard Cornelius
Sury Khatri

Anne Hutton
Gabriel Rozenberg

Dr Devra Kay
Laurie Williams

You are requested to attend the above meeting for which an agenda is attached.

Andrew Charlwood – Head of Governance

Governance Services contact:

Governance.ServicesPaul Frost paul.frost@barnet.gov.uk 020 8359 22052205

Media Relations contact: Sue Cocker 020 8359 7039

ASSURANCE GROUP

ORDER OF BUSINESS

Item No	Title of Report	Pages
1.	Minutes of the last meeting	1 - 8
2.	Absence of Members	
3.	Declarations of Members Disclosable Pecuniary Interests and Non-pecuniary Interests	
4.	Report of the Monitoring Officer (if any)	
5.	Public Questions and Comments (if any)	
6.	Members' Items (if any)	
7.	Brent Cross A5 Corridor Study	9 - 108
8.	Brent Cross Cricklewood Regeneration Infrastructure RMA	109 - 264
9.	Brent Cross Regeneration Central Brent Riverside Park RMA	265 - 340
10.	Brent Cross Regeneration Application Under Condition 2.4 and 2.5	341 - 354
11.	Dukes Avenue	355 - 364
12.	Phase 6a, Millbrook Park	365 - 400
13.	Monkfrith School	401 - 418
14.	Exempt - Dukes Avenue	419 - 422
15.	Any Item(s) that the Chairman decides are urgent	

FACILITIES FOR PEOPLE WITH DISABILITIES

Hendon Town Hall has access for wheelchair users including lifts and toilets. If you wish to let us know in advance that you will be attending the meeting, please telephone Paul Frost paul.frost@barnet.gov.uk 020 8359 2205. People with hearing difficulties who have a text phone, may telephone our minicom number on 020 8203 8942. All of our Committee Rooms also have induction loops.

FIRE/EMERGENCY EVACUATION PROCEDURE

If the fire alarm sounds continuously, or if you are instructed to do so, you must leave the building by the nearest available exit. You will be directed to the nearest exit by uniformed custodians. It is vital you follow their instructions.

You should proceed calmly; do not run and do not use the lifts.

Do not stop to collect personal belongings

Once you are outside, please do not wait immediately next to the building, but move some distance away and await further instructions.

Do not re-enter the building until told to do so.

This page is intentionally left blank

Decisions of the Planning Committee

29 July 2015

Members Present:-

AGENDA ITEM 1

Councillor Melvin Cohen (Chairman)
Councillor Wendy Prentice (Vice-Chairman)

Councillor Maureen Braun	Councillor Agnes Slocombe
Councillor Barry Rawlings	Councillor Stephen Sowerby
Councillor Tim Roberts	Councillor Jim Tierney
Councillor Gabriel Rozenberg	Councillor Sury Khatri

Apologies for Absence

Councillor Claire Farrier (Councillor Laurie Williams substitute)	Councillor Mark Shooter (Councillor Suri Khatri substitute)
--	--

Councillor Eva Greenspan (Councillor
Gabriel Rozenberg substitute)

1. MINUTES OF THE LAST MEETING

The minutes of the meeting held on 25 June 2015 were agreed as a correct record.

2. ABSENCE OF MEMBERS

Apologies were received from Councillor Claire Farrier, Councillor Eva Greenspan and Councillor Mark Shooter, with Councillor Laurie Williams, Councillor Gabriel Rozenberg and Councillor Sury Khatri acting as substitutes respectively.

3. DECLARATIONS OF MEMBERS DISCLOSABLE PECUNIARY INTERESTS AND NON-PECUNIARY INTERESTS

None.

4. REPORT OF THE MONITORING OFFICER (IF ANY)

None.

5. PUBLIC QUESTIONS AND COMMENTS (IF ANY)

None.

6. MEMBERS' ITEMS (IF ANY)

None.

7. FORMER PEEL CENTRE, PEEL DRIVE, COLINDALE, LONDON, NW9 5JE - H/04753/14 (COLINDALE WARD)

The Committee received the report and Addendum report.

Representations were heard from Mrs Veronica Heaven, Mrs Silvia Cabrera, Councillor Nagus Narenthira and Mr James Moody (Applicant).

RESOLVED – To approve the application as per the officer’s report and Addendum report and subject to (i) referral to the Mayor of London (ii) completion of the Section 106 Agreement and (iii) the conditions set out in the report.

The votes were recorded as follows:

For: 6

Against: 0

Abstained: 5

8. WOODSIDE PARK 'ERUV' - 15/00905/FUL (EAST BARNET, COPPETTS, EAST FINCHLEY, WOODHOUSE AND OAKLEIGH WARDS)

The Committee received the report.

Representations were heard from Mr Birju Shah and from Mr Daniel Rosenfelder (Applicant).

RESOLVED – That the Assistant Director – Development Management and Building Control approve the planning application reference 15/00905/FUL under delegated powers and grant planning permission subject to the conditions set out in the report and any changes to the wording of the conditions considered necessary by the Assistant Director – Development Management and Building Control, and the following informatives:

INFORMATIVES:

It is recommended that the following informatives be included on the decision notice in respect of this application:

- 1. The erection of the Eruv structures (poles, leci, wires and any other associated works) on the highway would require a Highways Licence under the Highways Act 1980. This Licence would be subject to a number of conditions such as design, use of an approved contractor, indemnity insurance and a bond. If there are problems with any of these matters the licence would not be granted. The Highway Licence covers the proposal in terms of the positions of each pole and will check for any potential concerns, including impacts on clutter, sight lines, obstruction (this would be assessed in relation to all including the needs of disabled people), security and technical specification (including colour of poles and type of wire). The terms of the Licence require weekly inspections for the lifetime of the Eruv and the applicant must submit reports on the outcome of the inspection, any defects identified and actions taken to resolve. The Highways*

Group also charge an annual fee via the licence to carry out ad hoc inspections to ensure maintenance is being carried out.

- 2. Structures located on a footway or a footpath must allow for a minimum clearance of 1.5 metres for pedestrians. Location of any existing furniture in the vicinity must be taken into consideration to ensure that the minimum clearance required for pedestrians is not compromised.*
- 3. In accordance with the general guidance given in the Traffic Signs Regulations and General Directions 2002, the applicant should ensure that structures located at the front of the kerb, on a verge or a footway should be a minimum of 0.45m away from the kerb line on borough roads and 0.6m on TLRN roads (trunk roads) to avoid damage and ensure safety.*
- 4. The applicant is advised that any structures to be sited within or project over adopted highway will require Licences under the Highways Act 1980 in addition to planning permission. The exact location and details of these structures will be agreed as part of the licensing process. Please note that Licenses under the Highways Act 1980 will be issued for structures located on areas under the Local Authority's responsibility. For structures located in other areas, the applicant should seek an agreement with the land owner. For structures impacting on adjacent boroughs, agreement must be sought from the relevant authorities.*
- 5. The applicant is advised that on sites located on traffic sensitive routes, deliveries during the construction period should not take place during restricted hours.*
- 6. Any and all works carried out in pursuance of this grant of planning permission will be subject to the duties, obligations and criminal offences contained in the Wildlife and Countryside Act 1981 (as amended). Failure to comply with the provisions of the Wildlife and Countryside Act 1981 (as amended) may result in a criminal prosecution.*
- 7. The applicant is advised that they would be fully responsible for the maintenance of the proposed Eruv poles, wire and leci to be placed on the public highway at all times.*
- 8. The applicant is advised that they would be liable for the cost of any rectification work to be undertaken to rectify damages caused to the public highway resulting from construction and maintenance of the proposed Eruv structures.*
- 9. The applicant is advised that they would be fully liable for claims and damages arising from third parties associated with the proposed Eruv poles, wire and leci to be erected on the public highway.*

The votes were recorded as follows:

For: 11

Against: 0

Abstained: 0

9. **NORTH WEST LONDON 'ERUV' - 15/01022/FUL - (EAST FINCHLEY; FINCHLEY CHURCH END; MILL HILL; GARDEN SUBURB; CHILDS HILL; AND EDGWARE WARDS)**

The Committee received the report.

RESOLVED – That the Assistant Director – Development Management and Building Control approve the planning application reference 15/01022/FUL under delegated powers and grant planning permission subject to the conditions set out in the report and any changes to the wording of the conditions considered necessary by the Assistant Director – Development Management and Building Control, and the following informatives:

INFORMATIVE:

It is recommended that the following informatives be included on the decision notice in respect of this application:

1. *The erection of the Eruv structures (poles, wires and any other associated works) on the highway would require a Highways Licence under the Highways Act 1980. This Licence would be subject to a number of conditions such as design, use of an approved contractor, indemnity insurance and a bond. If there are problems with any of these matters the licence would not be granted. The Highway Licence covers the proposal in terms of the positions of each pole and will check for any potential concerns, including impacts on clutter, sight lines, obstruction (this would be assessed in relation to all including the needs of disabled people), security and technical specification (including colour of poles and type of wire). The terms of the Licence require weekly inspections for the lifetime of the Eruv and the applicant must submit reports on the outcome of the inspection, any defects identified and actions taken to resolve. The Highways Group also charge an annual fee via the licence to carry out ad hoc inspections to ensure maintenance is being carried out.*
2. *Structures located on a footway or a footpath must allow for a minimum clearance of 1.5 metres for pedestrians. Location of any existing furniture in the vicinity must be taken into consideration to ensure that the minimum clearance required for pedestrians is not compromised.*
3. *In accordance with the general guidance given in the Traffic Signs Regulations and General Directions 2002, the applicant should ensure that structures located at the front of the kerb, on a verge or a footway should be a minimum of 0.45m away from the kerb line on borough roads and 0.6m on TLRN roads (trunk roads) to avoid damage and ensure safety.*
4. *The applicant is advised that any structures to be sited within or project over adopted highway will require Licences under the Highways Act 1980 in addition to planning permission. The exact location and details of these structures will be agreed as part of the licensing process. Please note that Licenses under the Highways Act 1980 will be issued for structures located on areas under the Local Authority's responsibility. For structures located in other areas, the applicant should seek an agreement with the land owner. For structures impacting on adjacent boroughs, agreement must be sought from the relevant authorities.*

5. *The applicant is advised that on sites located on traffic sensitive routes, deliveries during the construction period should not take place during restricted hours.*
6. *Any and all works carried out in pursuance of this grant of planning permission will be subject to the duties, obligations and criminal offences contained in the Wildlife and Countryside Act 1981 (as amended). Failure to comply with the provisions of the Wildlife and Countryside Act 1981 (as amended) may result in a criminal prosecution.*
7. *The applicant is advised that they would be fully responsible for the maintenance of the proposed Eruv poles, wire and other associated structures to be placed on the public highway at all times.*
8. *The applicant is advised that they would be liable for the cost of any rectification work to be undertaken to rectify damages caused to the public highway resulting from construction and maintenance of the proposed Eruv structures.*
9. *The applicant is advised that they would be fully liable for claims and damages arising from third parties associated with the proposed Eruv poles, wire and other structures to be erected on the public highway.*

The votes were recorded as follows:

For: 11

Against: 0

Abstained: 0

10. REPORT OF THE ASSISTANT DIRECTOR OF DEVELOPMENT MANAGEMENT AND BUILDING CONTROL

The Committee had considered the Addendum circulated under this item under the relevant applications.

11. LONDON ACADEMY SPUR ROAD EDGWARE HA8 8DE - 15/03550/FUL (EDGWARE WARD)

The Committee received the report and Addendum report.

RESOLVED – That the application be approved, subject to the conditions detailed in the report, the amendments in the Addendum and the following informative:

INFORMATIVE:

- 1 *In accordance with paragraphs 186 and 187 of the NPPF, the Local Planning Authority (LPA) takes a positive and proactive approach to development proposals, focused on solutions. The LPA has produced planning policies and written guidance to assist applicants when submitting applications. These are all available on the Council's website. The LPA has negotiated with the applicant/agent where necessary during the application process to ensure that the proposed development is in accordance with the Development Plan.*
- 2 *Your attention is drawn to the need to provide facilities for the disabled, as set out in Building Regulation Part M Access & Facilities for Disabled Persons. A copy of*

The votes were recorded as follows:

For: 11
Against: 0
Abstained: 0

12. OLD STATIONERS RECREATION GROUND BARNET LANE BARNET EN5 2UA - 15/01129/S73 (UNDERHILL WARD)

The Committee received the report and Addendum report.

The Committee received representations from Mr Robert Husband.

RESOLVED – That the application be approved subject to the conditions set out in the report, the Addendum report and the following informatives:

INFORMATIVES:

- 1 *In accordance with paragraphs 186 and 187 of the NPPF, the Local Planning Authority (LPA) takes a positive and proactive approach to development proposals, focused on solutions. The LPA has produced planning policies and written guidance to assist applicants when submitting applications. These are all available on the Council's website. The LPA has negotiated with the applicant/agent where necessary during the application process to ensure that the proposed development is in accordance with the Development Plan.*
- 2 *Applicants and agents are encouraged to sign up to the Considerate Contractors Scheme (www.ccscheme.org.uk) whereby general standards of work are raised and the condition and safety of the Borough's streets and pavements are improved.*
- 3 *The applicant is advised that due to the large number of schools and school children in the area, site deliveries during the construction period should not take place between 0800hrs - 0900hrs and 1500hrs - 1600hrs. Careful consideration must also be given to the optimum routes for construction traffic and the Traffic and Development section should be consulted in this respect.*
- 4 *Any and all works carried out in pursuance of this consent / notice will be subject to the duties, obligations and criminal offences contained in the Wildlife and Countryside Act 1981 (as amended). Failure to comply with the provisions of the Wildlife and Countryside Act 1981 (as amended) may result in a criminal prosecution.*

The votes were recorded as follows:

For: 11
Against: 0
Abstained: 0

13. ANY ITEM(S) THAT THE CHAIRMAN DECIDES ARE URGENT

There were none.

The meeting finished at 9.30 pm

This page is intentionally left blank

LOCATION: Brent Cross Cricklewood Regeneration Area, London, NW2 **AGENDA ITEM 7**

REFERENCE: 14/07402/CON **Received:** 11/11/2014
Accepted: 11/11/2014

WARD: Childs Hill, Golders **Expiry:** 06/01/2015
Green, West Hendon

APPLICANT: Brent Cross Development Partners

PROPOSAL: A5 Corridor Study submission to address condition 2.7 of S73 planning application ref: F/04687/13 dated 23/07/2014 for the comprehensive mixed use redevelopment of the Brent Cross Cricklewood Area.

RECOMMENDATION

This application is recommended for **APPROVAL**.

Informatives:

1. The plans accompanying this application are as follows:
A5 Corridor Study (BXCR-URS-47065005-TP-RPT-050 Rev 07) (Dated: July 2015)
 2. In accordance with Reg 3 (4) and Reg 8 (2) of the Town and Country Planning (Environmental Impact Assessment) Regulations 2011, it is considered that:
 - i. the submission under Condition 2.7 reveals, with regard to the subject matter of the condition, that there are no additional or different likely significant environmental effects than is considered in the environmental information already before the Council (the Environmental Statement (ES) (BXC02) submitted with the Section 73 application (F/04687/13) and any further and/or other information previously submitted; and
 - ii. the environmental information already before the Council (the ES submitted with the Section 73 application, along and any further and/or other information previously submitted) remains adequate to assess the environmental effects of the development.
-

1. APPLICATION SUMMARY

This application seeks approval of the A5 Corridor Study which has been submitted pursuant to condition 2.7 of the 2014 S73 Consent (reference F/04687/13).

The objectives of the A5 Corridor Study are to:

- Ensure any local traffic impacts are identified in the adjacent boroughs of Brent and Camden, as well as any further impacts in the London Borough of Barnet by ensuring the traffic modelling for the design stage is sufficiently detailed in areas of interest.
- Assess any identified impacts using appropriate junction modelling tools and produce outline designs of any mitigation measures where appropriate.
- Develop an A5 VISSIM design model, in conjunction with supporting local area models using complementary modelling packages TRANSYT and LinSig to assist with the following:
 - The development of detailed designs for the new and improved junctions.
 - The testing of any temporary traffic management measures during the construction period on highway operations.
- Define any new or improved facilities required in regard to multi-modal user requirements, e.g. walking and cycling and bus priority and setting out the identified interventions in line with the indicative phasing.
- Review of parking, loading and waiting restrictions along the A5 corridor, including any proposed changes to the existing provision and parking controls.
- Assess streetscape improvements, including enhancements to the public realm in relation to all modes.
- Undertake a road safety and accident analysis review.

The preparation of the A5 Corridor Study has involved officers working with the Developers, their advisors and other key stakeholders, such as TfL and the London Borough of Brent. The study includes an assessment of any local impacts of the BXC scheme, including in the adjoining areas within LB Brent and LB Camden, and builds on the outline scope originally contained in the 2010 Section 106 Agreement for the 2010 consented scheme. The full scope for the study, set out in the appendices to the BXC volume 5 Transport Report (2013) and attached as **Appendix 1**, has been agreed with all the relevant transport authorities and encompasses the detailed modelling of the A5 corridor and an assessment of all transport facilities along the A5 itself.

The assessment of any local impacts uses a new traffic model based on TfL's sub-regional model for north London, which is also being used for the detailed

junction design and approval processes under the Highways Act. This is known as the BXC Detailed Design Model (BXCDDM) and has been built using a national recognised traffic modelling package known as SATURN. This is discussed further in section 5.3. The developer is required under the 2014 S73 / S106 Agreement to fund any supplementary mitigation measures in Barnet, Brent and Camden that may be identified as part of the study.

The information submitted within the A5 Corridor Study (BXCR-URS-47065005-TP-RPT-050 Rev 07) dated 17th July 2015 uses the BXCDDM to identify the predicted changes in traffic flow across the study in 2021 (end of Phase 1) and 2031 (end state), with and without the development. A microsimulation (VISSIM) model has been utilised to assess the interaction of the junctions within the study corridor and alterations to all vehicular and bus journey times. Existing patterns of pedestrian and cyclist movements have been observed and existing associated facilities assessed. Bus priority, parking and servicing / delivery have also been reviewed.

Working in conjunction with officers of the London Borough of Barnet and Transport for London an agreed package of supplementary mitigation measures and improvements has been proposed by the Developers encompassing all modes of travel. The agreed package includes measures that the Developers will deliver directly in addition to and as part of schemes already part of the BXC phased improvement programme; alternatively, Developer contributions have been agreed so that the appropriate highway authority can implement the measures. The contributions include a capped sum of £300,000 secured for any potential traffic management measures that may be required on the local roads of Brent and Camden, should monitoring of future traffic levels identify noticeable increases in traffic that can be attributed to the BXC development.

2. BACKGROUND

2.1 Outline Consent

The principle of development at Brent Cross Cricklewood was first established by way of a site-specific Development Framework produced in April 2004 as Supplementary Planning Guidance (SPG) in accordance with the London Plan. The SPG established a vision to *'to create a new gateway for London and a vibrant urban area for Barnet'*.

The comprehensive redevelopment of the wider Brent Cross Cricklewood regeneration area was subsequently granted planning permission in outline in 2010 under planning permission C/17559/08 (the 2010 permission). Subsequently, this permission was revised under a Section 73 Planning application (F/04687/13) which was approved on 23 July 2014 (the 2014 permission) described below:

Section 73 Planning application to develop land without complying with the

conditions attached to Planning Permission Ref C/17559/08, granted on 28 October 2010 ('the 2010 Permission'), for development as described below: Comprehensive mixed use redevelopment of the Brent Cross Cricklewood Regeneration Area comprising residential uses (Use Class C2, C3 and student/special needs/sheltered housing), a full range of town centre uses including Use Classes A1 - A5, offices, industrial and other business uses within Use Classes B1 - B8, leisure uses, rail based freight facilities, waste handling facility and treatment technology, petrol filling station, hotel and conference facilities, community, health and education facilities, private hospital, open space and public realm, landscaping and recreation facilities, new rail and bus stations, vehicular and pedestrian bridges, underground and multi-storey parking, works to the River Brent and Clitterhouse Stream and associated infrastructure, demolition and alterations of existing building structures, CHP/CCHP, relocated electricity substation, free standing or building mounted wind turbines, alterations to existing railway including Cricklewood railway track and station and Brent Cross London Underground station, creation of new strategic accesses and internal road layout, at grade or underground conveyor from waste handling facility to CHP/CCHP, infrastructure and associated facilities together with any required temporary works or structures and associated utilities/services required by the Development (Outline Application).

Both the 2010 and 2014 permissions were subject to Environmental Impact Assessment.

The transport aspects of the approved BXC planning permission include the creation of new strategic highway accesses, a new internal road layout, infrastructure and associated facilities together with temporary works, structures and associated utilities/services required by the development. Rail based measures include provision of a new railway station and freight facilities. A new bus station is planned, together with vehicular and pedestrian bridges, underground and multi-storey car parking. Works to the River Brent and Clitterhouse Stream and associated infrastructure are also included together with improvements to Brent Cross London Underground Station. Pedestrian and cycle connectivity will be improved, in particular with the provision of the Living Bridge over the A406 North Circular Road which will provide better integration between the northern and southern components of the development.

The following transport documents were issued in support of the 2014 S73 application:

- BXC05 Volume 1 Consolidated Transport Assessment Main Report;
- BXC05 Volume 2 Consolidated Transport Assessment Appendices;
- BXC05 Volume 3 Consolidated Transport Assessment Travel Plans;
- BXC05 Volume 4 Consolidated Transport Assessment Highway Engineering Proposals;
- BXC05 Volume 5 S73 Transport Report; and
- BXC05 Volume 6 S73 Highway Engineering Report.

Phased Delivery

The Section 73 Consent proposes the phased delivery of acceptable comprehensive development for the whole site in accordance with the planning policy framework.

Phase 1 is proposed to be delivered in sub phases which are divided between north and south. The sub phases are as follows:

- Phase 1A (North) – this includes all the highways infrastructure to support the northern development including the key highways infrastructure to support the Phase 1 South, such as the improvements to the southern junctions of the A5/A407 Cricklewood Lane and the A407 Cricklewood Lane/Claremont Road Junction improvements. In addition the River Brent re-routeing and Bridge works will be delivered as part of Phase 1A (North), along with the Clitterhouse Playing Fields Part 1 (excluding the Nature Park) and the Claremont Park Improvements. The Living Bridge is included in (and its details will be approved before the commencement of) Phase 1A (North). Under the Revised Section 106 Agreement, its delivery will be triggered by the commencement of Phase 1B (North) and its delivery will be programmed to commence and be completed no later than before the occupation of Phase 1B North plots.
- Phase 1A (South) – A number of highway improvements needed to support Phase 1 of the Southern Development will be provided including the Waste Handling Facility (Diverted Geron Way/A5 junction); Claremont Park Road (Part 1) and the School Lane Works. In addition Waste Handling facility Rail Sidings and Gantry Craneworks and Threshold spaces at Layfield Place, Fenwick Place and Templehof Circus and Access to Plot 28 would come forward.
- Phase 1B (North) – This includes all of the plot development on the north side with the exception of the residential development within the Brent Cross West Zone. The sub phase also includes the new bus station, reconfigured shopping centre, Brent Cross Main Square, High Street North and other northern pedestrian routes, as well as the Riverside Park, Sturgess Park Improvements and approximately 300 housing units. Commencement of this Sub-Phase will trigger the BXP's obligations to deliver the Living Bridge which will link into the buildings and public realm to be provided on the Plots forming part of this Sub-Phase.
- Phase 1B (South) – This includes the Market Square, the Clarefield Park Temporary Replacement Open Space, the replacement food store, the Waste Handling facility, the CHP and the new and expanded Claremont School, in addition to more than 1000 residential units.

- Phase 1C – This will include the remaining plot development on the south side.

Pre-Reserved Matters Conditions

Due to the size and complexity of the scheme the outline planning permission acknowledged that there were a number of issues that require resolution prior the submission of Reserved Matters applications and prior to the commencement of development to ensure that development is brought forward in an acceptable way having regard to the EIA process and the environmental, social and transport impacts. As a result planning conditions attached to the 2014 S73 planning consent require a number of transport strategies, reports and feasibility studies to be submitted to the Council prior to submission of the first RMAs for the Development. These conditions are known as Pre-RMA conditions, and relate to overall transport strategies affecting the whole development, as well as information required that relates to the whole of Phase 1 and information required for just sub-phase 1A North.

The relevant transport Pre-RMA conditions comprise the following:-

Condition 1.9	Construction Consolidation Centre Feasibility Study
Condition 1.20	Area Wide Walking and Cycling Study (AWWCS)
Condition 1.21	Framework Servicing and Delivery Strategy
Condition 1.22	Phase 1A North Servicing and Delivery Strategy
Condition 2.7	A5 Corridor Study
Condition 2.8	Pedestrian and Cycle Strategy for Phase 1A North
Condition 7.1	Estate Management Framework
Condition 11.1	Car Parking Management Strategy
Condition 11.2	Phase 1 Parking Standards and Strategy
Condition 37.2	Phase 1 Transport Report
Condition 1.17	Illustrative Reconciliation Plan

This report relates only to the A5 Corridor Study and so seeks the discharge of condition 2.7.

3. DESCRIPTION OF THE SITE, SURROUNDINGS AND PROPOSALS

3.1 Site Description and Surroundings

The 151 hectare application site is defined to the west by the Edgware Road (A5) and the Midland Mainline railway line and to the east by the A41 and is bisected east to west by the A406 North Circular Road. It is adjacent to Junction 1 of the M1 (Staples Corner) and includes the existing Brent Cross Shopping Centre and Bus Station to the north or the A406.

To the south of the North Circular Road the area contains the Brent South Shopping Park, existing Tesco store and Toys 'R' Us store, the Whitefield

estate (approximately 220 units), Whitefield Secondary School, Mapledown Special School and Claremont Primary School, Hendon Leisure Centre, Brent Cross London Underground Station to the east, Clarefield and Claremont Parks and Clitterhouse Playing Fields (Metropolitan Open Land), the Hendon Waste Transfer Station, Claremont Way Industrial Estate and Cricklewood Railway Station to the far south. The application site includes parts of Cricklewood Lane, including the open space in front of the B & Q store.

Parking in and around the site is currently provided by way of controlled on-street zones, charged on-street bays, charged public off-street car parks, free off-street car parks and extensive free on-street car parking particularly in residential areas south of the A406.

A range of bus services, operated on behalf of TfL, pass through or close to the BXC site. The area is well served by the bus route network, with frequent services to a variety of destinations in London. The majority of the bus services start from or pass through the bus station at BXSC. This bus station serves the shopping centre and also operates as a local bus hub. Other bus hubs in the vicinity are located at North Finchley and Golders Green.

The Midland Mainline railway corridor passes through the western edge of the BXC site. The BXC site is served at the southern end by the existing Cricklewood Railway Station. Hendon Station is approximately 3km to the north, outside of the BXC site boundary.

The Edgware branch of the Northern Line passes to the east of the BXC site and the Jubilee Line passes to the southwest through Willesden Green and West Hampstead. Brent Cross Underground Station is nearest the site and is located to the southeast of the A406/A41 junction. To the north, Hendon Central is approximately 750metres from BXSC.

3.2 Description of Proposals

This application seeks clearance of condition 2.7 attached to application F/04687/13 in relation to the submission of the A5 Corridor Study. Condition 2.7 states:

Prior to or coincident with the submission of the first Other Matters Approval in respect of Phase 1 the A5 Corridor Study (including any necessary Supplementary Transport Measures required to address the detailed impacts identified in the study together with an indicative programme for carrying out such works) shall be submitted to the LPA, in consultation with the London Boroughs of Brent and Camden and the Transport Strategy Group. All other relevant Reserved Matters Applications and Other Matters Applications shall thereafter be in accordance with the A5 Corridor Study approved in accordance with this Condition (and including for the avoidance of doubt the approval of detailed delivery programmes in accordance with Condition 5 of this Permission).

Reason: To ensure the transport impacts of the scheme upon the A5 are fully evaluated and mitigated as part of the detailed design and programming of Phase 1 and the other relevant Phases of the Development.

Condition 2.7 prevents the submission of the first Other Matters Applications until the A5CS has been submitted. The Condition also requires that all relevant reserved matter applications shall thereafter be in accordance with the A5CS. The permission describes the A5 Corridor Study as follows:

'the A5 Corridor Study to cover the A5 between A407 Cricklewood Lane and Staples Corner including adjacent local roads where appropriate to be carried out by the developers on a joint and several basis and to be approved in accordance with:

(a) Condition 2.7 of this Permission; and

(b) the parameters and principles set out in Annex 7 to Schedule 17, the Matrix and Transport Reports Schedule to the S106 Agreement,

The A5 corridor study and monitoring, including bus journey times, should be used to inform future changes to the highways that serve the site, including modifying any of the gateway junctions, in accordance with the S106 Agreement (including the Matrix and Transport Reports Schedule).'

Schedule 17 of the Section 106 Agreement describes the framework of controls in relation to transport matters, which includes the A5 Corridor Study. Paragraph 4.9 of Schedule 17 explains that the scope for the A5 Corridor Study is set out at Annex 7 (or as agreed otherwise with the LPA in accordance with Condition 2.7).

Annex 7 sets out at Table 1 a scope of the A5 Corridor Study covering a variety of elements including traffic modelling, review of pedestrian and cycle routes, traffic management and accessibility.

The scope also describes the approach to local traffic management measures in Brent. It states that:

'The existing BXC strategic transport model will be used to further test any wider area implications that might arise from future local traffic management measures that are proposed to be introduced in LB Brent to address any supplementary/unforeseen impacts from the BXC proposals. Any changes to the strategic model, would be minor and targeted to provide a better representation of the local zonal structure and network, whilst still retaining the forecast

demand flows from the current matrices. In this manner, the fundamental traffic assumptions inherent within BXCO5 will be retained.'

The scope concludes by stating that:

'As a consequence of these tests, additional/ supplementary mitigation measures identified as being required will be the subject of detailed design, costing and public consultation and programmed for implementation at the appropriate time according to development phasing and impact. The associated costs of detailed design, costing, public consultation and implementation will be at the Developers' expense.'

The Development Partners subsequently prepared a detailed scope in accordance with Annex 7. On 12 August 2014 the London Borough of Barnet confirmed agreement to the scope of the A5 Corridor Study.

The agreed scope explains that the study has 3 primary objectives:

'1. Ensure that any local traffic impacts are identified in the adjacent boroughs of Brent and Camden by ensuring that the traffic modelling for the design stage is sufficiently detailed in areas of interest, e.g. the Dollis Hill area and south of Cricklewood Lane. Assess any identified impacts using appropriate junction modelling tools and produce outline designs of any mitigation measures.

2. Develop an A5 VISSIM design model to assist with the following:

- The development of detailed designs for the new and improved junctions along the A5 Edgware Road,*
- The testing of any temporary traffic management measures during the construction period on highway operations.*

3. Define any new or improved facilities required in regard to multi-modal user requirements, i.e. walking and cycling and bus priority, and setting out the identified interventions in line with the indicative phasing and construction programme.'

Paragraph 6 of Schedule 3 of the S106 Agreement states that the BXP's will fund or carry out or fund via a S278 Agreement the reasonable and proper costs of implementing any Supplementary Transport Measures in respect of the relevant Phase or Sub-Phase of the Northern Development identified in

the approved A5 Corridor Study. There are similar provisions for the Southern Development.

The Section 106 Agreement clearly defines Supplementary Transport Measures and explains that for those items that are necessary as a result of both Northern and the Southern development, both the Northern and Southern developers shall only be liable for such a proportion of those Supplementary Transport Measures as is reasonably related to the respective developments. Furthermore, the definition states that any Supplementary Transport Measures need to be directly, fairly and reasonably related to the development within the meaning of Regulation 122 of the Community Infrastructure Levy Regulations 2010 and be reasonably required in order to mitigate the impacts of the development.

4. MATERIAL CONSIDERATIONS

4.1 Key Relevant Planning Policy

In this case, the Development Plan comprises the London Plan (Consolidated with Further Alterations since 2011) (March 2015) at the strategic level and, at the local level, Barnet's Local Plan (Core Strategy (2012)) and the Saved UDP Policies GCRICK and C1-C11, which apply to the application site and are supplemented by the Cricklewood, Brent Cross and West Hendon Regeneration Area Development Framework (2005).

The Council's Development Management Policies DPD (2012) states at paragraph 1.4.3 that it will not apply to planning applications for comprehensive development in the Brent Cross unless and until the Core Strategy is reviewed in accordance with Policy CS2 and Section 20:13 of the Core Strategy.

Detailed consideration of the application against key London Plan and London Borough of Barnet policies can be found in **Appendix 2**. It is suffice to state here that the application is considered to be in accordance with Policy. The application is for matters reserved following the grant of the outline planning permission under the 2014 permission and as such the policy considerations have previously been considered and have been found to have been met.

4.2 Relevant Planning History

Reference:

C/17559/08 – granted 28 October 2010

Location:

Cricklewood Regeneration Area, North West London.

Description:

'Comprehensive mixed use redevelopment of the Brent Cross Cricklewood Regeneration Area comprising residential uses (Use Class C2, C3 and student/special needs/sheltered housing), a full range of town centre uses including Use Classes A1 – A5, offices, industrial and other business uses within Use Classes B1 - B8, leisure uses, rail based freight facilities, waste handling facility and treatment technology, petrol filling station, hotel and conference facilities, community, health and education facilities, private hospital, open space and public realm, landscaping and recreation facilities, new rail and bus stations, vehicular and pedestrian bridges, underground and multi-storey parking, works to the River Brent and Clitterhouse Stream and associated infrastructure, demolition and alterations of existing building structures, CHP, relocated electricity substation, free standing or building mounted wind turbines, alterations to existing railway including Cricklewood railway track and station and Brent Cross London Underground station, creation of new strategic accesses and internal road layout, at grade or underground conveyor from waste handling facility to CHP, infrastructure and associated facilities together with any required temporary works or structures and associated utilities/services required by the Development (Outline Application).

The application was accompanied by an Environmental Statement.'

Reference:

F/04687/13 – granted 23/7/14

Location:

Cricklewood Regeneration Area, North West London.

Description:

Section 73 Application to develop land without complying with the conditions attached to Planning Permission Ref C17559/08 granted on 28 October 2010 for comprehensive development (see description above).

4.3 Consultations and Views Expressed

Statutory consultees and other interest groups were initially consulted on 27th November 2014 allowing a 3 week period to respond.

Following the receipt of amendments and clarifications to the submitted study; statutory consultees and other interest groups were re-consulted on 20th July 2014 for a two week consultation period.

Though local residents were not directly consulted and it is not the council's requirement to consult local residents regarding conditions submitted to be discharged. Residents have been become aware of the condition submitted and have provided comments. 6 Letters of objection from residents were received in response to this second consultation period.

A detailed summary of the comments received from statutory consultees and other bodies and officer comments in response can be found under Appendix 3 of this report.

The consultation process carried out for this application is considered to be appropriate for a development of this nature. The extent of consultation exceeded the requirements of national planning legislation and the Council's own adopted policy.

Consultation Responses from Statutory Consultees and Other Bodies

Highways England – No objection

Email from Stephen Hall, Asset Manager, Highways England dated 28th July 2015.

TfL – No objection

Letter dated 19th January 2015

Made various comments in relation to modelling, transport improvements, bus journey time assessments, VISSIM, bus priority measures, bus service delays, loading and kerbside parking, urban realm, cycle measures. They concluded at that time that the A5 Corridor study is not to TfL's satisfaction.

Letter dated 1st September 2015 (following consultation on updated information in July 2015) confirm that TfL are satisfied with the A5 Corridor Study.

LB Brent – Object to the application

Full summary of LB Brent comments and LB Barnet officers responses are provided in Appendix 3.

LB Camden – Object to the application

Full summary of LB Camden comments and LB Barnet officers responses are provided in Appendix 3.

London Cycling Campaign (LCC) – Object to the application

Full summary of LCC's comments and LB Barnet officers responses are provided in Appendix 3.

Campaigns Manager, LCC – Object to the application

Full summary of the comments made by the manager of LCC and LB Barnet officers responses are provided in Appendix 3.

5. PLANNING AND TRANSPORT ASSESSMENT

5.1 Purpose of the Study

The scope and purpose of the A5 Corridor Study was agreed following significant consultation between the Developers and officers of the London Borough of Barnet and Transport for London. The scope sets out the aims and objectives of the study which in summary are to:

- Ensure any local traffic impacts are identified in the adjacent boroughs of Brent and Camden, as well as any further impacts in the London Borough of Barnet by ensuring the traffic modelling for the design stage is sufficiently detailed in areas of interest.
- Assess any identified impacts using appropriate junction modelling tools and produce outline designs of any mitigation measures where appropriate.
- Develop an A5 microsimulation (VISSIM) design model, in conjunction with supporting local area models, using complementary individual junction modelling packages (TRANSYT, LinSig and PICADY) to assist with the following:
 - The development of detailed designs for the new and improved junctions.
 - The testing of any temporary traffic management measures during the construction period on highway operations.
- Define any new or improved facilities required in regard to multi-modal user requirements, e.g. walking and cycling and bus priority and setting out the identified interventions in line with the indicative phasing.
- Review of parking, loading and waiting restrictions along the A5 corridor, including any proposed changes to the existing provision and parking controls.
- Assess streetscape improvements, including enhancements to the public realm in relation to all modes.
- Undertake a road safety and accident analysis review.

5.2 Area covered by the study

The A5 Corridor Study has been defined to cover a core area of the A5 between A407 Cricklewood Lane and A406 Staples Corner, including local roads within Barnet, Brent and Camden within an area anticipated to be approximately 800m from the A5, Claremont Road and the A407 Cricklewood Lane.

During the study, the microsimulation model has been extended from the A5 Corridor to include the A406 corridor between Staples Corner and the A406/A41 junction.

The area of the microsimulation model is shown on the plan in **Appendix 4**.

5.3 Methodology Used

Traffic flows within the study area have been based on the London Transportation Studies model, which Transport for London use as their

standard transport forecasting tool. A new sub-regional model of north London has been developed by TfL and it was agreed that this would be used as part of the detailed design for the phased BXC transport improvements. Known as the BXC Detailed Design Model (BXCDDM), this model was also used for the A5 Corridor Study as it provided more detailed, thorough and up to date (based on 2012 data) forecasts of traffic movements on local roads, particularly in LB Brent where the traffic model used to support the outline approved scheme (the BXC Transport Model, or BXCTM) was less detailed, and based on data from around 2006.

These BXCDDM predicted traffic flows have been modelled utilising SATURN computer software, the same package as was used for the BXCTM. The SATURN software enables traffic to reroute across the highway network to find the quickest path from an origin to a destination. The model has been updated with a greater level of detail of local roads and junctions, to enable the impact of changes to traffic flows on the local highway networks in Brent and Camden to be more accurately assessed.

The BXCDDM includes an allowance for background traffic growth and also enables the impact of other committed developments and / or highway schemes to be assessed as the traffic is reassigned across a wide highway network as a result of any such changes that take place. Area wide traffic forecasts have been produced for 2021 (the forecast year for phase 1) and 2031 (end-state forecast year) AM, PM and Saturday peaks.

The BXCDDM traffic modelling undertaken has been reviewed and accepted by officers of Transport for London and the London Borough of Barnet.

Where the BXCDDM traffic forecasts identify junctions in at least two time periods (as there is a limit to the accuracy that can be obtained from a strategic traffic model) reaching capacity (where the flow to capacity ratio is greater than or equal to 90%) with the proposed development but would be within capacity (less than 90%) if the development was not constructed, more detailed junction modelling has been undertaken using the industry standard software of TRANSYT or LinSig for traffic signal controlled junctions and PICADY for priority junctions.

Consideration has been given to the most appropriate package of mitigation, or as termed in the S106 agreement; 'Supplementary Transport Measures'.

Scheme designs for any additional mitigation in the form of Supplementary Transport Measures that are found to be necessary have been generated, including plans at an appropriate scale.

The microsimulation (VISSIM) model of the A5 Corridor has been developed for 2021. This microsimulation model provided more detail regarding movement on the highway than the BXCDDM (SATURN) model and enables:

- the interaction between adjacent junctions to be assessed;

- journey times for different modes, including buses, to be reviewed;
- the testing of any temporary traffic management measures during the construction period; and
- any new or improved facilities required in regard to multi-modal user requirements, e.g. walking and cycling and bus priority, to be defined.

Comprehensive bus journey time surveys of the A5 Corridor Study area were undertaken and compared to the times from the base year microsimulation model in the AM, PM and Saturday peak periods. As 85% or more of the routes analysed were within 15% or 60 seconds of observed data, the model was accepted as appropriate for use.

5.4 Findings of the Study

The area wide modelling identified the following junctions where, in at least two time periods, capacity was being reached in 2021 and / or 2031 when the traffic from the proposed BXC development was included, but that these junctions would otherwise be within capacity if the development was not constructed:

- A407 Chichele Road / Anson Road
- A407 Cricklewood Lane / A41
- Walm Lane / A407 High Road.

Approximately 40 junctions have been identified as reaching capacity with and without the development. Over half of which are proposed to be improved as part of the Brent Cross scheme. Of the remaining junctions only the following were identified as having a degree of saturation increase with the development in excess of 5% of the no development scenario:-

- A5 / Perryfield Way / Station Road (West Hendon)
- A5 / Cool Oak Lane (West Hendon)
- Lydford Road / A4003 Willesden Road (in Brent)
- A41 / The Vale.

Analysis has also been undertaken to identify the capacity, with development, at the following key traffic signal controlled junctions within the microsimulation model study area:-

- Staples Corner (A406/A5)
- A5 / Humber Road / Geron Way
- A5 / Oxgate Gardens / A5 Link Road
- A5 / Dollis Hill Lane / Residential Development
- A5 / Ashford Road / Depot Approach
- A407 Chichele Road / A5 Cricklewood Broadway
- A407 Cricklewood Lane / Claremont Road / Lichfield Road.

The results indicate at Staples Corner in 2021 the AM peak is within capacity but that three movements in the PM and Saturday peaks are overcapacity. In 2031 overcapacity is forecast for four movements. To reduce these queues, there is potential to adjust the signal times at the junction.

The A407 Cricklewood Lane / Claremont Road / Lichfield Road junction is forecast to be overcapacity in 2021 and 2031. However, deployment of a bespoke traffic signal control system at the junction will assist in mitigating the impact of additional traffic. Based on data collected by TfL, the deployment of such a system provides an average 12% reduction in delay.

The A407 Chichele Road / A5 Cricklewood Broadway is reaching capacity in 2031, with the A5 northbound movement predicted to be close to saturation in the PM peak period.

5.5 Mitigation Proposed

Various improvements have been proposed as part of the study, and these comprise of a mix of measures that will be directly delivered by the Developers, for example, as additional elements to schemes already included in the BXC phased transport improvements; alternatively, contributions have been agreed for the relevant highway authorities to implement the proposed improvement schemes themselves.

The various mitigation schemes are discussed in more detail under the headings below.

5.5.1 Junction Mitigation

Based on the area wide traffic modelling and detailed junction assessments above the following mitigation has been proposed:

A407 Chichele Road / Anson Road (Phase 1 and end state): Linkage to adjacent A5 Urban Traffic signal Control group, enabling improved traffic signal co-ordination, which can be expected to help alleviate any congestion and improve the overall efficiency of the local road network.

A407 Cricklewood Lane / A41 (end state): The detailed modelling identified that mitigation was not required.

Walm Lane / A407 High Road (end state): Pedestrian countdown at traffic signals is proposed to help mitigate the predicted increase in traffic demand. This feature is increasingly being introduced at signalised junctions in London and provides greater resilience to manage daily fluctuations in traffic flow and for occasions when pedestrian crossing demand is increased. The countdown enables greater green time to be provided to traffic at this location.

Of the junctions identified as overcapacity with and without the development (and so to be assessed as part of the 'Supplementary Transport Measures'), and having an increase with the development in excess of 5% of the without development scenario, the following mitigation is proposed:-

- A5 / Perryfield Way / Station Road: Committed improvements as part of the West Hendon Regeneration proposals.
- A5 / Cool Oak Lane: Committed improvements as part of the West Hendon Regeneration proposals.
- Lydford Road / A4003 Willesden Road: Detailed capacity analysis of this junction highlights that the junction is predicted to operate with adequate practical reserve capacity and a maximum degree of saturation at 85%.
- A41 / The Vale: Junction arrangement expected to be amended as part of the cycle super highway (CS11).

The key traffic signal controlled junctions within the section of A5 corridor under assessment have been analysed. The following are gateway junctions where design proposals as part of Phase 1A (North) (unless otherwise indicated) have already been approved as part of the 2014 Section 73 Consent:

- Staples Corner (Phase 1 and end state): Significant alterations to the existing junction.
- A407 Chichele Road / A5 Cricklewood Broadway (Phase 1 and end state): Compulsory purchase of the plot of land on the south east corner of the junction enables the arms of the A407 to be aligned and for them to operate at the same time creating a more efficient junction.
- A407 Cricklewood Lane / Claremont Road / Lichfield Road (Phase 1 and end state): A flared approach on the A407 western arm and an additional southbound lane on Claremont Road creating more traffic capacity.
- A5 / Humber Road / Geron Way (Phase 1 and end state): Four stage set of signals with advanced cycle stop lines and a pedestrian controlled crossing on the southern arm of the A5.
- A5 / Oxgate Gardens / A5 Link Road (end state): Creation of a four arm signalised junction to provide a new access across the Midland Mainline railway to the development in Phase 5 with Oxgate Gardens being one-way westbound.

In addition the A5 / Dollis Hill Lane junction is being converted from a three arm to a four arm traffic signal controlled junction, due to residential development on the former Parcelforce site.

The schedule of mitigation associated with the A5 Corridor Study is summarised in the four following tables which are provided in **Appendix 5**:

Table 1: Mitigation required for the A5 corridor.

Table 2: Mitigation required for the A407.

Table 3: Mitigation required for other areas.

Table 4: Further enhancements to encourage modal shift but not required to mitigate the development.

Within Tables 1 to 3, proposals are to be delivered directly by the Developers as part of Section 278 Agreements or via a contribution for implementation of schemes by the appropriate highway authority to complete the remaining works. A contribution of totalling £550,000 has been agreed with the Brent Cross Development Partners (letter dated 2nd September 2015). Within this contribution, a fund of £300,000 is to allow for traffic management interventions to be provided in the adjacent boroughs of Brent and Camden should monitoring demonstrate there are any additional adverse impacts of the development over and above those arising out of the study. The decisions on how to spend this money will be considered by the Transport Advisory Group (which includes TfL, LB Brent and LB Camden as members). Monitoring will be undertaken via the Monitoring Strategy (Condition 37.8). The mitigation proposals to be delivered within Phase 1 are identified in column 3.

Within Table 4, proposals are identified which will further contribute to the overall aims of the BXC development through encouraging mode shift. However, these are not considered necessary in order to mitigate the development, but are future proposals for the boroughs and TfL to progress as and when suitable funding becomes available.

Trips to and from the proposed development are expected to generally use the strategic highway network, the M1, A406, A5 and A41. Infrastructure improvements on these highways and junctions have been designed to accommodate the additional trips attributable to the BXC development. Mitigation is not proposed on all sections of these strategic roads, where significant increases in traffic are forecast, as these roads are designed to cater for such traffic.

These additional trips and infrastructure improvements will inevitably have an impact on existing trips and lead to re-routing of trips that have neither an origin nor a destination at the BXC development.

With regards to local roads, use of the BXCDDM has enabled changes in traffic flows to be assessed. The most significant increases in flow are predicted to be on local roads in Barnet, Brent and Camden, as tabulated below:

Link	Section	Direction	AM Peak	PM Peak	Sat Peak
A5	Layfield Road-Station	Northbound	*	*	

	Road				
Highfield Avenue	A41–The Drive	Eastbound	*		*
Humber Road	A5-Coles Green Road	Westbound	*	*	*
Parsifal Road	A41–Fortune Green Road	N/Eastbound	*		
Chichele Road	Anson Road-Walm Lane	S/westbound	*	*	
A41	A406 NCR - A598 Finchley Road	Northbound		*	
Claremont Road	Somerton Road-Pennine Drive	Southbound		*	*
Cricklewood Lane	Farm Avenue – A41	Eastbound			*
Fordwych Road	Maygrove Road-Mill Lane	Northbound			*
Walm Lane	Lydford Road-A5	Eastbound			*

Analysis of the traffic on these local roads indicates the majority of the increases are not BXC development related trips, with changes in the performance of links and junctions leading to re-routing of existing trips which in turn is the main contributor to increased flows on these roads.

There are some links with increased traffic flow which is related to the BXC development, such as Claremont Road, where changes to the layout and the strategic junctions mean these routes, being in such close proximity to the development, inevitably attract development related trips. However, the infrastructure improvements to these routes are designed to account for this increased demand.

Overall, total flows on all individual links within the BXCDDM model of the areas that fall within Camden and Brent, increase with the approved BXC development. The total increase (based on passenger car units where a cyclist = 0.5, a car = 1 and a HGV/Bus = 3) in each peak period is summarised in the following table:

Time Period	Camden Total Link Differences	Brent Total Link Differences
	Peak hour PCUs	Peak hour PCUs
AM 2021	2,946	4,724
AM 2031	8,282	7,072
PM 2021	5,173	4,390
PM 2031	7,331	8,461
Sat 2021	7,601	6,347
Sat 2031	10,046	12,347

The results indicate that with the development, there is an increase in traffic in all peak periods across the highway network in the neighbouring boroughs. However, it is generally predicted that the increases on the non-strategic local roads is due to rerouting of non-development related traffic, rather than due directly to development related traffic. Despite this, if monitoring of traffic flows shows noticeable increases in traffic flows on local roads in Brent or Camden due to the development, as mentioned above an additional capped contribution of £300,000 towards future Supplementary Transport Measures has been secured.

5.5.2 Pedestrian and Cycle Improvements

The A5 Corridor Study recognises that congestion on the network is a problem. Therefore, where practicable, as part of the overall approach to the A5 corridor and the wider regeneration scheme, where highway interventions are proposed, the aim has been to both protect buses from congestion, and encourage walking and cycling through positive design measures.

The A5 Corridor Study provides a review of pedestrian and cyclist accessibility, cycle parking and routing.

The volume of cyclists using the A5 corridor on a weekday ranges from 48 towards the north of the corridor (observed 2-way flow near Humber Road) to 73 towards the south of the corridor (observed 2-way flow near Chichele Road). The AM peak hour is the busiest period for cyclists out of the peak hours surveyed. Cyclists represent up to 4% of the traffic composition during this period.

On a Saturday, minimal cycle demand during the peak hour was observed with a maximum 2-way flow of 18 cyclists on the central section. Cyclists represent approximately 1% of the traffic composition during this period.

The existing pedestrian and cycle links along and alongside the A5 have been reviewed for this study using the Pedestrian Environment Review System (PERS) and (Cycling Environment Review System (CERS) assessment tools.

A total of 12 cycle links, 4 junctions and 4 cycle parking areas were audited along the A5. The links were determined by the changes in the cycle environment (such as type of cycle facility provided or change in surrounding land uses) and were separated as follows:

- Link 1 (L1): Staples Corner to Geron Way
- Link 2 (L2): Geron Way to Opposite Comfort Delgro Building
- Link 3 (L3): Opposite Comfort Delgro Building to Depot Approach
- Link 4 (L4): Depot Approach to A407 Junction
- Link 5 (L5): A407 Junction to Rondu Road
- Link 6 (L6): Rondu Road to Mill Lane
- Link 7 (L7): Mill Lane to Rondu Road

- Link 8 (L8): Rondu Road to A407 Junction
- Link 9 (L9): A407 Junction to Longley Way
- Link 10 (L10): Longley Way to Humber Road
- Link 11a (L11a): Humber Road to Staples Corner (on road route)
- Link 11b (L11b): Humber Road to Staples Corner (off road route)
- Link 12a (12a): Across A5 / A406 Staples Corner Junction (off road)
- Link 12b (12b): Across A5 / A406 Staples Corner Junction (on road)

With the exception of cycle parking near Keyes Road, which scored as green (good), all cycling provision was rated as amber (average).

The improvements put forward as part of the A5 study to improve conditions for pedestrians and cyclists on the A5 and encourage more people to travel by both modes on the corridor are contained within **Appendix 6**. The suggested improvements are initial proposals that are subject to feasibility and detailed design at a later stage:

5.6 Parking, loading and waiting restrictions

The Controlled Parking Zones within Barnet which are in closest proximity to the A5 Corridor Study area are:

- CT: Close to Cricklewood Railway Station and Cricklewood Town Centre: 09:00-20:00 Mon – Sun.

The Uncontrolled and Controlled Parking Zones within Brent which are in closest proximity to the A5 Corridor Study area are:

- UC7: The Dollis Hill area located to the west of the A5, between Brent reservoir and Gladstone Park with approximately 1,950 spaces.
- GM: Cricklewood Town: A5 to east, Dollis Hill to the north, Gladstone Park to the west and Olive Road to the south: 10:00-21:00 Mon – Sat.
- MA: Mapesbury Road: A5 to the east and Chichele Road to the west: 10:00-21:00 Mon – Sat.

The Controlled Parking Zones within Camden which are in closest proximity to the A5 Corridor Study area are:

- CA-P: University College Sports Ground to the north, Fortune Green Road to the east, Minster Road to the south, Westbere Road to the west: 10:00-12:00 Mon – Fri.

The CPZ is located approximately 3 km from Brent Cross Shopping Centre and 2.4 km from the centre of the Regeneration Area to the south of the A406.

- CA-Q: Richborough Road to the north, Fordwych Road to the east, Minster Road to the south, A5 Cricklewood Broadway to the west: 08:30-18:30 Mon-Fri

The CPZ is located approximately 3.1 km from Brent Cross Shopping Centre and 2.5 km from the centre of the Regeneration Area to the south of the A406. Cricklewood Railway Station is located approximately 200 metres to the north of the CPZ.

Monitoring of parking will be undertaken, taking into account any concerns from residents. The funding of new or extended Controlled Parking Zones is available through the Consolidated Transport Fund (maximum £1.25m) and would need to be applied for either through the Transport Advisory Group or directly to the Transport Strategy Group (London Borough of Barnet and TfL). The Transport Strategy Group is required to take account of the Transport Advisory Group's recommendations. The requirement for Controlled Parking Zones in relation to construction worker parking activity within Brent has been raised and discussed at the Transport Advisory Group and the need for provision within the Dollis Hill area (UC7) outside the scheme boundary has been agreed between Brent and the developer (as this is outwith the Section 106 agreement related to the Brent Cross Cricklewood development) with an associated financial contribution of £180,000.

Existing parking demand and servicing was surveyed and taken into account in the modelling of the A5 corridor. Observations indicated unloading takes place when prohibited in the AM peak hour on the southbound carriageway at the A5/A407 junction, highlighting a lack of enforcement. Therefore although the analysis has not highlighted any requirement to amend parking, loading or waiting restrictions along the A5, it has identified that the enforcement of current restrictions could be improved. As the detailed design of improvements on the A5 progresses, TfL guidance relating to freight will be taken into account.

5.7 Road Safety

Between 1st March 2008 and 31st December 2013 (70 months) a total of 267 accidents have occurred in the study area.

An examination of the recorded accidents indicated that 236 of the incidents resulted in slight injury (88.4%), 26 of the incidents resulted in serious injury (9.7%) and 5 incidents resulted in a fatality (1.9%).

A comparison of the proportion of accident severity types observed along the A5 corridor with proportions for LB Barnet, outer London boroughs and Greater London (based on data from TfL's *Level of collision risk in Greater London (issue 13), March 2013*) indicates that the make-up of the severity of collisions along the A5 Corridor study area is similar.

However, the number of accidents per km along the A5 corridor (9.31) in 2010 was higher than the average for Barnet (8.02), outer London boroughs (6.77) and Greater London (8.91), although slightly below 'A roads' in the LB Brent (9.35).

Many of the proposals associated with the BXC scheme are likely to enhance levels of road safety along the corridor, potentially reducing accident rates. The proposed BXC design includes a number of junction improvements along the A5 corridor, including new signalised junctions, controlled pedestrian crossings, footway improvements and cycle facilities, improved surfacing and road markings all of which will contribute towards the safe operation of junctions and links.

The following junctions in the study area have recorded the largest number of incidents:

- Staples Corner: 60
- A5 Cricklewood Broadway / A407 Cricklewood Lane / Chichele Road: 40
- A5 Cricklewood Broadway / Temple Road: 16
- A5 Edgware Road / Geron Way (S): 14
- A5 Edgware Road / Oxgate Lane: 12
- A5 Cricklewood Broadway / Ashford Road/ Depot Approach: 12

Junction improvements at the gateway junctions of Staples Corner and the A5 Cricklewood Broadway / A407 Cricklewood Lane / Chichele Road junctions, which have the greatest numbers of accidents, have already been approved as part of the Section 73 Planning Consent. The further improvements proposed within the A5 Study should assist in reducing accidents at the other junctions along the corridor.

In terms of accidents involving heavy goods vehicles, a total of 40 collisions occurred within the study area. This constituted 15% of all collisions along the corridor. 35 of the 40 collisions resulted in slight injury (87.5%), 2 of the incidents resulted in serious injury (5%) and 3 incidents resulted in a fatality (7.5%).

In total, 15 collisions occurred that involved goods vehicles and vulnerable road users within the study area. 9 of the 15 collisions involved non-motorised users (5 involved pedestrians and 4 cyclists) with the remaining 6 involving motorcyclists.

Of the 15 collisions, two were fatal, two resulted in serious injury and 11 were classified as slight in severity. The collisions involving goods vehicles and vulnerable road users occurred at a number of different locations throughout the scheme, with some occurring at junctions and others on links between junctions. An examination of the collisions revealed there were no discernible patterns in terms of the conditions, manoeuvres or characteristics of these types of collisions which occurred. Therefore, no improvements are required.

5.8 Bus priority

The BXC Development has an objective to encourage mode shift away from car borne travel. A target of 17% all development users to travel by bus at the end of Phase 1 rises to 32% by Phase 5, before reducing to 27% once the new train station is operational in the end state. The aim of the A5 Corridor Study in the s106 includes improving conditions for bus users.

The following bus services and associated routes operate within the study area:

- 16 - Mora Road to Victoria Station
- 32 - Edgware Station to Kilburn Park Station
- 245 - Golders Green via Cricklewood Station to Glacier Way
- 266 - Brent Cross Shopping Centre to Hammersmith via Willesden Junction station
- 316 - Mora Road to White City Bus Station
- 332 - Bishops Bridge to Brent Park Tesco
- N16 - Edgware Road station to Victoria Station
- 189 - Brent Cross Shopping Centre to John Prince's Street / Oxford Circus
- 226 - Golders Green station to Ealing Broadway
- 260 - Golders Green station to White City Bus Station
- 460 - North Finchley Bus Station to Pound Lane

There are 14 bus stops situated on the A5 Edgware Road between Staples Corner and the A407 Chichele Road/Cricklewood Lane junction. A further seven bus stops are located within the A5 Corridor Study area within proximity to the Chichele Road/Cricklewood Lane junction.

Bus lanes are located at a number of locations within the vicinity of the BXC development area with approximately 30% of the length of the A5 corridor between Staples Corner and Anson Road being specified as bus lane.

Analysis of base year traffic congestion for bus journey times along the A5 corridor has been undertaken using TfL (iBus) data. Wait times have also been reviewed.

The two key principles for the future year bus strategy consist of:

- Ensuring that sufficient capacity is in place to accommodate all expected bus passengers for each phase of development; and
- Make efficient use of the new transport infrastructure.

The proposed junction and bus infrastructure improvements at end state are highlighted in **Appendix 7**. For the A5 corridor, the junction improvements proposed, in conjunction with the existing bus lanes, results in less delay to buses when compared to all vehicles.

A comparison between the journey times of all traffic and that of just buses travelling on the A5 between the A5/A407 junction and Humber Road has been undertaken based on observed times in 2013 and microsimulation model times in 2021. The results indicate:

2013-2021	Direction	All modes	Buses
AM Peak	Northbound	17%	76 seconds (21%)
	Southbound	55%	94 seconds (25%)
PM Peak	Northbound	12%	52 seconds (12%)
	Southbound	15%	21 seconds (5%)
Saturday Peak	Northbound		-8 seconds (-2%)
	Southbound	9%	1%

Both in a northbound and southbound direction, in the AM peak, PM peak and Saturday peak periods (with the exception of northbound in the AM peak), delays to buses are less than for all traffic. This is due to the impact of bus priority measures. Therefore, the proposals envisaged are sufficient.

5.9 Monitoring

There is a requirement under Condition 37.8 of the Section 73 Consent for a separate Monitoring Strategy Report which must be submitted and approved prior to commencement of any part of the development. This will include monitoring on the A5 corridor.

The need to monitor the operation and transport related impacts of the development takes several forms. The information obtained from the various surveys and sources will be used to both control the impacts arising from the Development and to inform the later stages of the detailed design

The scope of the monitoring is to be agreed prior to commencement of each phase and reviewed annually. The minimum requirements include the following:

- Data for Travel Plans and Delivery Servicing Plans to be updated annually.
- Reporting on construction traffic to be undertaken every 6 months.
- Annual or pre-phase Bus journey time reliability surveys. Annually if based on iBus data or via a series of rolling surveys.
- Surveys for detailed design as and when required.
- Baseline information to be collected as part of Reserved Matter Transport Report where there are more than minor impacts on the networks.
- On going Automatic Traffic Count data, including on local roads, to produce trends and local growth factors, and monitor any local 'rat-running'.

5.10 Summary of the Findings

The analysis undertaken has identified the junctions within the study area where future capacity issues are likely to occur. These include three junctions where the development causes the capacity to exceed 90% and four junctions at capacity where the development causes an increase of saturation in excess of 5%.

Junction improvements are proposed at the previously approved gateway junctions (as per the Section 73 Consent), or via S278 works and a £550,000 fund agreed by the Brent Cross Development Partners in a letter dated 2nd September 2015. As part of this fund, £300,000 has been secured for supplementary measures to mitigate the impact of the development on local roads in the boroughs of Brent and Camden.

Existing pedestrian and cyclist use of the A5 corridor has been assessed. Current cycle usage is minimal and improvements including advanced stop lines, improved surfacing, cycle symbols markings and signage are proposed. The analysis has not highlighted any requirement to amend parking, loading or waiting restrictions along the A5 although the assessment has identified that the enforcement of current restrictions could be improved.

The make-up of the severity of collisions along the A5 Corridor is typical for London, although the number of accidents per km is slightly high. Of the 15 collisions that involved goods vehicles and vulnerable road users (4 of which were cyclists) in the 70 month period analysed, two were fatal and two resulted in serious injury. An examination of these collisions revealed there were no discernible patterns in terms of the conditions, manoeuvres or characteristics of these types of collisions which occurred. Therefore, no associated improvements are proposed.

Due to the impact of bus priority measures, in all three peak periods (with the exception of northbound in the AM peak) the predicted delays to buses in 2021 is less than for all traffic.

6. ENVIRONMENTAL IMPACT ASSESSMENT

The EIA procedure in the UK is directed by the Town & Country Planning (Environmental Impact Assessment) Regulations 2011 (the 'Regulations'), EU Directive 85/337/EEC (as amended), as well as the National Planning Practice Guidance (2014).

In accordance with Reg 3 (4) and Reg 8 (2) of the Town and Country Planning (Environmental Impact Assessment) Regulations 2011, it is considered that the submission under Condition 2.7 reveals, with regard to the subject matter of the condition, that:

- i. there are no additional or different likely significant environmental effects than is considered in the environmental information already before the Council (the Environmental Statement (ES) (BXC02) submitted with the Section 73 application (F/04687/13) and any further and/or other information previously submitted; and
- ii. the environmental information already before the Council (the ES submitted with the Section 73 application and any further and/or other information previously submitted) remains adequate to assess the environmental effects of the development.”

7. EQUALITY AND DIVERSITY ISSUES

Section 149 of the Equality Act 2010, which came into force on 5th April 2011, imposes important duties on public authorities in the exercise of their functions, including a duty to have regard to the need to:

- “(a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;*
- (b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;*
- (c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.”*

For the purposes of this obligation the term “protected characteristic” includes:

- age;
- disability;
- gender reassignment;
- pregnancy and maternity;
- race;
- religion or belief;
- sex; and
- sexual orientation.

Officers have in considering this application and preparing this report had regard to the requirements of this section and have concluded that a decision to grant approval for the discharge of this condition will comply with the Council’s statutory duty under this important legislation.

8. CONCLUSION

Officers have worked closely and collaboratively with the Developers and their advisors and have liaised appropriately with other key stakeholders to ensure that the impacts of the development on the A5 Corridor Study network have been robustly assessed and the enhanced mitigation package is appropriate.

The information submitted is considered to meet the requirements for the discharge of condition 2.7 of outline planning consent F/04687/13. It is

considered that the details submitted are acceptable and therefore APPROVAL is recommended in order to allow condition 2.7 to be discharged.

LIST OF APPENDICES

- APPENDIX 1 – Scope of the A5 Corridor Study
- APPENDIX 2 – Policy Compliance
- APPENDIX 3 – Objections and Officer Responses
- APPENDIX 4 – Extent of the A5 Corridor Study VISSIM Model
- APPENDIX 5 – Schedule of mitigation required as part of A5 Corridor Study
- APPENDIX 6 – Plan of Pedestrian and Cycling Improvements
- APPENDIX 7 – Plan of Bus and Infrastructure Improvements

This page is intentionally left blank

Appendix 1

Scope of Application documents for the A5 Corridor Study excluding Appendixes B and C



Brent Cross Cricklewood Regeneration

Scope of Application documents for:

A5 Corridor Study

October 2013

Doc. No: 47065005/TP/RPT/009



Prepared for



A5 Corridor Study

October 2013

Doc. No: 47065005/TP/RPT/009

URS Infrastructure & Environment (UK) Ltd

Scott House
Alençon Link
Basingstoke
Hants RG21 7PP

Tel +44 (0)1256 310 200
Fax +44 (0)1256 310 201

www.urs.com

Table of Contents

1	Introduction	1
1.1	Context of the Study	1
1.2	Objectives of the study	2
2	Background	4
2.1	Relevant planning conditions	4
2.2	BXC Detailed Design Model.....	5
2.3	Other Relevant Transport Proposals	5
2.4	Implementation	6
2.5	Phase Transport Reports	6
3	London Boroughs of Brent and Camden Area Studies	7
3.1	Stage 1 – Identify areas of material impact	7
3.2	Stage 2 – Detailed local road assessments	8
4	A5 VISSIM Design Model	9
5	A5 Multi-Modal Assessment.....	12
5.1	Bus priority	12
5.2	Parking, loading and waiting restrictions	12
5.3	Pedestrian and Cycling	13
5.4	Road Safety.....	13
5.5	Programme.....	13

Appendices

- Appendix A – Annex 7 Schedule 17 from the signed S106 Agreement
- Appendix B – Programme
- Appendix C – Current ‘living’ scope of the BXC DDM

1 Introduction

1.1 Context of the Study

There is a current planning consent in place for the Brent Cross Cricklewood Development (BXC). The regeneration proposals received outline planning consent from Barnet Council in October 2010. Attached to the permission are a number of planning conditions. These cover a wide range of topics and are to be discharged at defined points within the submission of reserved matters, detailed design process, construction and operation of the development. A condition of this consent was a requirement for an A5 Corridor Study to be submitted as a reserved matter. An initial scope of this study was included in the Section 106 agreement (see Appendix A).

There is a considerable amount of overlap between the individual conditions imposed on the permission. The A5 Corridor Study includes elements that will be input into work that will subsequently be undertaken to discharge other conditions, for instance the Area Wide Walking and Cycling Study. The purpose of this document is to further define the scope of the A5 Corridor Study and identify which elements of the original scope feeding into the discharge of other conditions.

Each element of the scope as defined in the Section 106 has been divided into key stages and defined and discussed in more detail. This version of the report aims to incorporate comments from Transport for London (TfL) and the London boroughs of Barnet, Brent and Camden.

For such a study, it is important to set out the roles and responsibilities of the local authorities along the A5 corridor, outlined below:

- London borough of Barnet – The section of the A5 running through the study area demarcates the Barnet/Brent borough boundary. An agreement between the boroughs is in place whereby Barnet are responsible for the management and maintenance of this section of the A5. Figure 2 shows borough boundaries. This section of the A5 is designated as strategic road network (SRN), where TfL are statutory consultees. Under the traffic management act, the London borough of Barnet has a responsibility to ensure its road network is managed effectively to minimise congestion and disruption to vehicles and pedestrians. In addition, the borough will review the impact of the development in their borough based on outputs from a strategic traffic model and assist with the development, review and approval of any mitigation packages deemed necessary from the traffic modelling assessment. Barnet are ultimately responsible for signing off the corridor study and clearing the condition in liaison with TfL through the Transport Strategy Group, and with all stakeholders via the Transport Advisory Group.
- London borough of Brent – will review the impact of the development in Brent based on outputs from a strategic traffic model and assist with the development, review and approval of any mitigation packages deemed necessary in Brent from the traffic modelling assessment.
- London borough of Camden – as above.
- TfL – TfL's road network (TLRN) in this area includes the A406, the A41, A5 slip roads on the approach to the intersection known as Staples Corner and the A5 between the A406 and Oxgate Lane, and therefore have a responsibility to ensure its road network is managed effectively to minimise congestion and disruption to vehicles and pedestrians. The remainder of the A5 in this area is designated as SRN (as described

above). TfL are also responsible for traffic signals on both the TLRN and SRN together with bus operations through the A5 corridor. TfL will appoint network assurance, model and signal audit engineers to review and approve each modelling stage associated with the proposed junction improvements on the TLRN and SRN.

1.2 Objectives of the study

There are three primary objectives of the A5 Corridor Study that the original scope captured. These are defined below:

1. Ensure that any local traffic impacts are identified in the adjacent boroughs of Brent and Camden by ensuring that the traffic modelling for the design stage is sufficiently detailed in areas of interest, e.g. the Dollis Hill area and south of Cricklewood Lane. Assess any identified impacts using appropriate junction modelling tools and produce outline designs of any mitigation measures.
2. Develop an A5 VISSIM design model to assist with the following:
 - i. The development of detailed designs for the new and improved junctions along the A5 Edgware Road,
 - ii. The testing of any temporary traffic management measures during the construction period on highway operations.
3. Define any new or improved facilities required in regard to multi-modal user requirements, i.e. walking and cycling and bus priority, and setting out the identified interventions in line with the indicative phasing and construction programme.

In order to address these three objectives the 'A5 Corridor Study' has been broken down into three individual components:

- London Borough of Brent and London Borough of Camden Area Studies
- A5 VISSIM Design Model
- A5 Multi Modal Assessments.

The structure of this scope is outlined below:

- Section 2 – Background: a description of the relevant planning condition (2.7) has been included together with background information about the BXC Detailed Design Model that is currently being developed, and is intended to be used to undertake the London Borough of Brent and London Borough of Camden Area Study.
- Section 3 – London Borough of Brent and London Borough of Camden Area Studies: provides a description of a two stage process that will identify in detail any areas of impact on local roads which have not already been identified as part of the work undertaken prior to 2010 within London boroughs of Brent and Camden and to develop and agree any necessary mitigation measures through detailed assessment that may include junction modelling.
- Section 4 – A5 VISSIM Design Model: define the extent of the VISSIM model along the A5 itself, the scenarios to be developed and tested, and how the models will be progressed.
- Section 5 – A5 Multi-modal assessment: feeding into a number of parallel and other assessments such as the area wide walking and cycling study, as well as undertaking

further analysis on bus priority, waiting and loading restrictions. Combining together all planned transport measures proposed along the corridor and linking them to the indicative programme.

Figure 1 provides an overview of the major elements of the A5 Corridor Study. The study will be used to inform the development of Phase Transport Reports, detailed further in section 2.5.

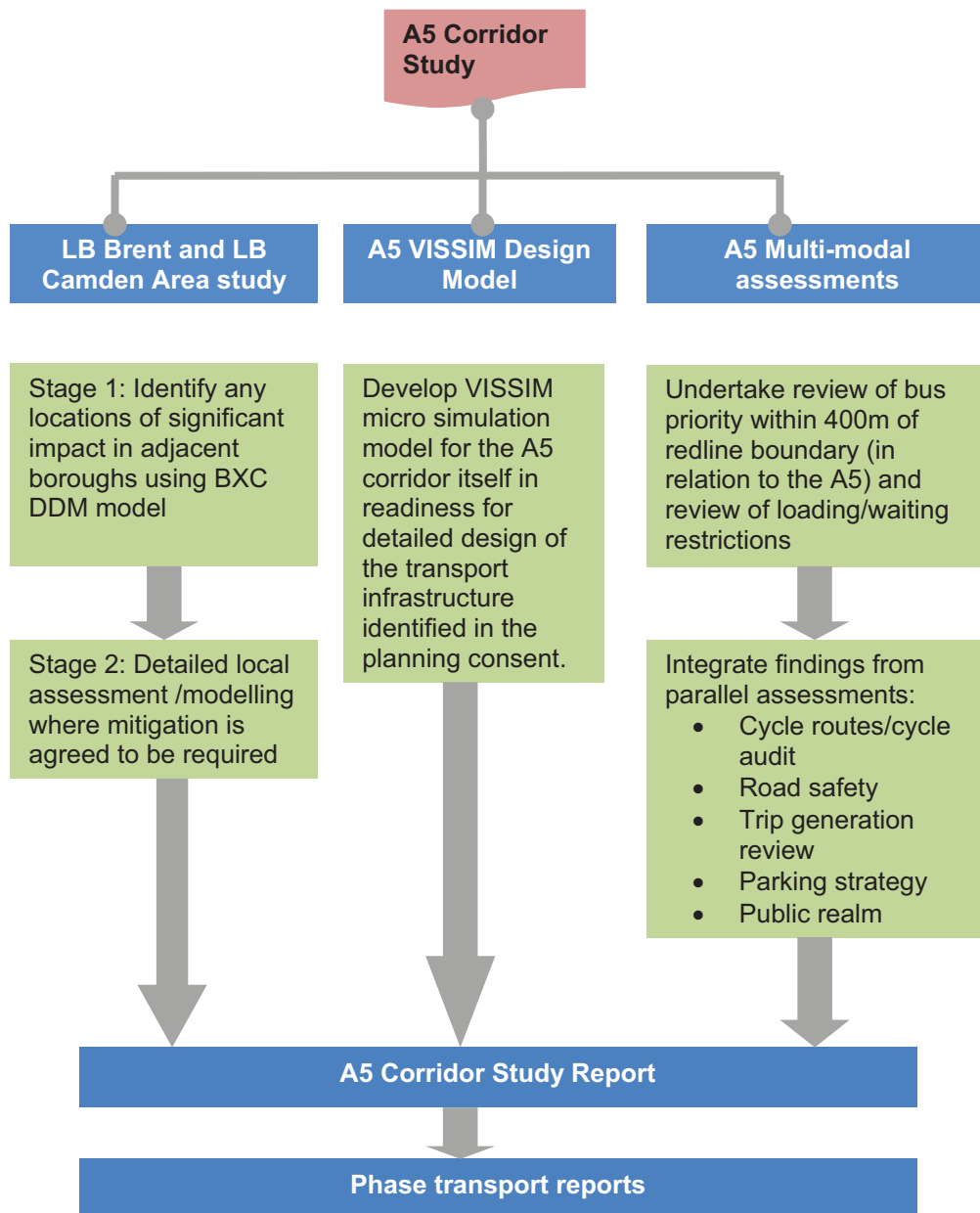


Figure 1 An overview of the key stages of the A5 Corridor Study

2 Background

2.1 Relevant planning conditions

Condition 2.7 of the outline planning permission states:

Prior to or coincident with the submission of the first Other Matters Approval in respect of Phase 1 the A5 Corridor Study (including any necessary Supplementary Transport Measures required to address the detailed impacts identified in the study together with an indicative programme for carrying out such works) shall be submitted to and approved by the LPA, in consultation with the London Boroughs of Brent and Camden and the Transport Strategy Group. All other relevant Reserved Matters Applications and Other Matters Applications shall thereafter be in accordance with the A5 Corridor Study approved in accordance with this Condition (and including for the avoidance of doubt the approval of detailed delivery programmes in accordance with Condition 5 of this Permission).

Reason: To ensure the transport impacts of the scheme upon the A5 are fully evaluated and mitigated as part of the detailed design and programming of Phase 1 and the other relevant Phases of the Development. The scope of the A5 Corridor Study was included within Annex 7 of schedule 17 of the signed S106 agreement (see Appendix A at the rear of this note) and is summarised below.

The A5 Corridor Study has been defined to cover a core area of the A5 between A407 Cricklewood Lane and A406 Staples Corner, including local roads within Barnet, Brent and Camden within an area anticipated to be approximately 800m from the A5, Claremont Road and the A407 Cricklewood Lane.

Table 1 lists the elements of the A5 study as detailed in Annex 7. It also illustrates where parallel work streams are being undertaken. Elements of the original A5 Corridor Study scope that are addressed in this report are also shown and highlighted in italics, and detailed further below.

Table 1 Elements of A5 scoping study as defined in annex 7 of Schedule 17 of S106

Element	Other Related Studies
<i>Traffic modelling – 2031* AM and PM peaks</i>	a) <i>A5 Corridor Study</i>
a) <i>Traffic modelling to inform detailed design</i>	b) Separate assessment being undertaken to update TA
b) Production of traffic forecasts	
Pedestrian environmental review/pedestrian routes	Area Wide Walking and Cycling Study
Pedestrian accessibility	Area Wide Walking and Cycling Study
<i>Traffic management – parking, loading and waiting review</i>	<i>A5 Corridor Study, Parking Strategy & Servicing and Delivery Strategy</i>
Cycle routes/cycle audit	Area Wide Walking and Cycling Study
<i>Traffic management – review of signals/linked signals/traffic management</i>	<i>A5 Corridor Study</i>
<i>Review of bus priority including all bus stops within 400m of the redline boundary</i>	<i>A5 Corridor Study</i>

Element	Other Related Studies
Public realm	Area Wide Walking and Cycling Study
Road safety/accident data review	S73 Transport Assessment
* End state assessment year altered to 2031 in line with the proposed BXC DDM methodology	

2.2 BXC Detailed Design Model

The overarching reason for this study is to identify and mitigate in more detail any local impacts of the development within the LB Brent and Camden and elsewhere in Barnet, that were assessed at a more strategic level of detail in the transport modelling for the initial outline planning application.

The authorities' interest and objective of the study is for the developers and highway authorities to agree a corridor based approach for this part of the A5 that can serve as a benchmark for subsequent phases of BXC development that includes measures proposed by the developers, any additional mitigation needed, and measures funded by the consolidated transport fund in co-ordination with public funded and third party measures/works that may emerge in this area during the life of the scheme.

Since the signing of the section 106 agreement in 2010, the Development Partners have agreed to develop a new BXC Detailed Design Model to ensure compliance with current TfL standards when informing the detailed design of the proposed highway improvements. The new transport model, known as the BXC DDM, comprises a core area, part of the NoLHAM model (TfL's area wide SATURN highway assignment model) and part of Railplan v6, TfL's public transport model. Future year models are being developed for years 2021 (to represent phase 1) and 2031 (to represent end state).

The scope for the development of the BXC DDM model is currently under development and forms an 'emerging document' as the detailed work progresses. A draft copy of the BXC DDM scope is provided in Appendix C. It is worth noting that this document will evolve as the project develops. The new BXC DDM model has been scoped to have a core area which includes the area identified in Annex 7 of Schedule 17, and so provides the opportunity to ensure that any detailed impacts within this area are identified and mitigated to ensure compliance with the section 106 requirements.

The BXC DDM provides context for detailed design models and design approval. It will be used iteratively not deterministically. The local authorities are expected to use their judgement and discretion on specific measures in a reasonable manner and are expected to propose measures they deem relevant or needed.

2.3 Other Relevant Transport Proposals

As part of the A5 studies, it will be necessary to identify and review any current proposals for transport improvements within the area of interest agreed with the authorities. It is proposed to liaise with the authorities to identify any proposals under the following headings to inform the development of the required models:

- Planned improvements (funded with programme date)
- Longer term proposals
- Major maintenance / renewals planned

- Major utility proposals
- Other third party works

This information will be collected and data incorporated as appropriate. All background information will be reported within the A5 Corridor Study Report.

2.4 Implementation

The implementation of any proposals identified through the A5 Area Studies will be monitored by the Transport Advisory Group (TAG), and subject to the agreement of the Transport Strategy Group (TSG).

2.5 Phase Transport Reports

Following the submission and approval of the A5 Corridor Study Report and therefore the discharge of condition 2.7 all subsequent changes to the A5 Corridor which are brought forward under later phases of the development will be addressed in the appropriate phase transport report, which has to be prepared in respect of each phase or sub-phase as a further S106 condition.

3 London Boroughs of Brent and Camden Area Studies

3.1 Stage 1 – Identify areas of material impact

As described earlier, the BXC DDM scope is currently being developed such that the model will be able to provide adequately detailed information within Brent and Camden to satisfy the requirements outlined in Annex 7, i.e. significant detail is being coded into BXC DDM to enable changes in traffic movements within the Dollis Hill and south of Cricklewood Lane areas to be identified (see Figure 2). It is anticipated that future year traffic flow data (with and without development traffic for phase 1 and ‘end-state’ scenarios) will be available from BXC DDM in early 2014. Discussions with TfL and others are on-going about the development of the BXC DDM model.

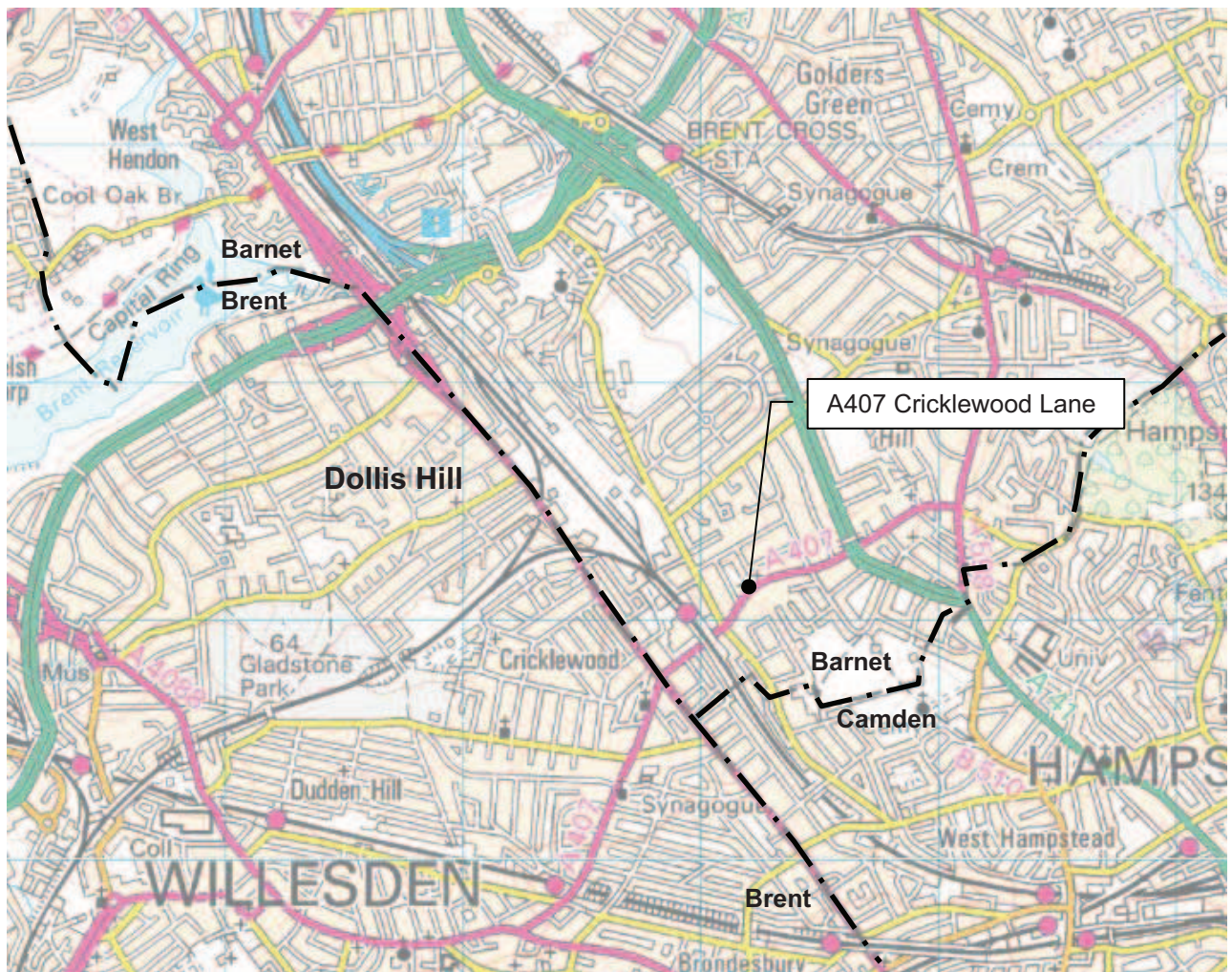


Figure 2 Area plan showing borough boundaries

As soon as data is available from BXC DDM this will be used to identify any material impacts on the local road network. The analysis will be undertaken to compare ‘V/C’ (flow to capacity ratios) from the BXC DDM Saturn modelling in the future year scenario with no development (Do Minimum) with the V/C for phase 1 and end state model (Do Something). Junctions where

'V/C' is more than or equal to 90% in the with development scenario and less than 90% in the Do Minimum will be subject to detailed capacity analysis using the appropriate junction modelling tool (i.e. TRANSYT/LinSig/PICADY/ARCADY).

Where BXC DDM identifies junctions where 'V/C' is greater than 90% in the Do Minimum (in the vicinity of the development), consideration will be given to the most appropriate package of mitigation, or as termed in the s106 agreement; 'supplementary transport measures'. Recommendations will be presented to the Transport Advisory Group where confirmation on how the transport fund should be used to progress intervention measures.

These junctions will be considered as having a '**material**' impact from the development on highway operations. The local models will be used to facilitate the preparation of outline designs of mitigation measures, which may entail revising proposed mitigation or recommending additional measures.

3.2 Stage 2 – Detailed local road assessments

Pending the outcome of the Stage 1 analysis, local static traffic models will be produced for weekday AM and PM peak hours as specified in the Section 106 agreement. Where possible, data transfusion between the BXC DDM model and more detailed local modelling will be undertaken to ensure reliable data is taken forward to the detailed design of the highway improvements. This will entail iterations between models where traffic flows and signal timing data are exchanged to refine model outputs more precisely. The process is defined more fully in the BXC DDM scoping report.

Detailed traffic modelling for the junctions identified for mitigation within Brent and Camden, will follow the process set out below:

- Model audit: undertake a review of any junction models that the local authorities may have already developed and/or model developed in the previous BXC assessments. Identify models fit for updating and where new models need to be developed.
- Surveys: develop a survey specification to enable the updating and validation of the traffic models, commission surveys and undertake analysis. The survey data from the June 2013 surveys (which was designed to principally aid the development of the BXC DDM) will provide some overlap with the requirements for the LB Brent and LB Camden Area Studies. Any additional surveys will be identified as necessary and programmed for October 2013.
- Undertake base model calibration and validation in line with the methodology for the BXC DDM for the local authorities approval prior to testing of future year scenarios
- Discuss and agree with the local authorities the phase 1 (2021) and 'end-state' (2031) development proposals and how they are to be modelled
- Code 'end-state' development proposals for future year modelling and obtain local authority approval. Undertake intervention testing and agree any appropriate supplementary mitigation measures with the authorities.
- Produce scheme designs for any additional supplementary mitigation measures that are found to be necessary, including plans at an appropriate scale, and undertake a completely independent Stage 1 Road Safety Audit compliant with TfL best practice, including the Designer's.

4 A5 VISSIM Design Model

The proposed extent of the A5 VISSIM Design Model is shown in Figure 3. The VISSIM model will be developed for weekday AM and PM peak periods and will be used to supplement appropriate junction models (TRANSYT and LinSig models for signalised junctions, PICADY for priority junctions and ARCADY for roundabouts) in the study area.

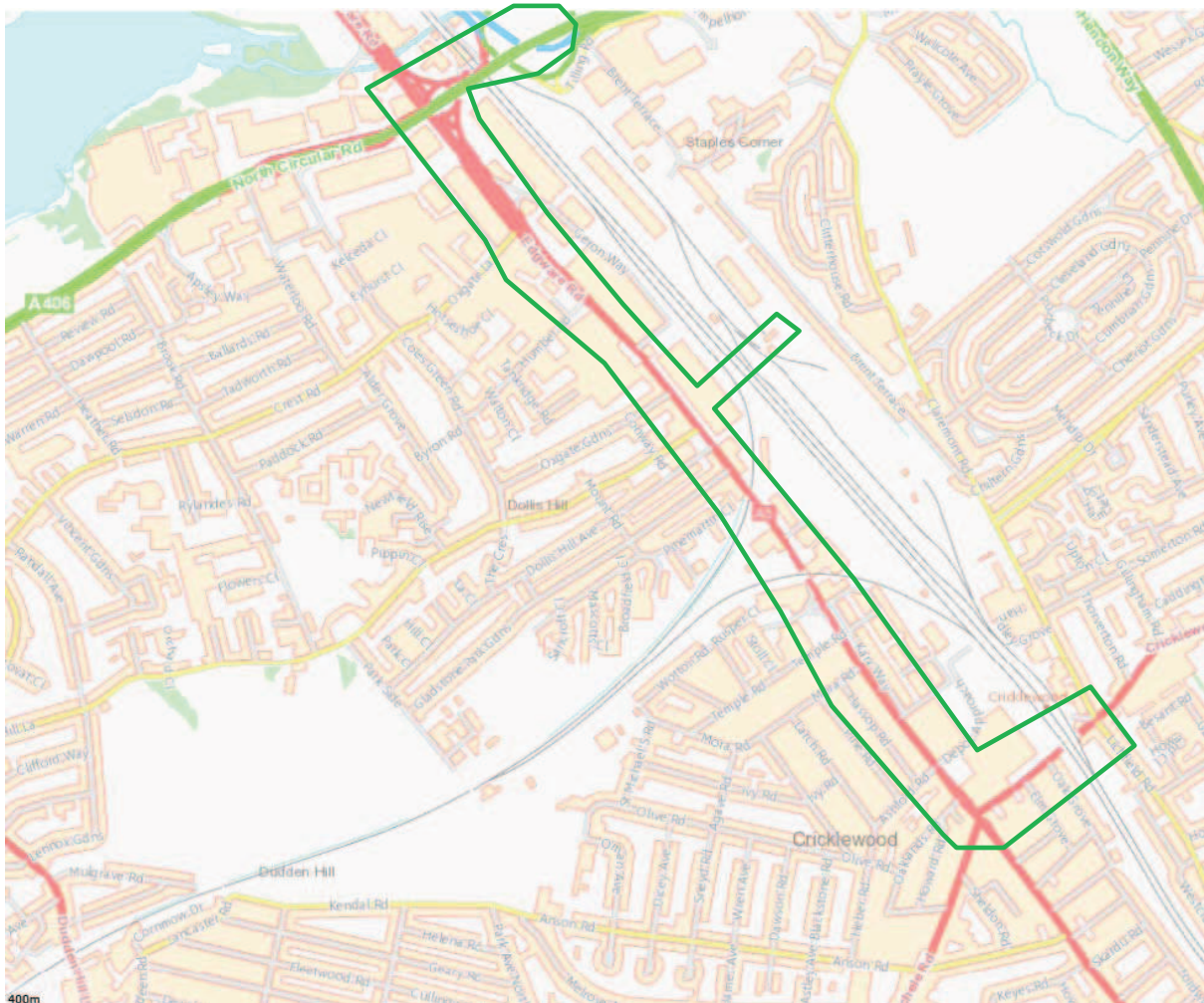


Figure 3 A5 VISSIM Design Model area

The primary objectives of the A5 VISSIM Design Model are:

- Provide a tool to assess journey times along the A5 corridor
- Advise the operational impacts of the proposed infrastructure improvements
- Advise the detailed design of the infrastructure improvements along the A5 corridor

At the time of writing this scoping report it is proposed that the development of appropriate junction models and VISSIM models will follow the outline process set out below, however it has been agreed that the nature and level of detail in the VISSIM model will be monitored and agreed through the model development process to ensure that the model and associated validation is appropriate for the proposed use of the model:

- VISSIM model audit: undertake a review of TfL’s A406 VISSIM model (see Figure 4) to ascertain whether elements of the network coding can be extracted and used for the development of the A5 VISSIM Design Model.
- Junction model audit: undertake a review of the junction models used in the previous assessments. Identify models fit for updating and where new models need to be developed
- Surveys: develop a survey specification in accordance with the data requirements defined in TfL’s modelling guidelines (this is being undertaken in collaboration with the BXC DDM scoping, with surveys being carried out in June 2013. Further specific surveys may be undertaken as part of the VISSIM modelling work as necessary)
- Undertake base model calibration and validation (following TfL’s MAP for stages 2 and 3) and seek TfL and LB Barnet approval prior to testing of future year scenarios
- Discuss with the authorities the ‘end-state’ development proposals and how they are to be modelled (TfL MAP stage 4)
- Code phase 1 and ‘end-state’ development proposals for future year modelling (following TfL’s MAP for stage 5) and seek the authorities approval, making full use of TRANSYT’s signal optimisation processes to inform VISSIM



Figure 4 The extents of TfL’s A406 Section 3 VISSIM network

The suite of traffic models along the A5 will provide the following functions:

- Inform the Phase Transport Reports and the detailed design of junctions along this corridor.
- The VISSIM model will be available to test scenarios during key construction phases and assist in identifying and testing mitigation measures. For example, the VISSIM model will be used to assess the effectiveness of any traffic management measures such as linked signals and dynamically controlled bus priority measures proposed as part of the multi-modal assessment described in section 5.1. Any proposals for scenario testing will be documented in the A5 Corridor Study report as appropriate.

5 A5 Multi-Modal Assessment

This element of the study is comprised of a number of stages, and predominately draws together the findings from other parallel studies (see section 2.1) to advise the A5 related detailed designs as follows:

1. Pedestrian accessibility and routing
2. Cycling accessibility and routing
3. A review of bus priority, including a review of all bus stops within 400m of the redline boundary along the A5 corridor, accessibility, and the forecast impact on bus journey times
4. A review of parking, loading and waiting restrictions along the A5 corridor, including any proposed changes to the existing provision and parking controls
5. Streetscape improvements, including enhancements to the public realm in relation to all modes
6. Road safety and accident analysis
7. Integration of conclusions and recommendations from all previous sections, and providing a linkage to the indicative programme for delivery.

5.1 Bus priority

A review of current and proposed bus stop locations and bus priority measures will be undertaken within 400m of the red line boundary along the A5 corridor. The following stages have been identified:

- Attend a joint inspection meeting with TfL Buses and the local authorities to understand bus priority issues, initiatives, proposals planned and/or committed.
- Base year bus infrastructure review: create an inventory of bus infrastructure, to include bus stop locations, bus stop facilities, i.e. shelter, timetable information, countdown etc., current bus priority measures e.g. selective vehicle detections at traffic signals. There is likely to be overlap here with the PERS audit that is being undertaken for the area wide walking and cycling study. A coordinated approach will be adopted to avoid any duplication of effort.
- Base year congestion analysis – a) identify ‘pinch-points’ where buses are delayed by general traffic by on-site observations and, b) review dwell time data at key bus stops (to be agreed with TfL and local authorities) and boarding and alighting data (to be provided by TfL) to identify areas for possible enhancement,
- Future year bus measures: identify what measures are proposed from the original consent and ensure these measures are included in the future year modelling assessment. Make recommendations for additional measures if warranted, such as selective vehicle detection for buses.

5.2 Parking, loading and waiting restrictions

The review of loading and waiting restrictions will be undertaken in areas along the A5 corridor (to be agreed with the authorities). Key steps will include:

- Base year assessment: map out on-street parking areas, loading and waiting restrictions.
- Analyse parking and loading survey which will be focussed on 'hot-spots' along the A5 corridor. Surveys will include arrivals, departures and dwell times by vehicle type. This information will be used to develop the base year VISSIM model.
- Review development proposals and how they might impact on parking, loading and waiting restrictions. Identify locations where restrictions should be modified to facilitate traffic movement whilst balancing the requirements of residents, local businesses etc.

A review of parking (including controlled parking zones) is being undertaken separately in the Car Parking Management Strategy report (Condition 11), outlined further in section 5.3.

5.3 Pedestrian and Cycling

The Area Wide Walking and Cycling Study will include:

- PERS and CERS audit for the A5 corridor, including public realm considerations
- A joint inspection meeting with TfL and the LB Barnet
- A review of pedestrian and cycling accessibility along the A5 corridor
- Proposed improvements to improve pedestrian and cycling accessibility
- Review of the public realm with specific reference to pedestrian and cycling facilities

Appropriate areas from the above study will be incorporated into the A5 Corridor Report to provide an overview of the pedestrian and cycling facilities along the A5 corridor.

5.4 Road Safety

A review of road safety will be undertaken as part of the S73 Transport Assessment. The area pertinent to the A5 corridor will form part of the A5 Corridor Report as appropriate.

5.5 Programme

Appendix B of this scoping document provides the outline programme for the development of the A5 corridor studies.

With regards to the parallel studies which feed into the A5 multi-modal assessment, the current programme has each report due for final issue on the following dates:

- Area wide walking and cycling study – February 2014
- BXC Detailed Design Model – Spring 2014
- Phase Parking strategy – February/March 2014
- Servicing and delivery strategy – February 2014

Sufficient time will be programmed to enable a comprehensive integration of any conclusions and recommendations made from these studies. It should be noted as each work stream progresses recommendations will be evaluated as necessary rather than as each study is finalised.

Appendix A – Annex 7 Schedule 17 from the S106

A5 Corridor Study

General Scope & Area of Study

The A5 Corridor Study will cover a core area of the A5 between A407 Cricklewood Lane and A406 Staples Corner including adjacent local roads within Barnet, Brent and Camden within an area anticipated to be 800 metres, or larger/smaller if considered necessary as part of the scoping, from the A5, the MML link, the Claremont Road corridor and the A407 Cricklewood Lane to include a study of road safety, cycle provision, pedestrian environment, bus priority, traffic control, freight and delivery, servicing and on-street parking strategy and management.

Table 1: Detailed Scope of A5 Corridor Study

Corridor Element	Aim	Current Guidance and relevant studies (or substitute with update guidance)	Types of Measures (illustrative/ examples)
Traffic Modelling – 2026 AM and PM peaks	(a) Traffic modelling to inform the Phase Transport Reports and the detailed design of the junctions along the A5 Corridor. (b) Production of traffic forecasts that enable fuller understanding of impacts and testing of local traffic management measures and/or supplementary mitigation in LB Brent	TfL modelling guidance, existing modelling work undertaken for the TA	(a) Development of a Vissim micro-simulation model as described below. (b) Refinement of TA strategic modelling work as described below, e.g. zonal disaggregation,,review of trip generation and distribution from key sites e.g, WHF.
Pedestrian Environmental Review / Pedestrian Routes	Create pedestrian network between the development and adjacent communities / key attractors	TRL's Pedestrian Environment Review System (PERS) as developed for TfL; TfL Streetscape Guidance	Links, footway design, pedestrian crossings, subways/bridges, routes/route choices, public spaces, interchanges, bus stops, signage and way finding

Pedestrian Accessibility	Make the above network fully accessible ³	Panel advice/ other relevant advice from the community. TfL guidance on providing for disabled people; Design Manual for Roads and Bridges	Legibility, level changes, car parking location/ design, taxi location and design, design/location of crossings and bus stops, design of interchanges (including step free access)
Traffic Management - Parking, loading and waiting review	Improve movement capacity and smooth traffic flows	Relevant TfL or Borough Guidance	Waiting/loading restrictions along corridors and parking controls in adjacent roads
Cycle Routes / cycle Audit	Create cycle network between the development and adjacent communities / key attractors	London Cycling Action Plan; Cycle Route Implementation Study Process (CRISP) studies; London Cycle Design Standards; LCN+ guidance	Cycle routes/ cycle lanes, all public highway/ footbridge designed for cyclists, Advanced Stop Lines/ Toucan crossings

Corridor Element	Aim	Current Guidance and relevant studies (or substitute with update guidance)	Types of Measures (illustrative/ examples)
Traffic Management - Review of signals/ linked signals/traffic management	Improve movement capacity and smooth traffic flows	TfL modelling Guidelines; TfL signal design standards	Signal equipment, re-timing/ staging, layout changes (kerb-re-alignment, crossings, lining, signing), lighting, linked signals.
Review of bus priority including all bus stops within 400 metres of the redline boundary	Improve movement capacity and smooth traffic flows. Improve conditions for bus users	London Buses advice on bus stops and bus priority	Selective Vehicle Detection, Bus SCOOT (or similar signal control), review waiting/ loading, location of crossings, Bus stops, lighting.
Public realm ⁴	Enhance the highway environment in the corridor	TfL Streetscape Guidance	Soft and hard landscaping, including trees; lighting, drainage and public realm
Road safety/ accident data review	Improve road safety as part of the traffic management proposals	TfL guidance on road safety schemes; Design Manual for Roads and Bridges	Relates to design of the above elements including minimising safety concerns during construction.

³ The study will inform the detailed design at the junctions and the wider connectivity at the junctions which will form part of the mitigation funded by the DPs.

⁴ The DPs will carry out the works identified in the Public Realm and Open Space Parameter Plan 003 and the associated landscaping to the new junctions. Any works identified beyond this will be funded if appropriate from the consolidated Transport fund, subject to a decision of the Transport Strategy Group.

Modelling for the A5 Corridor Study

A Vissim model will be developed to inform the A5 Corridor Study. It is proposed that a Vissim micro-simulation model is formed based on an area element of the wider A5 Corridor Study around the A5 between the section of this road between A5/A406 North Circular Road and A5/A407 Cricklewood Lane junctions, also extending to include A407/Claremont Road and A5/MML Bridge to be agreed as part of the scoping. The extent of the area to be covered by the Vissim model will be agreed with the Authorities before the time when the design work on the A5 corridor is to be undertaken.

The aim will be to cordon out the A5 area from the strategic BXC model and then introduce the modelling of more localised junctions and movements into the analyses so that the impacts of these movements can also be assessed in the detailed junction assessments, and hence detailed designs.

This will also enable the operational impacts of adjacent developments on the corridor to be further assessed. The traffic demand would be taken from the BXC strategic SATURN model.

All major and minor junctions on this section of the A5 would be included in the simulation model. This would require full classified turning movement surveys to be undertaken at each of these junctions. The Vissim model will be able to assess the linking of traffic signals

The trip generation and distribution assumptions made for the ParcelForce site opposite Dollis Hill Lane will be reviewed for the appropriate form of landuse being proposed at the time the study is conducted.

The network will be taken from the SATURN model and enhanced with the introduction of local roads to get a suitable level of local detail. In this way the zonal detail would be increased so that local movements will be further represented. Matrix estimation will then be used to control the demand to the locally observed movement totals. The model would then require a local validation, which would need some further counts on adjacent links and/or junctions.

A journey time survey on the A5 would also be undertaken to further inform the calibration and validation processes.

Forecasting with the model will be done by forecasting the relative change in demand in the A5 corridor using the BXC SATURN model forecasts, including the use of the junction adjustments as included in the TA assessments. These would be applied in a relative fashion to the local model matrix, which would allow local forecasts to be run. After that the model could be used to test the various mitigation measures.

It is proposed that this work is undertaken for the AM and PM peak periods, for development scenarios to be agreed with LB Barnet and TfL, and in consultation with LBs of Brent and Camden.

Local Traffic Management Measures in Brent

The existing strategic BXC transport model will be used, where necessary, with minor modifications, to further assess any local traffic management measures proposed in LB Brent. The existing BXC strategic transport model will be used to further test any wider area implications that might arise from future local traffic management measures that are proposed to be introduced in LB Brent to address any supplementary/unforeseen impacts from the BXC proposals. Any changes to the strategic model, would be minor and targeted to provide a better representation of the local zonal structure and network, whilst still retaining the forecast demand flows from the current matrices. In this manner, the fundamental traffic assumptions inherent within the TA will be retained.

Specifically the points would include:

- The adjustments that are currently applied in the junction assessment process as reported in the TA ensure that a robust assessment of the junctions has been undertaken. However, to refine the loading of trips from east and west of the A5 it is proposed to further disaggregate the zones, and reassess the zone centroid connections, for those zones that bisect the A5, namely zones 9201, 9203 and 9204.
- To also disaggregate zone 9209 which bisects the A406 North Circular Road at Neasden, and to review its zone centroid connections, to further reflect its loading onto the network east and west of the A406.
- To review and, where appropriate, modify the northern and southern junctions and links within the strategic model network definition at the A5 and Neasden Lane/Dudden Hill Lane for the Dollis Hill Area as defined by the boundary of the A406/ A5/ Cricklewood Freight Line and Neasden Lane/ Dudden Hill Lane.

The extent of the area to be covered by this modeling will be agreed with the Authorities at the appropriate time. Modelling will be undertaken for the AM and PM peak hours.

These enhancements could be introduced in a manner that would also inform the development of the Vissim model as described above.

As a consequence of these tests, additional/ supplementary mitigation measures identified as being required will be the subject of detailed design, costing and public consultation and programmed for implementation at the appropriate time according to development phasing and impact. The associated costs of detailed design, costing, public consultation and implementation will be at the Development Partners' expense

APPENDIX 2

Analysis of Scheme Compliance with Regional and Local Planning Policy

Table 2.1: Analysis of the proposals compliance with London Plan (March 2015) Policies

Policy	Content Summary	Extent of compliance and comment
Policy 1.1 (Delivering the strategic vision and objectives for London)	Strategic vision and objectives for London including managing growth and change in order to realise sustainable development and ensuring all Londoners are able to enjoy a good and improving quality of life. Improving environments which are easy, safe and convenient for everyone to access.	Compliant: As a London Plan Opportunity Area, the approved BXC scheme seeks to make the most of brownfield land to meet wider growth requirements in terms of housing, retail and commercial activities in a location accessible by a range of transport modes. The provision of the proposed infrastructure will assist in achieving the wider sustainable aims of the London Plan and provide easy, safe and convenient access for all.
Policy 2.6 (Outer London: vision and strategy)	Work to realise the full potential of outer London and enhance the quality of life for present and future residents. Understand the significant difference in the nature and quality of neighbourhoods; improvement initiatives should address these sensitively and draw upon strategic support where necessary.	Compliant: The proposed development represents one of the most important opportunity areas in outer London. The BXC Opportunity Area represents a significantly underutilised area of accessible brownfield land in need of regeneration. More specifically, in relation to the current application, the infrastructure will be enhanced for visitors to the Brent Cross Shopping Centre and the Southern development. It will be an asset for the public and existing neighbourhoods.
Policy 2.8 (Outer London: Transport)	Enhance accessibility by improving links to and between town centres and other key locations by different modes and promoting and realising the key improvements. Work to improve public transport access, provide improved traffic management, road improvements and address and manage local congestion	Compliant: The study proposes a package of improvements designed to achieve better efficiency of traffic movements along the A5. This includes several junction improvements and other traffic management measures to help alleviate congestion and provide an appropriate level of mitigation.
Policy 2.13 (Opportunity Areas and Intensification Areas)	Support the strategic policy directions for the opportunity areas, and where relevant, in adopted opportunity area planning frameworks Support wider regeneration, including in particular improvements to environmental quality, and integrate development proposals to the surrounding areas especially for regeneration.	Compliant: Nearly all the new phase 1A North roads and junctions are being designed with enough capacity to accommodate the predicted traffic levels for the whole development at end-state. The new infrastructure is designed to support the wider Brent Cross regeneration. The A5 Corridor Study; Condition 2.7 is a pre-reserved matters condition of the s73 permission and the study identified that there would be additional impacts on local roads in Brent and Camden; to improve these conditions a capped contribution has been secured towards future Supplementary Transport Measures if monitoring of traffic flows indicates increases due to the development.

Policy	Content Summary	Extent of compliance and comment
Policy 2.14 (Areas for regeneration)	Boroughs should identify spatial areas for regeneration and spatial policies to bring together regeneration.	<p>Compliant: The BXC site is identified as a London Plan Opportunity Area (See Policy 1.1 above) in need of comprehensive regeneration and capable of accommodating significant housing, jobs and community infrastructure.</p> <p>The detailed layout and network for vehicles, cycles and pedestrians that provide connections to the Northern development and provides sufficient connections across and to the south of the A406 to facilitate the start of the Southern development.</p> <p>The A5 Corridor will provide key traffic improvements and improve traffic movements along the A5.</p>
Policy 2.15 (Town Centres)	Development proposal should promote access by public transport walking and cycling. Promote safety and security and contribute towards an enhanced public realm and links to green infrastructure.	<p>Compliant: The regeneration development improves public transport by providing replacement of the existing bus station with a fully integrated new high quality facility within the extended Brent Cross Shopping Centre, improvements to pedestrian and cycle facilities; provides a new pedestrian and cycle bridge, known as the Living Bridge which will provide direct access from the southern development to the shopping centre situated to the north. Additionally, provide key connected pedestrian and cycle routes via Sturgess Park, Claremont Park and Clitterhouse Playing Fields.</p>
Policy 6.1 (Strategic approach);	The Mayor will work with all relevant partners to encourage a high quality public realm where appropriate, a corridor-based approach should be taken to ensure the needs of street users and improvements to the public realm are co-ordinated.	<p>Compliant: The regeneration scheme will see the provision of a network of connecting different green spaces and will primarily allow access to/from the Living Bridge which connects the southern development via the market square to Clitterhouse Playing Fields and Claremont Park; and access to the Brent Cross shopping Centre. The A5 Corridor will form part of this network and improve the needs for street users.</p>
Policy 6.2 (Providing public transport capacity and safeguarding land for transport)	Development proposal should improve integration, quality, accessibility, frequency and environmental performance of the public transport system.	<p>Compliant: The study has reviewed the mitigation measures approved as part of the outline scheme. The existing bus lanes which are all being retained and are considered to provide an effective package of improvements; to support bus operations along the corridor. Accessible bus stop improvements will be introduced as part of the Section 106 Consolidated Transport Fund.</p>

Policy	Content Summary	Extent of compliance and comment
Policy 6.3 (Assessing effect of development of transport capacity)	Development proposals should ensure that impacts on transport capacity and the transport network, at both a corridor and local level are fully assessed. Transport assessments will be required in accordance with TfL's Transport Assessment Best Practice Guidance for major planning applications.	Compliant: The A5 Corridor Study has assessed the impacts of the development in the study area using the latest BXCDDM. A micro-simulation model for 2021 has also been produced to look at detailed traffic capacity issues. The scope for the study was included in draft in the Section 106, and the latest version has been agreed and approved in close consultation with TfL
Policy 6.4 (Enhancing London's Transport connectivity)	Proposals illustrate opportunities related to locations which will benefit from increased public transport accessibility. Improve the public transport system to support future development and regeneration.	Compliant: To enhance and increase public transport accessibility, the new A5 link bridge over the Midland Mainline railway includes bus lanes in both directions. There will be a new connection for existing and modified bus services to connect with the southern development and the proposed new train station. This is expected to substantially improve the public transport accessibility in the local area
Policy 6.7 (Better streets and surface transport)	Development proposals should promote bus networks; allocating road space and providing high level priority on existing and proposed routes. Ensuring good access to and within areas served by networks, now and in future; and ensuring direct, secure, accessible and pleasant walking routes to stops.	Compliant: The proposed mitigation arising out of the study includes improvements to routes that are used to access bus stops. The A5 Corridor study complies with this policy.
Policy 6.9 (Cycling)	Proposals should identify and implement a network of cycle routes. Contribute positively to an integrated cycling network for London by providing infrastructure that is safe, comfortable, attractive, coherent, direct and adaptable and in line with the guidance set out in the London Cycle Design Standards (or subsequent revisions).	Compliant: The A5 corridor study has identified a network of cycle routes and various improvements to cycle facilities will be provided along the A5 corridor. It is considered the study complies with this policy.
Policy 6.10 (Walking)	Development proposals should ensure high quality pedestrian environments and emphasise the quality of the pedestrian and street space by referring to Transport for London's Pedestrian Design Guidance. Promote the 'Legible London' programme to improve pedestrian way finding. Encourage the use of shared space principles, such as simplified streetscape, de-cluttering and access for all.	Compliant: To improve pedestrian and way finding, signing throughout the A5 corridor will be provided in accordance with the guidelines set out in Legible London, and contained in the Way finding and Inclusive Access Strategy.

Policy	Content Summary	Extent of compliance and comment
Policy 6.12 (Road network capacity)	Proposals should improve the road network by improving or extending existing capacity or providing new links. Assess the extent of any additional traffic and any effects it may have on the locality. Proposals should also focus on how conditions for pedestrians, cyclists, public transport users and local residents can be improved.	Compliant: The study includes the road improvements approved as part of the outline Planning Permission, including the proposed A5 link road over the railway. Area wide modelling of traffic movements using the BXCDDM has been undertaken, and all key junctions modelled. A micro-simulation model of the corridor has also been developed. Traffic impacts are therefore considered to have been fully assessed. The study has also proposed a package of various improvements for non-car users.

Table 2.2: Analysis of the proposals compliance with Barnet's Local Plan Policies (September 2012)

Policy	Content Summary	Extent of Compliance and Comment
Core Strategy		
CS NPPF (National Planning Policy Framework – presumption in favour of sustainable development)	Take a positive approach to proposals which reflect the presumption in favour of sustainable development and approve applications that accord with the Local Plan, unless material considerations indicate otherwise. Where there are no policies relevant to the proposal or the relevant policies are out of date permission should be granted, unless material considerations indicate otherwise.	Compliant: The study is considered to be in accordance with the NPPF and complies with Local Plan taken as a whole. It is therefore recommended for approval.
CS1 (Barnet's place shaping strategy – the three strands approach)	As part of its 'Three Strands Approach' the council will: <ul style="list-style-type: none"> - Concentrate and consolidate growth in well located areas that provide opportunities for development, creating a high quality environment that will have positive impacts. - Focus major growth in the most suitable locations and ensure that this delivers sustainable development, while continuing to conserve and enhance the distinctiveness of Barnet as a place to live, work and visit. - Ensure that development funds infrastructure through Section 106 Agreements and other 	Compliant: the proposal is considered to show the influence of this policy and demonstrates compliance with its key objectives. As an Opportunity Area in the Mayor's London Plan, the BXC scheme has been developed with the consideration that the site has significant capacity for new housing, commercial and other development linked to existing or potential improvements to public transport accessibility. The proposed development relates to matters reserved following the grant of planning permission in 2014. The majority of the infrastructure improvements associated with the scheme will be delivered during Phase 1 in preparation for the proposed development due to come forwards during the subsequent phases. The network created in sub-phase 1A North provides all necessary connections to facilitate the rest of the northern development and

Policy	Content Summary	Extent of Compliance and Comment
	<p>funding mechanisms.</p> <ul style="list-style-type: none"> - Protect and enhance Barnet's high quality suburbs. 	<p>in addition, some of the key roads south of the A406. Key elements of the road network for the Southern development are provided.</p>
<p>CS2 – Brent Cross - Cricklewood</p>	<p>The Council will seek comprehensive redevelopment of Brent Cross – Cricklewood in accordance with the London Plan, the saved UDP policies (Chapter 12) and the adopted Development Framework. The Policy makes provision for the following:</p> <ul style="list-style-type: none"> • It is considered likely that comprehensive regeneration will be achieved in accordance with the planning permission. If this is not achieved, the Council will consider whether in the circumstances the Local Plan needs to be reviewed. • Specific monitoring indicators for Brent Cross – Cricklewood are set out in Appendix B of the Core Strategy On the basis of these indicators it is expected that comprehensive re-development will commence in relation to Phase 1 at some time between 2015 and 2017. • If these milestones are not achieved (or are not likely to be capable of being delivered) we will consider the possible need for a review of the Core Strategy Policy on Brent Cross – Cricklewood • The key milestone for the regeneration of Brent Cross – Cricklewood is likely to be the Phase 1 Compulsory Purchase Order (CPO). If by the end of 2014 any CPO that is required to deliver Phase 1 and commence the development has not been made and submitted for confirmation we will instigate a review of the policy framework for Brent Cross – Cricklewood. 	<p>Compliant: The 2014 consent of the S73 application continues to ensure the comprehensive redevelopment of Brent Cross Cricklewood in accordance with this policy and the Saved UDP policies.</p> <p>The infrastructure will be enhanced for visitors to the Brent Cross Shopping Centre and the Southern development. It will be an asset for the public and existing neighbourhoods. The new infrastructure will support the wider Brent Cross regeneration.</p>
<p>CS9 (Providing safe, efficient and effective travel)</p>	<p>Developments should provide and allow for safe effective and efficient travel and include measures to make more efficient use of the local road network.</p> <p>Major proposals should incorporate</p>	<p>Part Compliant: The study includes a review of several of the key gateway junction improvements and the new A5 link road across the railway line. A package of proposals has arisen out of the study designed to improve conditions for non-car users.</p>

Policy	Content Summary	Extent of Compliance and Comment
	<p>Transport Assessments, Travel Plans, Delivery and Servicing Plans and mitigation measures and ensure that adequate capacity and high quality safe transport facilities are delivered in line with demand.</p> <p>The council will support more environmentally friendly transport networks, including the use of low emission vehicles (including electric cars), encouraging mixed use development and seeking to make cycling and walking more attractive for leisure, health and short trips.</p>	
CS12 (Making Barnet a safer place)	<p>The Council will:</p> <ul style="list-style-type: none"> - Encourage appropriate security and community safety measures in developments and the transport network. - Require developers to demonstrate that they have incorporated community safety and security design principles in new development. - Promote safer streets and public areas, including open spaces. 	<p>Compliant: a lighting strategy is proposed by the development partners. Although indicative details have been submitted with the current application full design details are still required and will be subject to the requirements of a planning condition. The lighting strategy will ensure the environment is secure.</p> <p>In addition, the Estate Management Framework Agreement advises that a team of SIA (Security Industry Association) accredited CCTV operators (closed circuit television) will operate an extensive system within Brent Cross shopping centre 24 hours of every day supporting the Duty Assistant team in providing a safe and secure environment.</p>
CS15 (Delivering the Core Strategy)	<p>The council will work with partners to deliver the vision, objectives and policies of the Core Strategy, including working with developers and using planning obligations (and other funding mechanism where appropriate) to support the delivery of infrastructure, facilities and services to meet needs generated by development and mitigate the impact of development.</p>	<p>Compliant: The Section 73 permission was subject to a S106 Agreement. The A5 Corridor Study condition 2.7 is a pre-reserved matters condition of the S73 permission. The requirements of the s106 are still applicable. Where required, the s106 will be varied in order to secure deliver of specific elements within the current application.</p>
<p>Adopted UDP (May 2006) Saved Policies (May 2009) as referred to in Core Strategy Policy CS2 – Chapter 12 of the UDP: Cricklewood, Brent Cross and West Hendon Regeneration Area</p>		
GCrick Cricklewood, Brent Cross and West Hendon Regeneration Area	<ul style="list-style-type: none"> - The Council seeks integrated regeneration in the Cricklewood, Brent Cross and West Hendon Regeneration Area. - All development to the highest environmental and design standards - Aim to develop a new town centre 	<p>Compliant: The A5 Corridor is consistent with this strategic aim</p>

Policy	Content Summary	Extent of Compliance and Comment
	over the plan period.	
C1 Comprehensive Development	<ul style="list-style-type: none"> - The Council seeks the comprehensive development of the regeneration area in accordance with the area framework and delivery strategy. - Development proposals will need to meet policies of the UDP and their more detailed elaboration in the development framework. 	Compliant: The regeneration scheme accords with this policy. The proposals are Phase 1A (N) proposals within the scope of the S73 permission, the first phase of development within the wider Brent Cross Cricklewood regeneration project.
C7 Transport Improvements	<p>Transport improvements - the following should be provided through planning conditions and/or Section 106 agreements:</p> <ul style="list-style-type: none"> i. Connections and improvements to the strategic road network. ii. Sufficient transport links to and through the development, to include at least one vehicular link across the North circular Road and one vehicular link crossing the railway to the Edgware Road. iii. A new integrated railway station and new integrated bus station at Cricklewood, linked by a rapid transport system to Brent Cross Bus Station and Hendon Central and/or Brent Cross Underground Stations on the Northern Line. iv. A new bus station at Brent Cross, to north of the North Circular Road, with associated improvements to the local bus infrastructure. v. An upgrade of the rail freight facilities. vi. Provision of an enhanced, rail-linked waste transfer station serving North London. vii. Priority measures for access to disabled persons, pedestrians, buses and cyclists throughout the Regeneration Area. 	Part Compliant: The study includes a review of several of the key gateway junction improvements and the new A5 link road across the railway line. A package of proposals has arisen out of the study designed to improve conditions for non-car users.
UDP Site Specific Proposals (2006)	<p>Parts of the BXC development site are subject to site-specific proposals as shown in the Proposals Map (2006) (as saved). The aspirations for these sites are set out below:</p> <ul style="list-style-type: none"> • Brent Cross New Town Centre (Site 31) – new town centre, comprising a mix of appropriate 	Compliant: the regeneration proposal is consistent with the Proposals Map.

Policy	Content Summary	Extent of Compliance and Comment
	<p>uses, improved public transport and pedestrian access, landscaping and diversion of the River Brent;</p> <ul style="list-style-type: none"> • Cricklewood Eastern Lands (Site 37) – mixed use including residential, office, leisure, local and neighbourhood shops, education, community uses and open space; • New Railway Station Cricklewood (Site 38) – railway station and public transport interchange; • New Waste Transfer Station (Site 39) – waste handling facility. 	

Cricklewood, Brent Cross, West Hendon Regeneration Area Development Framework SPD 2005.

The Cricklewood, Brent Cross and West Hendon Regeneration Area Development Framework was adopted by the council and the Mayor of London as Supplementary Planning Guidance in 2005. This Development Framework was produced in collaboration with the Mayor and the Greater London Authority, landowners and developers in order to guide and inform the design and delivery of the development with the aim of achieving high quality comprehensive redevelopment of the area around a new sustainable mixed use town centre spanning the North Circular Road.

The London Plan and the UDP saved policies combined with the Development Framework establishes a series of strategic principles for the comprehensive redevelopment of the area to create a new town centre, the overall vision for which is set out in UDP Policy GCrick.

Compliant: The A5 Corridor study is considered to be in accordance with the principles set out in the guidance contained in the adopted Development Framework (2005).

Key relevant local and strategic supplementary planning documents

Local Supplementary Planning Documents and Guidance:

- Infrastructure Delivery Plan (2011)
- Sustainable Design and Construction (2013)
- Planning Obligations (Section 106) (April 2013)

APPENDIX 3

CONSULTATION RESPONSES

First Consultation Responses for Consultation Period Ending 18/12/2014

Resident Response	Officer Comments
No objections or comments were received by residents during this initial consultation period	

Second Consultation Responses for Consultation Period Ending 03/08/2015

Resident Responses:	Officer Comments
<p><u>Resident 1 Response:</u></p> <p>To improve the traffic impacts in the area a priority bus route across the railway and running West to East through the middle of the development needs to be created. The road should be situated half way between the North Circular and Cricklewood Lane; this would create a more practical and circumferential route.</p> <p>The submitted documents states there is no space or capacity in the area for more road traffic and congestion is probably limiting traffic growth; the developers should think outside the box and consider other options to increase the comfortable movement and invest in long term needs.</p>	<p>Two Reserved Matters Applications and the A5 Corridor Study condition 2.7 of the Section 73 approved application (14/07402/CON) are before this Planning Committee for consideration; and this particular submission specially relates to the A5 Corridor Study; Condition 2.7. Whilst the objection letter refers to both the Infrastructure RMA and the A5 Corridor Study condition 2.7 planning references, officer comments can be found under Appendix 6 in the committee report for the Infrastructure RMA (15/03315/RMA).</p>

<p><u>Resident 2 Response:</u></p> <p>The Cricklewood part of the Edgware Road is already heavily congested and will be unable to cope with the massive increase in traffic generated by the proposed development. The results estimated does not give confidence that the A5 will be able to deal with the proposed changed in traffic.</p>	<p>The A5 Corridor Study recognises that congestion on the network is a problem. Where practicable, the aim has been to both protect buses from congestion, and encourage walking and cycling through positive design measures. The increase in traffic flow and change in journey time on the A5 corridor has been quantified and improvements are proposed at the traffic signal junctions on Edgware Road between Staples Corner and the A5/A407 junction to help mitigate the impact of the development</p>
<p><u>Resident 3 Response:</u></p> <p>The roads currently are already heavily congested and at certain times of the day the traffic is standstill causing dangerous levels of pollution. Any development designed will increase the pollution or attract higher number of visitors to Brent Cross; which will have a negative impact to the area.</p>	<p>Two Reserved Matters Applications and the A5 Corridor Study condition 2.7 of the Section 73 approved application (14/07402/CON) are before this Planning Committee for consideration; and this particular submission specially relates to the A5 Corridor Study; Condition 2.7. Whilst the objection letter refers to both the Infrastructure RMA and the A5 Corridor Study condition 2.7 planning references, officer comments can be found under Appendix 6 in the committee report for the Infrastructure RMA (15/03315/RMA).</p>
<p><u>Resident 4 Response:</u></p> <p>The Junctions at Claremont, Cricklewood Lane and Lichfield Road are currently very busy and difficult to navigate. When this junction gets busy it impacts the junction of Chichele Road, Cricklewood Broadway and Cricklewood Lane. Are there any proposals to improve these junctions?</p>	<p>These junctions are both proposed to be improved as part of phase 1A North. The schemes themselves have already received planning approval as they are 2 of the key gateway junctions to the BXC development, approved as part of the outline permission.</p>

Resident 5 Response:

There is no clarity whether the traffic impacts of the surrounding regeneration developments (West Hendon and A2 Dominion) have been taken into consideration.

Existing traffic saturations have not been taken into consideration. Only the calculated baseline traffic; presuming with all the surrounding developments has been predicted. A comparison of queue lengths and journey times is essential.

The visual report does not correlate with actual experiences of travelling on the A5; including significant delays to buses in the PM peak.

We are not certain where Kara road is where there is unused cycle provision.

Increases of 2-3% in saturation are considered insignificant, but such an increase on a road 95% saturated increases the risk of melt down by 100%. The congestion on the network will impact the buses, and these journey times need improving.

Traffic from committed developments i.e. those with full planning consent in the study area have been included within the modelling based on their predicted trip generation at the point of planning permission. The London Transport Studies model also includes background growth of the existing traffic (which takes into account potential development without planning permission) and the predicted traffic from the proposed development.

The traffic model has been validated by comparing the traffic flows and journey times calculated by the model with traffic flows and journey times recorded on street. This process was fully scrutinised by TfL and Barnet. Queue lengths actually provide a fairly unreliable way of assessing the performance of a network as they are constantly changing so the analysis looks at the ability of junctions to allow the demand traffic to pass through the junction, and this is the best indicator of congestion. Separate analysis has been undertaken to make sure that the queues are unlikely to block back to upstream junctions.

As part of the stage 3 base year validation process, the AM, PM and Saturday VISSIM models were validated in accordance with TfL VMAP validation criteria, which modelled journey times for 85% or more of the routes were within 15% or 60 seconds of observed data. The model successfully demonstrated this at VMAP stage. Bus journey times form part of the on-going monitoring.

This should read Kara Way. The developer is only required to provide mitigation of impacts of the development; this does not require them to solve existing traffic issues within Barnet or the adjacent boroughs.

These small predicted increases are regarded as insignificant because the forecasting methods are not accurate enough for these very small changes to be reliable. Where the predicted changes are so small it is just as likely that there will actually be a reduction in traffic flow at these junctions.

Junctions in the vicinity of the scheme where the flow to capacity ratio is greater or equal to 90% in the 'with development' scenario and less than 90% in the 'without

<p>The mitigations included in the outline approval seem to be renegotiable. The A5 Corridor study submitted states that there is some unresolved traffic jam which will have to be resolved at Barnet Councils expense. Can you clarify this?</p> <p>It is proposed the New Railway Station was a planning gain and would only be built when the developer have sufficient revenue to justify. It is not proposed to bring new station works forward, which will be funded by the Central Government and not the developer.</p> <p>What is the turnaround for each bus stop and how many buses need to stop at each location at one time? This information has not been provided and will this be available?</p> <p>There is no drop off proposed for Cricklewood station and why has this been excluded? The outline application stated that there would be step free access for Cricklewood Station; has this been withdrawn?</p> <p>Widths of pavements are a concern particularly if the bus stops are to cope with huge crowds of people</p>	<p>development' are identified as having a material impact and have then been reviewed in detail. Journey times of buses with and without the development have been assessed in the VISSIM micro simulation model.</p> <p>The Developer is only responsible for mitigating all impacts arising from the BXC Development, transport networks will operate in the same manner as that which would have occurred had the Development not been provided.</p> <p>The developer is contributing funding to improve capacity at junctions which exceed 90% due to the proposed development.</p> <p>Works regarding the railway station is not part of this A5 Corridor study planning application.</p> <p>A review of the bus services formed part of the Transport Assessment (BXC05) for the original consent and the S106 Agreement includes provision of a substantial bus subsidy for TfL to use to provide the additional bus services required to satisfy the forecast passenger demand on each existing and new routes. As stated beforehand, Bus journey times form part of the on-going monitoring.</p> <p>Works to Cricklewood Station are not part of this Phase 1a North RMA submission, and will be established in a future phase.</p> <p>There are bus stops immediately adjacent to Cricklewood Railway Station which serve the Scheduled Bus Services. These bus services were found by TfL to provide the necessary capacity and frequency to satisfy all forecast demand for passengers wishing to transfer to the rail mode at this station.</p>
--	---

<p>Servicing and delivery has not been adequately solved in this application.</p> <p>Housing still being shown on the plans in front of B&Q where roadside servicing is indicated in the outline approval.</p> <p>The A5 Corridor study should have outlined how traffic would be monitored through the development programme; to confirm the level of modal shift that would occur and how it would be adjusted. These aspects have not been taken into consideration.</p>	<p>Servicing and delivery for the proposed development is taken into account via a separate Framework Servicing and Delivery Strategy report prepared in accordance to condition 1.21.</p> <p>This proposal does not form part of Phase 1A North nor part of this current applicant. It will be considered at a later date when this plot comes forward in a future phase.</p> <p>There is a requirement under Condition 37.8 for a separate Monitoring Strategy Report which must be submitted and approved prior to commencement of any part of the Development.</p>
---	--

Other Interest Groups Consultation Responses

	Officer Comments
<p><u>Interest Group 1:</u></p> <p>Concerns have been raised that the documents included in the current application for the A5 Corridor Study is not the same study as what was approved by the Planning Committee in November 2009</p> <p>Residents have raised concerns that the 1996 Brent Cross Planning application was rejected by the High Court in 2002; and as a result the London Borough of Barnet has encouraged a private-sector Brent Cross Master plan and there has been no community involvement.</p> <p>Have the 'Eastern Lands' been added, and the incinerator site been moved slightly further away from the Railway Cottages?</p> <p>Earlier designs illustrated the two Brent Terrace Triangles as green open spaces. By the time of the 'Brent Cross Supplementary Planning Guidance' (SPG) was prepared, the two triangles had designed to be residential land. Was the SPG written in order to meet the requirements of the Master plan?</p> <p>Attempts were made by members of the public to introduce wider transport issues at the time of the SPG, but they were rejected by the authority.</p>	<p>The A5 Corridor study is a Pre RMA condition attached the Section 73 approval, and was prepare after this planning approval. This Study informs whether there are any forecast traffic impacts that are significant on the local roads. The developer has committed to fund any necessary supplementary measures to mitigate any such significant local traffic impacts.</p> <p>This concern is noted and is not relevant to this current Planning Application.</p> <p>This proposal does not form part of Phase 1A North nor part of this current applicant. It will be considered at a later date when this plot comes forward in a future phase.</p> <p>This concern is noted and is not relevant to this current Planning Application. Please note that the Brent Terrace Triangles Plots 53 & 54 has already been determined.</p> <p>This concern is noted and is not relevant to this current Planning Application.</p>

<p>Public consultation conducted in 2007 and onwards by developers in “caravans” was run solely by staff from a PR company with no planning knowledge.</p> <p>In the November 2009 planning committee it was stated that Brent Council had no objections to the Planning Application.</p> <p>Why was the A5 Corridor study only a 14 day consultation period?</p> <p>Could you supply an audit of the A5 Corridor Study from 2009 to date?</p>	<p>Please see above</p> <p>Brent Council did raise objections and these were recorded in the committee report presented at the 18 and 19 November 2009 Planning and Environment Committee.</p> <p>Consultation time periods comply with the statutory requirements. Please see the main report for further details.</p> <p>The A5 Corridor Study Report satisfies the requirements of the S106 Agreement and has been developed in consultation with the London Borough of Barnet and Transport for London and based on the agreed ‘Scope of Application documents for the A5 Corridor Study’ Revision 06, dated October 2013 (doc No. 47065005/TP/RPT/009). The study has gone through the following iterations in its formulation; to ensure that officers from the London Borough of Barnet and Transport for London are satisfied that the study meets the condition imposed:</p> <p>Revision 1: 10th October 2014 Revision 2: 5th November 2014 Revision 3: 2nd April 2015 Revision 4: 24th April 2015 Revision 5: 7th May 2015 Revision 6: 26th June 2015 Revision 7: 17th July 2015.</p>
--	--

Statutory Bodies and Neighbouring Boroughs Consultation Responses

Highways England

Email from Stephen Hall, Asset Manager, Highways England dated 28th July 2015.

No objection to the proposals.

TfL

Letter dated 19th January 2015

Made various comments in relation to modelling, transport improvements, bus journey time assessments, VISSIM, bus priority measures, bus service delays, loading and kerbside parking, urban realm, cycle measures. They concluded at that time that the A5 Corridor study is not to TfL's satisfaction.

Letter dated 1st September 2015 (following consultation on updated information in July 2015)

Confirm that TfL are satisfied with the A5 Corridor Study and no objection to the application.

LB Brent

Letter of Objection from the London Borough of Brent dated 9th January 2015

Brent Council wishes to strongly object to this application seeking to discharge condition 2.7 (A5 Corridor Study) of S73 planning application ref: F/04687/13 dated 23/07/2014 for the following reasons:

The report does not provide confidence that the BXC development will not result in a strongly adverse impact on the local and strategic road networks. Mitigation measures proposed are limited and no evidence appears to be provided that they will be sufficient, in fact the modelling outputs provided appear to suggest that they will not be. The following points need to be addressed urgently:

- The high Degree of Saturation measure used to assess junctions. This needs to be lower.
- The lack of mitigation measures proposed for junctions within Brent that will be directly affected by the development. As a minimum some contribution to mitigation is expected.
- The apparent lack of mitigation proposed for the A5 itself, particularly given the evidence suggesting that the A407 Cricklewood Lane/Claremont Road/Lichfield Road junction will be operating far over capacity in the end state.
- Parking management needs to be discussed urgently with Brent Borough Council.
- More robust walking and cycling measures need to be provided if the modelling is to rely on these to mitigate traffic growth due to development.
- The increased bus journey times need to be addressed.
- The modelling outputs need to be provided in a format which is readily interpreted and which therefore provides confidence in the modelling process.
- The VISSIM modelling needs to be completed.

Letter of Objection from the London Borough of Brent dated 14th August 2015

Confirmed that following the objections raised by Brent in January 2015 and those raised by other organisations some changes have been made to the document. It is considered that the following issues have been resolved:

- The increased bus journey times have been addressed within the new VISSIM modelling and are no longer significant once background increases in congestion have been accounted for. The impact on Brent residents will therefore be minimal.
- The modelling outputs have been provided in a format which can be interpreted and completion of the VISSIM modelling provides some confidence in the modelling process. There is therefore less concern regarding potential impacts that may have been missed.
- The VISSIM modelling has now been completed to stage 4+ and signed off by Transport for London.

However, the main concerns of Brent have not been addressed. These are as follows:

- The lack of mitigation measures proposed for junctions within Brent that will be directly affected by the development. The A5CS proposes mitigation only for those junctions that operate at below 90% saturation pre-development and above 90% capacity post development. Within Brent the junctions that fulfil this criteria are Chichele Road/Anson Road and High Road/Walm Lane, and some mitigation for these has been proposed. However, this methodology results in some junctions which are already over 90% capacity receiving no mitigation, even if considerable increases in the degree of saturation are predicted. This is particularly concerning given the 7.5% increase in saturation at the junction of Lydford Road and Willesden Road, suggesting that this junction will be materially impacted to the detriment of local traffic flow with no prospect of mitigation. Brent Council continues to object to this methodology and the lack of proposed mitigation where impacts are apparent.

Officer Response: *The 90% measure was agreed by Transport for London and is within the agreed scoping documents for the study.*

A £300,000 fund towards future Supplementary Transport Measures within Brent and Camden has been agreed with the Brent Cross Development Partners (letter dated 2nd September 2015).

- The lack of mitigation proposed for the A5 itself, particularly given the evidence suggesting that the A407 Cricklewood Lane/Claremont Road/Lichfield Road junction will be operating far over capacity in the end state. Some works are proposed here, however the junction appears to still be very close to capacity in the end state scenario. We would require further measures at this location.

Officer Response: *The A407 Cricklewood Lane/Claremont Road/Lichfield Road junction is a gateway junction with a previously approved junction design. Forecast SATURN flows for 2031 indicate the following traffic flow increases from 2021:*

*AM peak – from 2,428 to 2,737 (increase of 13%)
 PM peak – from 2,456 to 2,466 (an increase of less than 1%)
 Saturday peak – from 2,290 to 2,414 (increase of 5%)*

The maximum degrees of saturation in 2031 are 113%, 124% and 111% in the AM, PM and Saturday peaks.

The deployment of SCOOT will reduce delay at this junction and further opportunities to optimise performance will be discussed with TfL during the detailed design of this junction.

Mitigation for the A5 is summarised in Tables 5.1 – 5.3 of this report and includes improvements at multiple junctions.

- There has been considerable discussion regarding the need to ensure Brent residents are not unduly impacted by overspill parking or construction worker parking. It has been pointed out that the monitoring regime suggested is not sufficient and that a CPZ in the Dollis Hill area will likely be required during construction, with the CPZ in place prior to works starting. Though we have had verbal agreement to address this, the offer in the document does not provide adequate certainty and we require a separate written undertaking to be provided if the study is not changed. Due to this lack of certainty, our objection to the document on this continues to stand.

Officer Response: *The funding of new or extended Controlled Parking Zones in Brent is available through the Consolidated Transport Fund and would need to be applied for either through the Transport Advisory Group or by the London Borough of Brent directly to the Transport Strategy Group (London Borough of Barnet and TfL). The Transport Strategy Group is required to take account of the Transport Advisory Group's recommendations. The requirement for Controlled Parking Zones within Brent has been raised and discussed at the Transport Advisory Group and the need for provision within the Dollis Hill area outside the scheme boundary has been agreed between Brent and the developer (as this is outwith the Section 106).*

- Walking and cycling measures are mostly contained within the AWWCS, however this forms an element of the A5CS. The comments and proposals made by Brent have not been taken forward and this needs to be addressed.

Officer Response: *The extent of walking and cycling provision to and from the development is a combination of measures detailed within the A5 Corridor Study, the Area Wide Walking and Cycling Strategy and the Phase 1A North Pedestrian and Cycle Strategy. Modal split target figures are set for the development at each phase with the Transport Advisory Group of which the London Borough of Brent are a member, reviewing any failure to meet such targets.*

With the exception of cycle parking near Keyes Road, which scored as green (good), all cycling provision was rated as amber (average).

The improvements put forward are with a view to increase the Pedestrian Environment Review System and the Cycling Environment Review System scores of the A5 links and to improve conditions for pedestrians and cyclists heading through the junctions. The suggested improvements are initial proposals that are subject to feasibility and detailed design at a later stage.

These improvements would offer an improved cycle environment over the current situation, and are considered to be appropriate when the requirements of other modes along this corridor are considered.

LB Camden

Email of Objection from the London Borough of Camden dated 6th January 2015

The London Borough of Camden object on the following grounds:

- The A5 corridor study does not address impacts in Camden and despite reference being made to Camden, this is not in context of the vehicle impacts but appears to be only be in context of pedestrians and cyclists.

Wording: Ensure that any local traffic impacts are identified in the adjacent boroughs of Brent and Camden, as well as any further impacts in the LB of Barnet by ensuring that the traffic modelling for the design stage is sufficiently detailed in areas of interest, e.g. the Dollis Hill area and south of Cricklewood Lane. The A5 corridor study as presented seems to miss the point of this statement as very little evidence has been presented that Camden can assess to understand the impact south of the borough boundary.

Officer Response: *The A5 Corridor Study does assess local traffic impacts including those on roads in Camden.*

Use of the SATURN model has enabled changes in traffic flow on local roads in Brent and Camden to be assessed. The difference in total flows in passenger car units, on all individual links in Camden and Brent, are summarised below, identifying a rise in traffic flows with the development in place:

Time Period	Camden Total Link Differences	Brent Total Link Differences
AM 2021	2946	4724
AM 2031	8282	7072
PM 2021	5173	4390
PM 2031	7331	8461
Sat 2021	7601	6347

Sat 2031	10046	12348
----------	-------	-------

The most significant changes in flow on the local roads in Camden in 2021 have been analysed and are summarised in the following tables:

Top Ten Increases in Traffic Flow on Roads in Camden (2021)

Road	From	To	2012 Actual Flow (pcu/hr)	2021 No Development (pcu/hr)	2021 With Development (pcu/hr)	Difference	% Difference
A41	Studholme Court	Croft Way	751	872	1172	300	34%
A41	Croft Way	Ingham Road	797	917	1217	300	33%
A41	Parsifal Road	Studholme Court	797	917	1217	300	33%
A5	Skardu Road	Anson Road	489	582	762	179	31%
A41	Ingham Road	Ingham Road	997	1246	1421	174	14%
A41	Ingham Road	Weech Road	952	1201	1375	174	14%
A41	Weech Road	Weech Road	998	1247	1421	174	14%
A41	Weech Road	Ardwick Road	1020	1204	1367	163	14%
A41	Ardwick Road	Ardwick Road	1066	1249	1412	163	13%
A41	Ardwick Road	Ardwick Road	1066	1249	1412	163	13%

Road	From	To	2012 Actual Flow (pcu/hr)	2021 No Development (pcu/hr)	2021 With Development (pcu/hr)	Difference	% Difference
A41	Parsifal Road	Studholme Court	1610	1519	1687	168	11%
A41	Studholme Court	Croft Way	1563	1475	1641	166	11%
A41	Croft Way	Ingham Road	1609	1521	1687	166	11%
A41	Burgess Hill	Finchley Road	1918	2009	2163	155	8%
A41	Platt's Lane	Burgess Hill	1918	2009	2163	154	8%
A5	Skardu Road	Anson Road	579	767	916	150	20%
A41	Weech Road	Ardwick Road	1655	1697	1822	125	7%
A41	Ardwick Road	Ardwick Road	1701	1743	1868	125	7%
A41	Ardwick Road	Ardwick Road	1701	1743	1868	125	7%
A41	Ingham Road	Weech Road	1656	1705	1828	123	7%

Road	From	To	2012 Actual Flow (pcu/hr)	2021 No Development (pcu/hr)	2021 With Development (pcu/hr)	Difference	% Difference
A5	Skardu Road	Anson Road	468	591	941	350	59%
A5	Anson Road	Skardu Road	478	515	840	325	63%
A5	Skardu Road	Manstone Road	404	409	668	259	63%

A5	Manstone Road	Skardu Road	523	642	870	228	35%
A4200	Polygon Road	Cranleigh Street	298	330	479	148	45%
Phoenix Road/A4200	Werrington Street	Polygon Road	67	155	278	123	79%
Garlinge Road	A5	Fordwych Road	135	132	253	121	91%
Phoenix Road	Werrington Street	Chalton Street	171	121	221	100	83%
A41	Alvanley Gardens	Lymington Road	1313	1503	1596	92	6%
B507 Abbey Road	Belsize Road	Boundary Road	344	320	410	90	28%

Top Ten Decreases in Traffic Flow on Roads in Camden (2021)

Road	From	To	2012 Actual Flow (pcu/hr)	2021 No Development (pcu/hr)	2021 With Development (pcu/hr)	Difference	% Difference
Fortune Green Rd	Parsifal Road	Burrard Road	610.36	669.05	437.3	-231.75	-35%
Fortune Green Rd	Burrard Road	Parsifal Road	534.32	646.23	477.88	-168.35	-26%
Fortune Green Rd	Lyncroft Gardens	Mill Lane	878.29	979.85	819.5	-160.35	-16%

Fortune Green Rd	Parsifal Road	Lyncroft Gardens	838.61	954.71	801.25	-153.46	-16%
Mill Lane	Fortune Green Road	Holmdale Road	594.38	656.85	510.01	-146.84	-22%
Mapesbury Rd	A5 Shoot-Up-Hill	Exeter Road	468.47	524.23	379.01	-145.22	-28%
Mill Lane	Holmdale Road	Westbere Road	586.86	644.06	504.03	-140.03	-22%
Burrard Rd	Fortune Green Road	Ingham Road	200.73	329.19	203.3	-125.89	-38%
Burrard Rd	Ingham Road	Finchley Road	200.73	329.19	203.3	-125.89	-38%
A5 Shoot-Up Hill	Minster Road	Walm Lane	438.4	524.62	401.51	-123.11	-23%

Road	From	To	2012 Actual Flow (pcu/hr)	2021 No Development (pcu/hr)	2021 With Development (pcu/hr)	Difference	% Difference
Dersingham Road	Cricklewood Lane	Caddington Road	260.39	216.76	69.26	-147.5	-68%
Ebbsfleet Road	Cricklewood Bdwy	Fordwych Road	150.59	219.63	73.68	-145.95	-66%
Fordwych Road	Ebbsfleet Road	Cricklewood Lane	274.76	406.42	262.1	-144.32	-36%
Mill Lane	Fortune Green Road	Holmdale Road	519.76	672.77	536.79	-135.98	-20%
Mill Lane	Holmdale	Westbere Road	426.16	583.01	450.56	-132.45	-23%

Road	Road							
Mill Lane	Westbere Road	Fordwych Road	288.7	438.83	321.53	-117.3	-27%	
Fortune Green Road	Lyncroft Gardens	Mill Lane	721.08	903.87	803.27	-100.6	-11%	
Mapesbury Road	A5 Shoot-Up-Hill	Exeter Road	274.84	354.85	257.19	-97.66	-28%	
B525 Avenue Road	Avenue Close	St Edmunds Terrace	627.06	666.1	570.09	-96.01	-14%	
B525 Avenue Road	Queens Grove	Avenue Close	613.27	653.12	557.2	-95.92	-15%	

Road	From	To	2012 Actual Flow (pcu/hr)	2021 No Development (pcu/hr)	2021 With Development (pcu/hr)	Difference	% Difference
Fordwych Road	Ebbsfleet Road	Cricklewood Lane	220.51	410.88	200.48	-210.4	-51%
Whitestone Walk	N End Way	W Heath Road	620.49	651.22	472.63	-178.59	-27%
Cranleigh Street	Werrington Street	Eversholt Street	395.23	340.05	171.07	-168.98	-50%
Anson Road	Chichele Road	Sheldon Road	169.29	240.89	86.81	-154.08	-64%
A502 North End Way	Spaniards Road	Whitestone Walk	1379.3	1423.16	1276.56	-146.6	-10%

A502 North End Way	Whitestone Walk	Spaniards Road	1405.95	1398.9	1272.71	-126.19	-9%
Rondu Road	Cricklewood Bdwy	Fordwych Road	264.77	281.96	166.13	-115.83	-41%
Mill Lane	Fortune Green Road	Holmdale Road	495.93	628.13	520.48	-107.65	-17%
Cranleigh Street	Chalton Street	Werrington Street	507.68	590.1	483.8	-106.3	-18%
Harman Drive	Farm Avenue	Brondesbury Cricket	313.8	320.66	216.95	-103.71	-32%

An additional capped contribution of £300,000 towards future Supplementary Transport Measures in Camden and Brent has been agreed with the Brent Cross Development Partners (letter dated 2nd September 2015) and can be utilised if monitoring of traffic flows shows an increase in traffic due to the development. Allocation of this funding will be via the Transport Advisory Group, membership of which is open to both boroughs.

- The information submitted does not allow Camden the opportunity to assess the impact on the junctions, it simply talks about junctions with impacts above 90%, there is no consideration if junction impacts has increased significantly and might just below this figure at say 87% or 88%.

Officer Response: *The analysis has been undertaken based on the scope of the study, agreed with both Transport for London and the London Borough of Barnet. For improvements at junctions that do meet the set saturation requirements, funding is available via the supplementary transport measures allocation. The scope states:*

‘The analysis will be undertaken to compare ‘V/C’ (flow to capacity ratios) from the BXC DDM Saturn modelling in the future year scenario with no development (Do Minimum) with the V/C for phase 1 and end state model (Do Something). Junctions where ‘V/C’ is more than or equal to 90% in the with development scenario and less than 90% in the Do Minimum will be subject to detailed capacity analysis using the appropriate junction modelling tool (i.e. TRANSYT/LinSig/PICADY/ARCADY).

Where BXC DDM identifies junctions where 'V/C' is greater than 90% in the Do Minimum (in the vicinity of the development), consideration will be given to the most appropriate package of mitigation, or as termed in the s106 agreement; 'supplementary transport measures'.

Recommendations will be presented to the Transport Advisory Group where confirmation on how the transport fund should be used to progress intervention measures.'

- Parking impacts stop at the borough boundary, given the size of Brent X and the potential draw from Camden, it is considered that impacts on parking would be felt in Camden, no evidence has been submitted to determine what the impacts could be. This statement is also noted in context of the PERS and CERS audits in that although some aspects are within Camden minimal assessment has been attached to the information on which Camden can comment. There is also a concern, specifically in relation to the CERS audit that this has just concentrated on the A5, no consideration has been given to parallel routes to the east, within Camden.

Officer Response: *The impact of parking within Camden has been assessed within the Car Parking Management Strategy. The Controlled Parking Zones (CPZ's) within Camden which are in closest proximity to the proposed regeneration are:*

- *CA-P: University College Sports Ground to the north, Fortune Green Road to the east, Minster Road to the south, Westbere Road to the west: 10:00-12:00 Mon – Fri.*

The CPZ is located approximately 3 km from Brent Cross Shopping Centre and 2.4 km from the centre of the Regeneration Area to the south of the A406.

- *CA-Q: Richborough Road to the north, Fordwych Road to the east, Minster Road to the south, A5 Cricklewood Broadway to the west: 08:30-18:30 Mon-Fri*

The CPZ is located approximately 3.1 km from Brent Cross Shopping Centre and 2.5 km from the centre of the Regeneration Area to the south of the A406. Cricklewood Railway Station is located approximately 200 metres to the north of the CPZ.

The decision to either change existing CPZ restrictions or to introduce new CPZs in areas with uncontrolled parking would be made by the relevant borough, based on the results of monitoring and taking into consideration complaints about overspill parking from residents. However,

the CPZs that are most at risk from overspill parking from areas to the south of the A406 are Brent Cross Station (BX), Golders Green (H) and Cricklewood (C1) and not those within Camden.

As part of the Area Wide Walking and Cycling Study, CERS audits extending into Camden on alternative routes to the A5 have been undertaken, with improvements for cyclists within Camden being identified and funded by the developer. These consist of provision of:

- *Directional signage*
- *Cycle awareness signage*
- *Carriageway symbol markings*
- *Refreshed road markings*
- *Improved carriageway surfacing*
- *Extended cycle lane*
- *Widen feeder lanes*

on routes to West Hampstead and Kilburn Town Centre / Kilburn High Road London Overground station.

- Overall, both these reports are extremely detailed and complex covering several key aspects. The advice in the submissions that the impacts will not be felt beyond the borough boundary despite the size and draw of the development continues to be questioned by Camden. It is our view that the impacts in Camden have not been considered in detail and continue to lack information on which comments can effectively be made.

Officer Response: *The highway modelling, car parking review and cycling linkage to the site have including parts of Camden. Based on the concerns of the London Borough of Camden, the links with the most significant changes in traffic flow in 2021 and 2031 have been assessed. If monitoring of traffic flows shows increase due to the development, an additional capped contribution of £300,000 towards future Supplementary Transport Measures has been agreed with the Brent Cross Development Partners (letter dated 2nd September 2015) for the adjacent boroughs. Monitoring of parking in Barnet, Brent and Camden will be undertaken as the development progresses, but it is not envisaged that demand will increase in Camden due to the proposals. Improvements for cyclist are also being funded by the development to improve sustainable linkage to the development site.*

Consultation Responses from Other Groups

London Cycling Campaign (LCC) in Brent, Barnet and Camden dated 18th December 2014

“We are principally concerned in this matter because the proposed changes to the junction of the A5 with the A406 North Circular Road at Staples Corner West will impact on many of our members and other cyclists in Brent, Barnet and Camden using the A5 as a cycle route between local suburbs, and between these suburbs and the West End, for which journeys the A5 is the most direct and practical route. The A5 has been designated as a cycle route, LCN+5, and therefore our views should be strongly weighed.

We consider that the A5 Corridor Study submitted here does a wholly inadequate job of examining and analysing the issues for cycling on the A5 corridor, in particular at Staples Corner, and that it should not be considered as an adequate document to discharge the conditions of the planning permission.”

Officer Response: *The A5 Corridor Study recognises that congestion on the network is a problem. Therefore, where practicable, as part of the overall approach to the A5 corridor and the wider regeneration scheme, where highway interventions are proposed, the aim has been to both protect buses from congestion, and encourage walking and cycling through positive design measures.*

The A5 Corridor Study provides a review of pedestrian and cyclist accessibility, cycle parking and routing.

The volume of cyclists using the A5 corridor on a weekday ranges from 48 towards the north of the corridor (observed 2-way flow near Humber Road) to 73 towards the south of the corridor (observed 2-way flow near Chichele Road). The AM peak hour is the busiest period for cyclists out of the peak hours surveyed. Cyclists represent up to 4% of the traffic composition.

On a Saturday, cycle demand during the peak hour was observed to low with a maximum 2-way flow of 18 cyclists on the central section. Cyclists represent approximately 1% of the traffic composition.

The existing pedestrian and cycle links along and alongside the A5 have been reviewed for this study using the PERS (pedestrian environment review system) and CERS (cycling environment review system) assessment tools.

A total of 12 cycle links, 4 junctions and 4 cycle parking areas were audited along the A5. The links were determined by the changes in the cycle environment (such as type of cycle facility provided or change in surrounding land uses) and were separated as follows:

- *Link 1 (L1): Staples Corner to Geron Way*
- *Link 2 (L2): Geron Way to Opposite Comfort Delgro Building*
- *Link 3 (L3): Opposite Comfort Delgro Building to Depot Approach*
- *Link 4 (L4): Depot Approach to A407 Junction*
- *Link 5 (L5): A407 Junction to Rondou Road*
- *Link 6 (L6): Rondou Road to Mill Lane*
- *Link 7 (L7): Mill Lane to Rondou Road*
- *Link 8 (L8): Rondou Road to A407 Junction*
- *Link 9 (L9): A407 Junction to Longley Way*
- *Link 10 (L10): Longley Way to Humber Road*
- *Link 11a (L11a): Humber Road to Staples Corner (on road route)*
- *Link 11b (L11b): Humber Road to Staples Corner (off road route)*
- *Link 12a (L12a): Across A5 / A406 Staples Corner Junction (off road)*
- *Link 12b (L12b): Across A5 / A406 Staples Corner Junction (on road)*

With the exception of cycle parking near Keyes Road, which scored as green (good), all cycling provision was rated as amber (average).

The plan in Appendix 6 of this report identifies the improvements put forward as part of the A5 study to improve conditions for pedestrians and cyclists on the A5 and encourage more people to travel by both modes on the corridor. The suggested improvements are initial proposals that are subject to feasibility and detailed design at a later stage.

“The authors of the study have not assessed cycling conditions on the A5 against modern London guidance. They should have used the new London Cycle Design Standards (issued in draft earlier this year) and they should have used the Cycling Level of Service Assessment contained in that document to assess the level of service provided to cyclists by the A5 as it stands and as it would stand under the proposed

developments. They should then have made recommendations as to measures that should be taken to bring the Level of Service score to an acceptable value.”

Officer Response: *The basis, scope and extent of the study were agreed prior to the issue of the new London Cycle Design Standards. Therefore, the proposals are based on the standards at the time of commencement. Where possible, changes in standards have been taken into account as the study has progressed. Transport for London has agreed with this approach.*

“The volume of traffic on the A5 corridor is such that to provide an acceptable cycling environment, segregated cycle tracks are required. The report fails in an elementary way in its analysis of cycling in failing to make this point. There is no way that the painted cycle symbols suggested for the A5 carriageways are an adequate treatment for cycling on such a busy road, and one that will contain even more HGVs when the new waste facility is built.”

Officer Response: *Segregated cycle provision will have significant implications on other road users on this transport corridor in terms of travel times / delay for other modes on the corridor, including buses. The cycling and walking networks proposed within the development provide improved permeability, safety and the quality of provision for both cyclists and pedestrians. Many of the proposals are aimed at increasing the safety of vulnerable users within the context of the assessment.*

“The report, critically, also fails to make recommendations for how a safe route could be created through the Staples Corner West junction; in fact it totally ignores the issues around this junction, and is thus, from our point of view, a complete failure and not up to a basic standard of competence to be expected for such a report.

Any argument that cycling conditions at Staples Corner junction need not be considered because alternative cycle routes will be provided through the development is unacceptable because:

1. The scenario that all or even most cyclists currently using the A5 will divert to these new routes is not a credible one, as the new routes will be less direct, and will be slow and inconvenient as involving convoluted ramps and paths shared with pedestrians;
2. As we do not know the phasing of the building of the new facilities within the development area, there is no guarantee that these routes will come into existence before works take place on the A5 corridor which will probably make it more dangerous for cycling than at present; and,

3. The new routes appear, even when fully built, not to provide a connection with the northbound carriageway of the A5 immediately north of Staples Corner, and therefore it appears cyclists travelling northbound on the A5 corridor will have no alternatives to using the A5 carriageways under these plans.

Since none of these points are addressed by the A5 Corridor Study, we consider it to be a very defective document with respect to cycling. We suggest that the relevant sections of report be rewritten taking these points into account, and that the application not be allowed to proceed until this has been done.”

Officer Response: *The proposed improvements for cyclists at M1/A406 and A5/A406 (Staples Corner) consist of:*

- *Provision of a toucan crossing across the A5 on the northern side of Staples Corner*
- *Provision of continuous off-road shared footway/cycleway facilities alongside both the eastbound and westbound A406 carriageways linking from the A5 toucan crossings and Bridge B6 (pedestrian and cycle bridge)*
- *Bridge B6 (pedestrian and cycle bridge) over the A406 linking with shared footway/cycleway facilities on either side*
- *Replacement pedestrian bridge provided to the west of Bridge B6 (west of the rail arches), the new ramp on the northern side of the bridge will be wider than the existing provision to benefit mobility impaired users*
- *General refurbishment of the remaining existing Staples Corner footbridges (lighting/painting etc.)*

Staples Corner is a gateway junction, the design of which has full planning permission. The proposed highway layout remains as shown in the current Planning Consent, but the detailed assessments carried out during the preparation of the A5 Corridor Study have shown that the lane markings need to be refined so that the road junction can better manage the pattern of traffic demand. The minor changes are as follows:

- *Lane configuration on the A5 southbound off-slip has been modified to better suit the level of demand heading to the A406 east and westbound*
- *Changing lane markings on the western arm of the A406 from ‘two to four lanes’ to ‘three to four lanes’ to facilitate greater lane/flare occupancy*
- *Change in lane markings to give greater priority to the dominant movement from the M1 to the A406 westbound*

The introduction of a dedicated north-south cycle facility would be a significant change to the existing design and therefore require a new planning application to be approved.

Email from Campaigns Manager, London Cycling Campaign dated December 2014 (Appendix G)

“We are concerned because the proposed changes to the junction of the A5 with the A406 North Circular Road at Staples Corner West will impact on many of our 12 500 members who live, cycle and work in this area, along with significant numbers of other cyclists in Brent and Barnet. Specifically those who use the A5 as a cycle route between local suburbs, and between these suburbs and the West End, for which journeys the A5 is the most direct and practical route. Given that the A5 has been designated as a cycle route, LCN+5, the comments and concerns put forward by Brent Cyclists should be strongly weighed.

As Brent Cyclists have outlined, we consider that the A5 Corridor Study submitted does an inadequate job of examining and analysing the issues for cycling on the A5 corridor, in particular at Staples Corner. It should not be considered as an adequate document to discharge the conditions of the planning permission.”

Officer Response: *The proposed improvements for cyclists at M1/A406 and A5/A406 (Staples Corner) consist of:*

- *Provision of a toucan crossing across the A5 on the northern side of Staples Corner*
- *Provision of continuous off-road shared footway/cycleway facilities alongside both the eastbound and westbound A406 carriageways linking from the A5 toucan crossings and Bridge B6 (pedestrian and cycle bridge)*
- *Bridge B6 (pedestrian and cycle bridge) over the A406 linking with shared footway/cycleway facilities on either side*
- *Replacement pedestrian bridge provided to the west of Bridge B6 (west of the rail arches), the new ramp on the northern side of the bridge will be wider than the existing provision to benefit mobility impaired users*
- *General refurbishment of the remaining existing Staples Corner footbridges (lighting/painting etc.)*

Staples Corner is a gateway junction, the design of which has full planning permission. The proposed highway layout remains as shown in the current Planning Consent, but the detailed assessments carried out during the preparation of the A5 Corridor Study have shown that the lane markings need to be refined so that the road junction can better manage the pattern of traffic demand. The minor changes are as follows:

- Lane configuration on the A5 southbound off-slip has been modified to better suit the level of demand heading to the A406 east and westbound
- Changing lane markings on the western arm of the A406 from 'two to four lanes' to 'three to four lanes' to facilitate greater lane/flare occupancy
- Change in lane markings to give greater priority to the dominant movement from the M1 to the A406 westbound

The introduction of a dedicated north-south cycle facility would be a significant change to the existing design and therefore require a new planning application to be approved.

"We agree with Brent Cyclists in their assessment that the authors of the study have not assessed cycling conditions on the A5 against modern London guidance. They should have used the new *London Cycle Design Standards* (issued in draft earlier this year) and they should have used the *Cycling Level of Service Assessment* contained in that document to assess the level of service provided to cyclists by the A5 as it stands and as it would stand under the proposed developments. They should then have made recommendations as to measures that should be taken to bring the Level of Service score to an acceptable value."

Officer Response: *The basis, scope and extent of the study were agreed prior to the issue of the new London Cycle Design Standards. Therefore, the proposals are based on the standards at the time of commencement. Where possible, changes in standards have been taken into account as the study has progressed.*

"The volume of traffic on the A5 corridor is such that to provide an acceptable cycling environment, segregated cycle tracks are required. The report is deficient in its analysis of cycling in failing to make this point. It also fails to make recommendations for how a safe route could be created through the Staples Corner West junction; in fact it totally ignores the issues around this junction, and is thus, as Brent Cyclists have suggested, not up to a basic standard of competence to be expected for such a report."

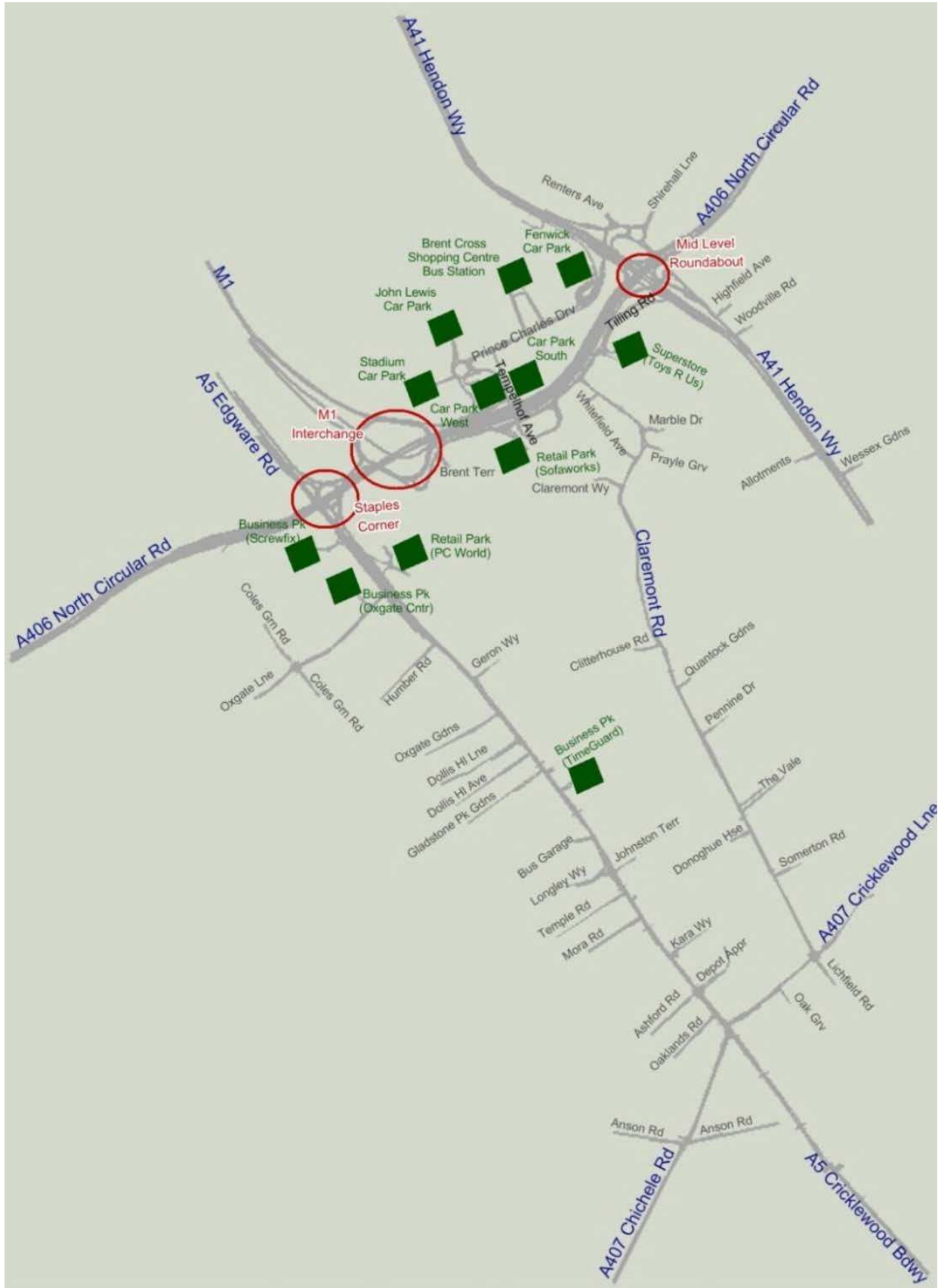
Officer Response: *Segregated cycle provision will have significant implications on other road users on this transport corridor in terms of travel times / delay for other modes on the corridor, including buses. The cycling and walking networks proposed within the development provide improved permeability, safety and the quality of provision for both cyclists and pedestrians. Many of the proposals are aimed at increasing the safety of vulnerable users within the context of the assessment.*

“As Brent Cyclists have requested, we suggest that the relevant sections of report be rewritten taking these points into account, and that the application not be allowed to proceed until this has been done.”

This page is intentionally left blank

APPENDIX 4

Plan showing the extent of the A5 Corridor Study Microsimulation Model



This page is intentionally left blank

APPENDIX 5

The schedule of all mitigation required as a result of the A5 Corridor Study

Table 1 Schedule of all mitigation required as a result of the A5 corridor study for the A5 corridor*

A5 Corridor	Gateway Junction	Phase 1	Items to be included as identified by the multimodal study					Public Realm
			Highway Capacity	Walking/Cyclists	Road Safety	Bus Priority	Parking and Loading	
Staples Corner	Yes	Yes	Junction configuration	Directional signage using 'Legible London' signage Bridge lighting Upgrade footway surfacing Upgrade pelican crossing to toucan	Design in accord with Road Safety Audit	Stop SJ and SB Live bus arrivals	-	CCTV and improved lighting
A5/Humber Road/Geron Way	Yes	Yes	New junction waste handling facility	Advanced stop lines on all arms with lead-ins of adequate width Pedestrian refuge on A5 Signage/tactile paving	Improved lighting Anti-skid to be provided as required by design standards	-	-	Improve lighting
A5 between Staples Corner and Oxgate Lane (link)	No	Yes	-	Dropped Kerbs Tactile at Oxgate Lane Improved surfacing on roads and off-road facility Cycle signs and lines Shared cycle provision to be provided where practicable possibly within existing space constraints		-	-	Clear clutter
A5/Oxgate Gardens/A5 Link Road	Yes	No	New junction A5 link road	Pedestrian phases Advanced stop lines on all arms with lead-ins of adequate width Cycle signage Directional signage	Anti-skid to be provided as required by design standards	Consider use of SVD on the Bus Lanes over MML Bridge	-	

A5 / Ashford Road / Depot Approach	No	Yes	-	Controlled pedestrian crossing on Ashford Road Cycle signage / markings Include Advanced stop lines as appropriate.	Anti-skid to be provided as required by design standards. Refuge island to prevent overtaking and protect pedestrians Anti-skid to be provided as required by design standards.	-	-	Improve lighting
A5 / Temple Road	No	Yes	-	Advanced stop lines on all arms Cycle signing and markings Junction parking restrictions Road surfacing	Anti-skid to be provided as required by design standards.	-	-	Remove obstructions
A5 / A407 Chichele Road	Yes	Yes	Reconfigure junction	-	Anti-skid to be provided as required by design standards.	-	-	
A5 Corridor	No	Yes		Use of pedestrian countdown at traffic signals to be considered at detailed design	Anti-skid to be provided as appropriate	Maintenance of bus lane markings		Remove redundant or unnecessary street furniture to de-clutter the corridor

* All items to be designed in accordance with current design standards and to be subject to full road safety audit procedures. For measures that are not part of Phase 1 further details will be supplied as part of the appropriate Phase Transport Report.

Table 2 Schedule of all mitigation required as a result of the A5 corridor study for the A407*

A5 Corridor	Gateway Junction	Phase 1	Items to be included as identified by the multimodal study					Public Realm
			Highway Capacity	Walking/Cyclists	Road Safety	Bus Priority	Parking and Loading	
A407 between A5 and Claremont Road (Link)	No	Yes	-	Advanced stop lines at junctions Lighting in Tunnel Cycle Signage	Crossing points and desire line (informal and formal)	-	-	
A407/Claremont Road	Yes	Yes	Refigured junction	Advanced stop lines on all arms Cycle signage	Anti-skid to be provided as required by design standards	-	-	
Anson Road/A407 Chichele Road	No	Yes	Link with Cricklewood Broadway UTC Group	-	-	-	-	
A407/A41	No	No	Increase cycle times	-	-	-	-	
A407 Walm Lane/High Road	No	Yes	Re-time traffic signals to be compatible with pedestrian countdown	Install pedestrian countdown at traffic signals	Anti-skid to be provided as required by design standards.	-	-	
All the locations listed above				Use of pedestrian countdown at traffic signals to be considered at detailed design Legible London signage is included in the Wayfinding Strategy and is to be considered as part of detailed design	Anti-skid to be provided as appropriate			

* All items to be designed in accordance with current design standards and to be subject to full road safety audit procedures. For measures that are not part of Phase 1 further details will be supplied as part of the appropriate Phase Transport Report.

Table 3 Schedule of all mitigation required as a result of the A5 corridor study for other areas*

A5 Corridor	Gateway Junction	Phase 1	Items to be included as identified by the multimodal study					Public Realm
			Highway Capacity	Walking/Cyclists	Road Safety	Bus Priority	Parking and Loading	
Dollis Hill Area - UC7	No	Yes	-	-	-	-	Consideration of a CPZ will be the subject of a separate agreement with LB Brent.	
Links identified in the Monitoring Strategy (within the area of influence) in the London boroughs of Barnet, Brent and Camden	No	Yes	Traffic management interventions to be considered at the Transport Advisory Group should monitoring and/or modelling demonstrate significant adverse effects from the development.					

Table 4 Schedule of further enhancements identified by the authorities to encourage mode shift but not required to mitigate the Development*

Ref	Section/Junction	Short/medium/long term	Measure
1	Overall corridor measures	Long	Undertake a feasibility study to test impact of 20 mph speed limit on the 'high street' section (see Figure 4.2) to encourage cycling, improved conditions for pedestrians without reducing peak per hour journey time reliability, particularly buses (within VISSIM model). The scheme could be treated as an experimental or pilot scheme.
2	Overall corridor measures	Medium/Long	Review committed junction works beyond phase 1 and decide if necessary
3	Staples corner	Short/medium/long	Provide entry treatments (such as raised tables) at minor road roads and access – considering needs of cyclists, visual and physical impaired road users. Apply 'Better Streets' grading of treatments
4	Eastern side of the A5	Medium and long	Safeguard land as plots come forward for development on the eastern side of the A5 to potentially increase pedestrian/cycle/road space.
5	A5/Longley Way	Short	Reconfigure kerb line to reduce size of junction bell mouth
6	A5 Geron Way (South)	Short	Tighten radius on the southern side of the junction
7	A407 Cricklewood Lane/Claremont Road Junction	Long Term	Where possible, look to acquire third party land at junction to facilitate the provision of bus priority on the southbound approach to the junction.
8	A5 Cricklewood Broadway/A407 Cricklewood Lane	Long Term	Incorporate bus priority measures on westbound approach to junction
9	A5/MML Link Road	Long Term	Incorporate bus priority measures on northbound and southbound approaches to junction. Access feasibility of formal pedestrian crossing on all arms of junction
10	A407 between A5 and Claremont Road (Link)	Short/medium/long	Undertake freight environment review system (FERS) audit
11	A5 Northbound from Ashford Road to Comfort Delgro	Medium term	Advisory on-road cycle lane Improve surfacing on both footway and carriageway
12	A5 northbound – Oxgate Gardens and Humber Road	Medium term	Provide off-road shared facility Provide northbound bus lane south of Humber Road

* All items to be designed in accordance with current design standards and to be subject to full road safety audit procedure

This page is intentionally left blank

APPENDIX 6

**Pedestrian and Cycling Improvements put forward as part of the A5 Corridor
Study**

APPENDIX 7

Proposed junction and bus infrastructure improvements at end state

ISSUE	NO.	DATE	BY
FIRST ISSUE	1	14.04.15	P01

BASED ON THE ORIGINAL STREET LAYOUT AND THE CONTROLLER OF HIGHWAYS' PERMISSIONS. THE ORIGINAL DESIGN WAS APPROVED BY THE CONTROLLER OF HIGHWAYS' PERMISSIONS. THE ORIGINAL DESIGN WAS APPROVED BY THE CONTROLLER OF HIGHWAYS' PERMISSIONS.

NOTES:
1. THE DRAWING IS A PRELIMINARY DESIGN AND IS SUBJECT TO CHANGE WITHOUT NOTICE.
2. THE DRAWING IS A PRELIMINARY DESIGN AND IS SUBJECT TO CHANGE WITHOUT NOTICE.
3. THE DRAWING IS A PRELIMINARY DESIGN AND IS SUBJECT TO CHANGE WITHOUT NOTICE.

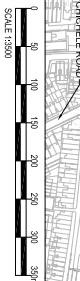
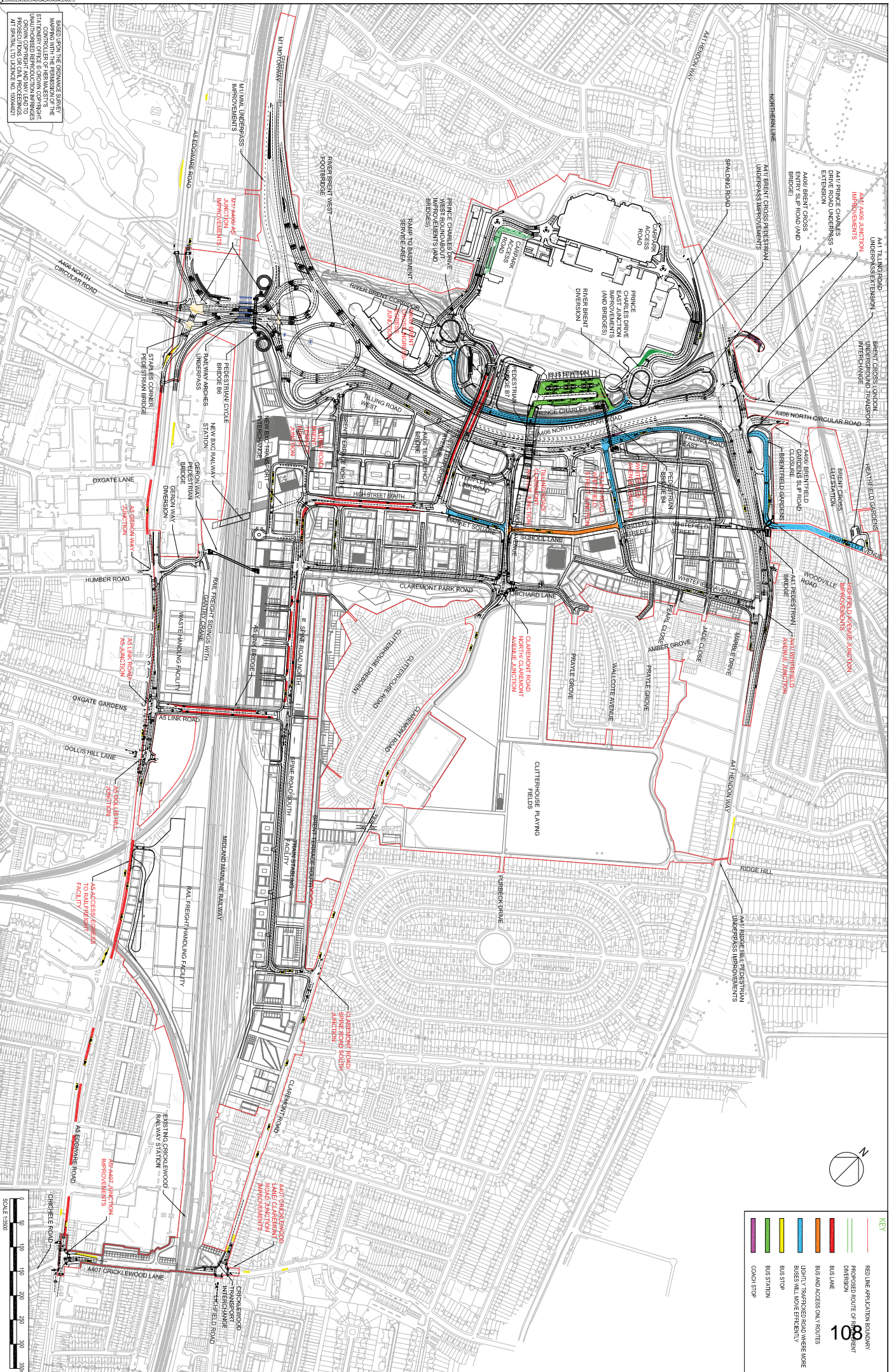
FOR INFORMATION ONLY

Standard Life
Investments
Hammerston

BRENT CROSS
CRICKLEWOOD

BRENT CROSS
GENERAL ARRANGEMENT
END STATE
BUS PRIORITY MEASURES

URS
BRENT CROSS CRICKLEWOOD
BUS PRIORITY MEASURES
P01



	RED LINE APPLICATION ROADWAY
	PROPOSED ROUTE OF HIGHWAY
	BUS LANE
	BUS AND ACCESS ONLY ROUTES
	LIGHTLY TRAFFICED ROAD WHERE MORE BUSES WILL MOVE EFFICIENTLY
	BUS STOP
	BUS STOP
	BUS STATION
	COACH STOP

LOCATION: Brent Cross Cricklewood Regeneration Area, London,
NW2

AGENDA ITEM 8

REFERENCE: 15/03312/RMA

Received: 29 May, 2015

Accepted: 02 June, 2015

WARD: Childs Hill, Golders
Green, West Hendon

Expiry: 01 September,
2015

APPLICANT: Brent Cross Development Partners

PROPOSAL: Reserved Matters application within Phase 1a (North) of the Brent Cross Cricklewood Regeneration scheme relating to Layout, Scale, Appearance, Access and Landscaping for infrastructure including roads, bridges and River Brent diversion works. Submission is pursuant to conditions 1.2.1.A, 2.1 and for the part discharge of condition 13.1 of planning permission F/04687/13 dated 23 July 2014 for the comprehensive mixed use redevelopment of the Brent Cross Cricklewood regeneration area.

Application is accompanied by an Environmental Statement Further Information Report.

RECOMMENDATION

Resolution to approve subject to:

Part 1:

The completion of a satisfactory Deed of Variation to make the necessary amendments the existing Section 106 Agreement dated 22nd July 2014 attached to planning permission F/04687/13, to secure the following:

- 1) Amendment to introductory text to Schedule 1 to allow the minor amendment of defined terms in Schedule 1 of the S106 agreement to be agreed in writing between the Brent Cross Partners, CRL, the LPA and / or TFL where applicable;
- 2) Amendment to Definitions to accord with submissions against agreed definition changes under section 96A applications and changes resulting from conditions 2.4 and 2.5 and in accordance with approvals of Phase 1A (North) Reserved Matters Applications; and
- 3) A contribution of £200,000 (Index Linked) for naturalisation works to an area of Mutton Brook upstream of the application site to compensate for the loss of 1217m² to The Brent River Corridor.

Part 2:

That upon completion of the Deed of Variation specified in Part 1 of the recommendation above, the Assistant Director of Development Management and Building Control approve reserved matters application reference 15/03312/RMA under delegated powers subject to the conditions set out in Appendix 1 to this report and any changes to their wording and or deleting and or adding conditions as considered necessary by the Assistant Director for Development Management and Building Control.

1. APPLICATION SUMMARY**What matters does this Reserved Matters Application (RMA) for Infrastructure consider and how does it relate to the Section 73 Application approved in January 2014?**

This application relates to Phase 1A (North) of the Brent Cross Cricklewood regeneration scheme and seeks approval of detailed matters reserved under the 2014 Section 73 outline planning permission (the '2014 S73 Consent').

The application was originally submitted in January 2015 as part of a suite of four separate reserved matters applications in relation to Phase 1A (North) covering the following:

- Development Plots 53 and 54, Brent Terrace (Reserved Matters consent granted on 9 June 2015);
- Clitterhouse Playing Fields and Claremont Park Open Spaces (Reserved Matters consent granted 10 July 2015);
- Central Brent Riverside Park (also on this agenda before the committee); and
- Infrastructure (resubmitted in June 2015 following on-going discussions with the Council and the subject of this report)

The 2014 S73 Consent provided full planning permission for major highway works to key junctions including the M1/A406/A5 (including pedestrian and cycle bridge B6), Brent Cross A406 Ingress/Egress, A406/A41, A5/A407 and A407/Claremont Road. These are referred to as the 'gateway junctions', and as such do not form part of this RMA.

This RMA relates to the transport enabling infrastructure to facilitate the development of the whole of the Northern part of the Brent Cross Cricklewood scheme, as well as providing the key transport infrastructure to allow development south of the A406 to also commence. In addition to the gateway junctions listed above which were granted planning permission under the 2014 S73 Consent the works include:

- Prince Charles Drive and western and eastern roundabouts at either end
- New connection from the A406/A41 inbound to the eastern roundabout
- Modified Car Park Access Roads
- New Tempelhof Avenue and bridge over A406
- Living bridge over the A406
- Tempelhof Link Road and junction with Claremont Avenue
- Market Square and junction with Claremont Avenue and School Lane
- Claremont Avenue
- Modified connections to Tilling Road, Brentfield Gardens and other local roads in the vicinity of the A41
- Changes and improvements to Tilling Road, junction with Claremont Avenue and modified junction with Brent Cross South retail park
- Orchard Lane and junction with Claremont Avenue, Claremont Park Road and Claremont Road
- Various minor modifications to tie-in with the existing road network, such as Brent Terrace North, Spalding Road, Prince Charles Drive east
- Temporary bus station facilities on the south west and north west car park sites
- Other accompanying bridges, structures and retaining walls, particularly those associated with the diversion of the river Brent.

The plan at **Appendix 8** highlights the approved Gateway Junctions in yellow and the road and bridge infrastructure proposed under this RMA in red.

The committee is being asked to approve the detailed layout and network for vehicles, cycles and pedestrians that provide connections to the gateway junctions, completes the networks for the Northern development and provides sufficient connections across and to the south of the A406 to facilitate the start of the Southern development. The application being recommended for approval therefore includes roads, footways, cycle paths, indicative bus stops, amended A406 coach stops, bridges and indicative retaining wall structures. It also includes the diversion of a stretch of the River Brent to facilitate the expansion of the Brent Cross Shopping Centre.

A separate RMA application is also before Members in relation to the associated landscaping works for the central section of a new green corridor and riverside park along the diverted section of the River Brent, known as Central Brent Riverside Park, which includes a pedestrian and cycle path.

What progress has been made since the approval of the 2014 Section 73 Application?

The 2014 Section 73 planning conditions require a number of transport strategies, reports and feasibility studies to be submitted to LBB prior to submission of the first RMAs for the Development. These conditions are known as Pre-RMA conditions, and a number relate to overall transport strategies which affect the whole development, information required that relates to the whole of Phase 1 and information required for just Phase 1A (North). They comprise the following:-

Condition 1.9	Construction Consolidation Centre Feasibility Study
Condition 1.20	Area Wide Walking and Cycling Study (AWWCS)
Condition 1.21	Framework Servicing and Delivery Strategy
Condition 1.22	Phase 1A (North) Servicing and Delivery Strategy
Condition 2.7	A5 Corridor Study (Subject of a separate report also before this Committee)
Condition 2.8	Pedestrian and Cycle Strategy for Phase 1A (North)
Condition 7.1	Estate Management Framework
Condition 11.1	Car Parking Management Strategy
Condition 11.2	Phase 1A (North) Parking Standards and Strategy
Condition 37.2	Phase 1 Transport Report
Condition 1.17	Illustrative Reconciliation Plan

At the time of writing this report, the necessary information needed to satisfy these conditions has been submitted and agreed with Officers. An update on the discharge of these conditions will be provided in the Addendum. Details in relation to the content of these pre RMA conditions are addressed later in this report under section 5.2.

In order to do any alterations to existing adopted roads, the developers will need approval from the relevant highway authority responsible for looking after the road (either London Borough of Barnet, Transport for London (TfL) or Highways England (HE)) for the technical design and engineering for the roads. This approval process is separate to the Planning process and is done under Section 278 of the Highways Act. Therefore in addition to the above reports and assessments, the highways feasibility design of the new roads and junctions for Phase 1A (North), including the gateway junctions which already have planning approval, has also been progressed in conjunction with HE and TfL.

To support the highway design and approval process the authorities previously agreed that a new transport model (BXC detailed design model - DDM) for predicting traffic flows should be built based on TfL's latest sub-regional model. It was further agreed that this transport model would be used for the A5 Corridor Study as TfL's latest sub-regional model network contained more detail than the model previously used at outline application stage. The BXCDDM has been used to sense check the Phase 1A (North) highways scheme and junction layouts. The results of the junction feasibility design and modelling work (excluding the already approved gateway junctions) are set out in the Reserved Matters Transport Report (RMTR) and explained in this report.

The Transport Advisory Group (TAG) has also been established and has met on a number of occasions to discuss the draft plans, studies and strategies. The TAG members include LBB, TfL, HE, and the Developers team, and one of the main tasks has been to liaise with and inform other key stakeholders of progress on the project. Various meetings have been held involving LB Brent and LB Camden, particularly with regard to the A5 Corridor Study.

How will these proposals change the existing vehicle access to Brent Cross Shopping Centre?

Vehicles will have new and improved access to and from the Brent Cross Shopping Centre (BXSC) and to the Southern development, as a result of the new Phase 1A (North) road layout. The key changes include a new slip road directly from the improved western roundabout onto the A406 and a new slip road for traffic exiting the A406 from the west, connecting via the A41 junction to the new eastern roundabout. Traffic from the A406 east will be able to access the BXSC directly via the A41 junction, thus avoiding Tilling Road and Tempelhof bridge. Traffic from the A41 south similarly benefits. To the south of the shopping centre, traffic crossing Tempelhof Bridge has a more direct route to and from Claremont Road, avoiding Tilling Road altogether.

How will these proposals improve access and facilitate regeneration to the South of the North Circular?

Traffic will have easier access to the Southern development by the end of Phase 1 via the M1/A406/A5, A406/A41, A5/A407 and A407/Claremont Road gateway junctions. The Tempelhof bridge and its approach roads on both sides of the A406 are being widened to provide additional capacity, particularly for pedestrians, cyclists and buses. In addition the junctions of Tilling Road with Brent Terrace North, Tilling Road with Claremont Avenue, Market Square / Claremont Avenue / School Lane and Claremont Road / Claremont Avenue / Claremont Park Road / Orchard Lane are all being provided with new traffic signals, and the roundabout west of the Holiday Inn is being improved. Southern development plots will be accessed in due course from Brent Terrace North, Tilling Road, School Lane and Claremont Park Road. There are also improvements for pedestrians, cyclists and bus users, as discussed below.

How does this application relate to future phases of the project? How does it provide a transport network necessary to accommodate future phases?

Nearly all the new Phase 1A (North) roads and junctions are being designed with enough capacity to accommodate the predicted traffic levels for the whole development at end-state. The only exception is Tilling Road west of the Holiday Inn which is only being improved to accommodate the early phases. Otherwise, the network created in Phase 1A (North) provides all necessary connections to facilitate the rest of the northern development and in addition, some of the key roads south of the A406. Key elements of the road network for the Southern development are provided for, including the

western end of School Lane (which will eventually connect to Whitefield Avenue when complete); Claremont Park Road eastern end (which will extend in a future phase to link with Spine Road North); and the corner forming Market Square / New Tempelhof Avenue which will be the point at which High Street South joins. The RMA for Phase 1A South is anticipated soon and will include some additional roads in the southern area to further facilitate the southern development.

The new diverted Prince Charles Drive will provide vehicle links to the new bus station and proposed basement servicing between the western and eastern roundabouts. The bus station and basement servicing area will be provided as part of the BXSC in Phase 1B North.

The Phase 1A (North) part of the pedestrian and cycle network is also provided.

As the new highway layout will be incomplete at this stage some temporary road layouts and associated amendments to traffic flows are proposed in order to provide intermediate arrangements pending the completion of the road layout at end-state. Some details of these temporary road layouts are granted permission under this RMA, with others to be submitted under Conditions 1.8 and/or 1.8a.

What will be the impact on the highway network (including local roads in Barnet, Brent and Camden)? What mitigation is proposed and how will it be monitored?

The highway impacts of the overall scheme were assessed in the Section 73 Transport Assessment using the previously approved transport model (BXCTM). However, an updated assessment was carried out in more detail as part of the A5 Corridor Study which, as mentioned above, used the new BXCCDM. The A5 Corridor Study identified there would be additional impacts on local roads in Brent and Camden and an additional capped contribution of £300,000 has been secured from the Brent Cross Development Partners (as per letter dated 2nd September 2015) towards future Supplementary Transport Measures if monitoring of traffic flows indicates increases due to the BXC development. The traffic impacts of Phase 1, based on the BXCTM, are detailed in the Phase Transport Report (submitted pursuant to Condition 37.3). This report includes several sensitivity tests to alleviate the authorities concerns regarding the traffic impact due to the interim highway layouts, in relation to the various amounts of future development expected to be implemented prior to the delivery of the new Thameslink train station. The impacts on Phase 1 junctions are assessed in the RMTR as explained above. Monitoring of the impact of the development is controlled through a Monitoring Strategy, which is a pre-commencement condition and so the exact detail is yet to be agreed, however, in broad terms, this will involve various traffic and other transport surveys being undertaken on a regular basis to help understand the on-going impacts of the development, inform future reserved matters applications and assist in the development of any further mitigation measures.

Additionally, there is a control mechanism included in the Conditioned scoping of each Phase Transport Report that ensures that the impacts of the Development remain within the overall predictions identified in the Section 73 Transport Assessment. If there are any adverse impacts identified through the monitoring then the scoping of the next Phase Transport Report must propose Supplementary Transport Measures in order to provide further mitigation.

How will the proposals deal with concerns in relation to the impact of the development on the A5 and on surrounding roads in Brent and Camden?

The A5 Corridor Study is one of the Conditions (2.7) that under the Section 73 2014 Section 106 agreement must be considered before or at the same time as the RMA that is the subject of this report. The A5 Corridor Study is subject of a separate report before Members at this meeting and includes an assessment of any local impacts of the BXC scheme, including in the adjoining areas within LB Brent and LB Camden, and builds on the outline scope contained in the 2010 Section 106 Agreement for the 2010 consented scheme. The full scope for the study has been agreed with the relevant transport authorities and encompasses a detailed model of the A5 and an assessment of all transport facilities along the corridor. The assessment of local impacts uses a new BXCDDM traffic model. The Developers are required under the Revised S106 Agreement to fund any supplementary mitigation measures in Barnet, Brent and Camden that may be identified as part of the study, and the separate report provides full detail of the agreed measures and funds arising out of the study, including several junction improvement schemes and the £300,000 mentioned above.

It should be noted that the LB Brent and LB Camden have both objected to the A5 Corridor Study (although this was prior to the funding of the study proposals being agreed with the Developers). LB Brent expressed concerns about a lack of mitigation measures along the A5 and in Brent itself, and parking pressures in the Dollis Hill area. LB Camden are also concerned about the identification of traffic and parking impacts in Camden. The full objections and detailed responses are contained in the separate report before Members, which explains how concerns about traffic and parking impacts, both predicted and potential, are proposed to be dealt with.

Officers are satisfied that the study has been carried out in accordance with the agreed and approved scope, and that the key impacts have been correctly identified and assessed. A package of supplementary mitigation measures and additional contributions has been agreed which is considered to adequately address expected impacts, as well as providing additional safeguards should additional supplementary mitigation be needed in future phases.

What are the proposed arrangements for car parking – both in the new shopping centre and on local roads?

Parking standards set out in the 2014 S73 Consent may lead to 6,330 car parking spaces for residential use with an additional 11,565 spaces intended for non-residential use. 7,600 car parking spaces are allocated for retail and related uses within the BX East zone. The majority of residential spaces and spaces for office, industrial and community use are proposed to the south of the A406. The vast majority of parking proposed is on plot with only a small proportion located on street. The car parking levels and arrangements are not affected by this RMA.

Car parking charges at Brent Cross Shopping Centre will be introduced when the shopping centre extension within Phase 1B (North) is first occupied, as set out in paragraph 2.1 of Schedule 16 of the S106 Agreement, reproduced below:

Less than 1 hour: £1	3-4 hours: £2.50	6-7 hours: £12.00
1-2 hours: £1	4-5 hours: £3.50	7-8 hours: £14.00
2-3 hours: £2	5-6 hours: £6.00	8 hours plus: £30.00

The whole BXC site (northern and southern developments) within the red line boundary will be a designated Controlled Parking Zone and all residents and visitors within the area will be given the opportunity to be issued with resident/visitor parking permits. Any new residents provided with the opportunity for dedicated off-road parking will normally be excluded from applying for on-street residents parking permits.

The decision to introduce any other new Controlled Parking Zones or extensions to existing ones outside the red line boundary, whether related to construction activity or due to the resultant development, is controlled through the S106 (schedule 3), and other elements of the framework of control. In any event any new or amended CPZ will need careful consideration, involving the TAG as appropriate, and will be subject to comprehensive monitoring prior to and post implementation.

What are the proposals for public transport?

The key change as part of the Phase 1 development for public transport is the replacement of the existing bus station with a new fully integrated high quality facility within the extended BXSC. The new permanent bus station will be considered in detail as part of the next phase – Phase 1B North.

Subject to the proposed Phase 1 construction programme (which is still under consideration) a temporary bus station may be required for up to 4 years. The temporary bus station facility is proposed to be located on the southwest car park site comprising a number of bus stops, bus stands and driver facilities. This bus station will operate 24 hours a day as the existing Brent Cross bus station does. Further temporary bus stops will also be located on part of the existing shopping centre car park to the rear of properties in Layfield Close, known as plot 113. During daytime operating hours (7:30am to 9:30pm on weekdays and Saturdays and 9am to 9:30pm on Sundays and Bank holidays) buses would set down and pick up passengers from the temporary stops on

plot 113 which is closer to the BXSC, with the southwest car park being utilised to park up buses between shifts and provide bus services overnight.

Other changes and improvements as part of Phase 1A (North) include new bus lanes across Tempelhof Bridge in both directions and along part of the new Tempelhof Avenue, 5 new bus stops and 10 replacements, as well as modifications to the 2 coach stops currently on the A406 western slip roads to the A41 junction. The illustrative location of these bus stops are shown on the RMA drawings and the final locations will be agreed with TfL during the S278 Agreement process.

How will this application improve the pedestrian and cycle network in the Regeneration Area?

Within the development area the network of local streets will provide substantially improved pedestrian and cycle routes as part of Phase 1A (North), as set out in the Pedestrian and Cycle Strategy (submitted pursuant to Condition 2.8 of the S73 Consent). The development improves access across the A406 by providing two new pedestrian and cycle bridges, one at Staples Corner and one close to the shopping centre, known as the Living Bridge. Access over the A406 will be further enhanced by the improved replacement Tempelhof Bridge and the improvement of facilities at the A406 / A41 junction. Cycle routes across the A406 provide a link to the proposed A41 Cycle Superhighway CS11 via a mostly segregated cycle route through Clitterhouse Playing Fields. Both pedestrians and cyclists will be able to access and use the new walkway/cycleway through the new riverside park which will run along the new diverted River Brent, thus avoiding the eastern and western roundabouts and Prince Charles Drive, which have not been designed to accommodate pedestrians and cyclists.

Pedestrian and cycle networks will connect to the development's hinterland via the routes approved in the Area-Wide Walking and Cycling Study (AWWCS) submitted pursuant to Condition 1.20 of the Section 73 Consent. These will connect to Brent, Camden and other locations in Barnet including Hendon (Brent Street), Hendon Central tube station, Hendon railway station, West Hendon, Temple Fortune, Golders Green and Childs Hill. Key connecting points completed as part of Phase 1A (North) include via Sturgess Park, Claremont Park, Clitterhouse Playing Fields, Staples Corner and across the A41 via existing improved subways at Haley Road and Ridge Hill, and via the A41 / A406 junction.

It should be noted that a developer contribution of £1,250,000 towards routes providing connections beyond the Regeneration Area has been agreed with the Development Partners (letter dated 2nd September 2015) as part of the Area Wide Walking and Cycling study and will be secured through the existing mechanisms within the Section 106 agreement.

The London Borough of Brent objected to the AWWCS, key concerns being that the measures proposed were too limited to provide a sufficient incentive for people to walk or cycle and that the measures did not mesh with LB

Brent's own cycle route plans and proposals. However, officers consider that the agreed measures arising out of the AWWCS does provide sufficient proposals to comprehensively improve key local walk and cycle routes in the vicinity of the BXC development opportunity. The £1,250,000 mentioned above includes a £300,000 contribution towards a separate cycle route requested by LB Brent between BXC and Wembley.

How will the Construction Impacts be dealt with?

The construction of the new roads and junctions are currently expected to take place over a similar time period as the BXSC expansion, Phase 1B North, which will be the subject of a future RMA. Construction work on Phase 1A (North) is currently due to start in 2016 and take approximately 4 years, although it should be noted that a detailed development delivery programme for Phase 1A (North) will be submitted under condition 5 and will set out the sequencing and approximate duration of works within the phase. During this time various temporary road layouts and routes for buses, cyclists and pedestrians will be needed, together with parking controls to manage on-street parking for up to 3,300 anticipated construction workers who are expected to be on-site at the busiest times. There are controls within the Section 106 legal agreement to protect the transport network in the local area, including a requirement for a Construction Transport Management Plan and a Construction Workers Travel Plan, which must be submitted and approved before any construction can commence. There are also other conditions attached to the Section 73 Consent which require various Construction Mitigation and Management Plans to be submitted to and approved by the Council before the main construction work can begin.

How does this RMA relate to the detailed highway designs to follow as part of the Section 38 and 278 Highways works?

Feasibility design work on all Phase 1A (North) transport infrastructure, whether related to the gateway junctions already approved by the Section 73 Consent or to the remaining road and transport infrastructure within this RMA, has been progressing between the Developer and LBB, TfL and HE. This process and the associated technical approvals that the Developers will need to obtain through Section 38 and 278 agreements with the highway authority is separate to the Planning process, however the technical design work on these roads and junctions has progressed to the stage where it is now considered appropriate to determine this RMA. If Members are minded to approve this application then the detailed design of the schemes will continue under the appropriate highways legislative control (Section 38 / 278 of the Highways Act 1980), as per all other planning-approved highway schemes. It should be noted that it is normal for there to be minor changes, such as to the exact alignment of particular carriageways, as part of the detailed design processes but there has been a significant amount of design and assessment already undertaken and officers consider the RMA designs robust and unlikely to change significantly.

What changes are proposed for the River Brent?

Included in the Infrastructure RMA is the diversion of the River Brent and the associated formation of a new river channel corridor running east to west. The River Brent in its current form is a canalised concrete channel running east to west through the car parks to the south of the existing Shopping Centre. Diverting the river to the south, to reflect the line of the A406 North Circular Road, will enable a more deliverable regeneration development to be undertaken within Brent Cross. Diverting the River also affords an opportunity to create a more naturalised river course. The proposals, in broad outline, create a meandering river course that will re-connect at the eastern and western ends of BXC into the existing river course. The opportunity arises to create a naturalised parkland riverside environment with marginal and wetland landscaping throughout the river channel, improved biodiversity throughout and ultimately improved water quality. In the latter case, improved water quality will benefit the River Brent and the Welsh Harp Reservoir Site of Significant Scientific Interest (SSSI) that it leads into. Outwith the river channel, land is provided to form a parkland setting and public access. Overall the intention is to improve access and enrich biodiversity.

What is the Living Bridge?

The Living Bridge is a key element of the BXC regeneration scheme which aims to link the existing and proposed southern and northern development areas at Brent Cross via a 'City Garden' bridge structure that will connect the areas. The bridge will provide a seamless quiet connecting route for pedestrians and cyclists from south to north avoiding the A406 North Circular. The route is approached by 1:30 graded southern bridge approach which rises gently to the bridge through future development plots and which then crosses Tilling Road, the A406, Prince Charles Drive and the diverted River Brent Corridor. On the north side the bridge will connect into the Brent Cross Shopping Centre development. The bridge will be 16m wide with a clear through route not less than 6m in width.

2. BACKGROUND TO THE BRENT CROSS CRICKLEWOOD REGENERATION

2.1 Outline Consent

The principle of development at Brent Cross Cricklewood was first established by way of a site-specific Development Framework produced in April 2004 as Supplementary Planning Guidance (SPG) in accordance with the London Plan. The SPG established a vision to *'to create a new gateway for London and a vibrant urban area for Barnet'*.

The comprehensive redevelopment of the wider Brent Cross Cricklewood regeneration area was subsequently granted planning permission in outline in 2010 under planning permission C/17559/08 (the 2010 permission). Subsequently, this permission was revised under a Section 73 Planning

application (F/04687/13) which was approved on 23 July 2014 (the 2014 permission) described below:

Section 73 Planning application to develop land without complying with the conditions attached to Planning Permission Ref C/17559/08, granted on 28 October 2010 ('the 2010 Permission'), for development as described below: Comprehensive mixed use redevelopment of the Brent Cross Cricklewood Regeneration Area comprising residential uses (Use Class C2, C3 and student/special needs/sheltered housing), a full range of town centre uses including Use Classes A1 - A5, offices, industrial and other business uses within Use Classes B1 - B8, leisure uses, rail based freight facilities, waste handling facility and treatment technology, petrol filling station, hotel and conference facilities, community, health and education facilities, private hospital, open space and public realm, landscaping and recreation facilities, new rail and bus stations, vehicular and pedestrian bridges, underground and multi-storey parking, works to the River Brent and Clitterhouse Stream and associated infrastructure, demolition and alterations of existing building structures, CHP/CCHP, relocated electricity substation, free standing or building mounted wind turbines, alterations to existing railway including Cricklewood railway track and station and Brent Cross London Underground station, creation of new strategic accesses and internal road layout, at grade or underground conveyor from waste handling facility to CHP/CCHP, infrastructure and associated facilities together with any required temporary works or structures and associated utilities/services required by the Development (Outline Application).

Both the 2010 and 2014 permissions were subject to Environmental Impact Assessment.

The transport aspects of the approved BXC planning permission include the creation of new strategic highway accesses, a new internal road layout, infrastructure and associated facilities together with temporary works, structures and associated utilities/services required by the development. Rail based measures include provision of a new railway station and freight facilities. A new bus station is planned, together with vehicular and pedestrian bridges, underground and multi-storey car parking. Works to the River Brent and Clitterhouse Stream and associated infrastructure are also included together with improvements to Brent Cross London Underground Station. Pedestrian and cycle connectivity will be improved with the provision of the Living Bridge over the A406 North Circular Road which will provide better integration between the northern and southern components of the development.

The following transport documents were issued in support of the 2014 S73 application:

- BXC05 Volume 1 Consolidated Transport Assessment Main Report;
- BXC05 Volume 2 Consolidated Transport Assessment Appendices;
- BXC05 Volume 3 Consolidated Transport Assessment Travel Plans;

- BXC05 Volume 4 Consolidated Transport Assessment Highway Engineering Proposals;
- BXC05 Volume 5 S73 Transport Report; and
- BXC05 Volume 6 S73 Highway Engineering Report.

2.2 Phasing of the BXC Regeneration Scheme

The 2014 S73 Consent is a multi-phase scheme for delivery over a period of 16 years. The permission proposes the phased delivery of acceptable comprehensive development for the whole site in accordance with the planning policy.

Phase 1 is proposed to be delivered in sub phases which are divided between north and south. This is to reflect the new delivery responsibilities including the Council's intention to procure a new development partner for the area south of the A406. The sub phases are as follows:

- Phase 1A (North) – this includes all the highways infrastructure to support the northern development including the key highways infrastructure to support the Phase 1 South, such as the improvements to the southern junctions of the A5/A407 Cricklewood Lane and the A407 Cricklewood Lane/Claremont Road Junction improvements. In addition the River Brent re-routeing and Bridge works will be delivered as part of Phase 1A (North), along with the Clitterhouse Playing Fields Part 1 (excluding the Nature Park) and the Claremont Park Improvements. The Living Bridge is included in (and its details will be approved before the commencement of) Phase 1A (North). Under the Revised Section 106 Agreement, its delivery will be triggered by the commencement of Phase 1B (North) and its delivery will be programmed to commence and be completed no later than before the occupation of Phase 1B North plots.
- Phase 1A (South) – A number of highway improvements needed to support Phase 1 of the Southern Development will be provided including the Waste Handling Facility (Diverted Geron Way/A5 junction; Claremont Park Road (Part 1); School Lane Works. In addition Waste Handling facility Rail Sidings and Gantry Craneworks and Threshold spaces at Layfield Place, Fenwick Place and Templehof Circus and Access to Plot 28 would come forward.
- Phase 1B (North) – This includes all of the plot development on the north side with the exception of the residential development within the Brent Cross West Zone. The sub phase also includes the new bus station, reconfigured shopping centre, Brent Cross Main Square, High Street North and other northern pedestrian routes, as well as the Riverside Park, Sturgess Park Improvements and around 300 housing units. Commencement of this Sub-Phase will trigger the BXP's obligations to deliver the Living Bridge which will link into the buildings

and public realm to be provided on the Plots forming part of this Sub-Phase.

- Phase 1B (South) – This includes the Market Square, the Clarefield Park Temporary Replacement Open Space, the replacement food store, the Waste Handling facility, the CHP and the new and expanded Claremont School, in addition to more than 1000 residential units.
- Phase 1C – This will include the remaining plot development on the south side.

2.3 Phase 1A (North)

This application is one part of the Phase 1A (North) Reserved Matters applications.

Phase 1A (North) is largely an infrastructure phase. It includes necessary highways infrastructure to support the northern development as well as improvements to critical southern junctions including A5/A407 Cricklewood Lane and the A407 Cricklewood Lane/Claremont Road Junction improvements. The infrastructure required relevant to the River Brent re-routeing and Bridge works are also delivered as part of Phase 1A (North), along with the Living Bridge, Replacement Tempelhof Bridge, Clitterhouse Playing Fields Part 1 (excluding the Nature Park) and the Claremont Park Improvements.

The highways and infrastructure improvements to the gateway junctions of the M1/A406/A5, Brent Cross A406 Ingress/Egress, A406/A41, A5/A407 and A407/Claremont Road will also be delivered in Phase 1A (North). However, the design of these gateway junctions has already been approved as part of the 2014 Section 73 Consent. The plan at **Appendix 8** highlights the approved Gateway Junctions in yellow and the road and bridge infrastructure proposed under this RMA in red.

The reserved matters for Phase 1A (North) have been broken down into four separate reserved matters submissions due to the size, scale and complexity of this initial sub phase of the Brent Cross Cricklewood Regeneration scheme.

The reserved matters that make up Phase 1A (North) are shown in the table below:

Table 1.0

No.	Summary Description	Planning Reference	Status
1.	The residential development of Plots 53 and 54.	15/00720/RMA	Approved at Committee on 18 th May, 2015.
2.	The Open Space Improvements of Clitterhouse Playing Fields	15/00769/RMA	Approved at Committee on

	(Part 1) and Claremont Park		25 th June, 2015.
3.	Open Space proposals for Central Brent Riverside Park	15/03315/RMA	Re-Submitted in June 2015 and the subject of a separate report before Members at this committee.
4.	Infrastructure Proposals including Roads and Junctions, Tempelhof and Living Bridges and diversion of the River Brent corridor.	15/03312/RMA	Re-Submitted in June 2015 and the subject of this report.

Four Reserved Matters Applications were submitted to London Borough of Barnet for Phase 1A (North) in January, 2015. However as a result of discussions with the Development Partners, it was agreed that the Infrastructure and Central Brent Riverside Park Reserved Matter Applications would be withdrawn prior to registration whilst the designs and supporting information was progressed further. These two RMAs were subsequently submitted together in June 2015.

2.4 Pre-Reserved Matters Conditions

The 2014 S73 Consent for the Brent Cross Regeneration project includes a number of Pre-Reserved Matters conditions intended to establish key principles of the forthcoming development. The majority of these require submission of reports and strategies prior to applications for reserved matters being submitted to the Council. Reserved Matters applications are required to accord with commitments and strategies approved under these conditions where relevant. The relevant Pre-RMA Conditions related to this application are discussed in section 5.2 and detailed in Appendix 4.

The wording of these conditions does not specifically require their discharge prior to the approval of Reserved Matters but in some instances there is a direct material relationship between the details of the reserved matters submission and the principals being captured within the conditions.

Appendix 4 of this report shows those 2014 S73 Consent Pre Reserved Matters Conditions for Phase 1A (North) which have relevance to the Infrastructure RMA but have yet to be formally determined. At the time of writing this report, the necessary information needed to satisfy these conditions has been submitted and agreement reached with Officers but conditions may not have yet been formally discharged. An update on the discharge of these conditions will be provided in the Addendum.

Condition 1.31 allows for the determination of relevant reserved matters applications in circumstances where conditions have yet to be approved:

“Where in these conditions any document strategy plan or other document or information is required to be submitted prior to any Reserved Matters Application or Other Matters Application required under this Permission, the grant of the relevant Reserved Matters Approval(s) or Other Matters Approval(s) shall be subject to the LPAs prior or simultaneous approval or confirmation (as the case may require) of the relevant document strategy plan or other document or information.

Reason: To ensure that an accurate and consistent interpretation is applied in the application and enforcement of these Conditions and the corresponding provisions in the S106 Agreement.”

As the relevant aspects of these conditions are acceptable to the LPA in the development of the Infrastructure RMA, Members are in a position to make a decision prior to the formal approval of these outstanding conditions.

3. DESCRIPTION OF THE SITE, SURROUNDINGS AND PROPOSALS

3.1 Site Description and Surroundings

The application site currently comprises car parking and associated urban form related to Brent Cross Shopping Centre. The wider site comprises a mix of different land uses broken up by major road and rail infrastructure. As part of the wider Brent Cross Cricklewood Regeneration proposals (BXC) the River Brent will be diverted from its current channelled/canalised course to create a more naturalised meandering river course.

The consented development site is located in the southern part of the London Borough of Barnet. The site is bounded by Park Road to the north, the A41 to the east (including Brent Cross Underground Station), the A5 and the border with London Borough of Brent to the west and the A407 Cricklewood Lane to the south.

Parking in and around the site is currently provided by way of controlled on-street zones, charged on-street bays, charged public off-street car parks, free off-street car parks and extensive free on-street car parking particularly in residential areas south of the A406.

A range of bus services, operated on behalf of TfL, pass through or close to the BXC site. The area is well served by the bus route network, with frequent services to a variety of destinations in London. The majority of the bus services start from or pass through the bus station at BXSC. This bus station serves the shopping centre and also operates as a local bus hub. Other bus hubs in the vicinity are located at North Finchley and Golders Green.

The Midland Mainline railway corridor passes through the western edge of the BXC site. The BXC site is served at the southern end by the existing

Cricklewood Railway Station. Hendon Station is approximately 3km to the north, outside of the BXC site boundary.

The Edgware branch of the Northern Line passes to the east of the BXC site and the Jubilee Line passes to the southwest through Willesden Green and West Hampstead. Brent Cross Underground Station is nearest the site and is located to the southeast of the A406/A41 junction. To the north, Hendon Central is approximately 750metres from BXSC.

The existing facilities for pedestrians are in general poor. Facilities for cyclists are somewhat better, with a number of signed routes and areas of both shared surface facilities and cycle lanes. However cycle parking facilities in the Brent Cross area remain limited and cyclists still face traversing a number of busy junctions and highways.

Appendix 3 provides a full description of the existing transport conditions for the site and surrounding area.

3.2 Description of the RMA Proposals

As explained at section 2.3 and table 1.0 above, Phase 1A (North) comprises four separate RMAs covering infrastructure, enhanced open spaces provision, the delivery of residential development plots and the provision of the River Brent Riverside Park.

A separate RMA for the Central Brent Riverside Park (15/03315/RMA) which provides details of the proposed landscaping and ecological enhancements for the Central section of the proposed Riverside Park, including public access and maintenance access provision, is also before the Committee for consideration.

The application which is the subject of this report (15/03312/RMA) seeks reserved matters approval for the detailed road layout and network within Phase 1A (North) for vehicles, cyclists and pedestrians which provide connections to the gateway junctions; the diversion works for the River Brent to enable the future extension of the shopping centre; and the provision of bridges across the A406 and diverted River Brent, including the replacement Tempelhof bridge and proposed Living Bridge. The proposals complete the networks for the Northern development and provide sufficient connections across and to the south of the A406 to facilitate the start of the Southern development. The proposals are briefly summarised in this section, with a fuller discussion provided in section 5. More detail can be found in Appendix 2. The plan at Appendix 8 highlights the approved Gateway Junctions in yellow and the road and bridge infrastructure proposed under this RMA in red

As a result of the proposed Phase 1A (North) road layout, vehicles will have new and improved access to and from the BX Shopping Centre, as well as to the Southern development. The key changes include a new slip road directly from the improved western roundabout onto the A406 and a new slip road for

traffic coming off the A406 from the west connecting via the A41 junction to the new eastern roundabout. Traffic from the A406 east will be able to access the BXSC directly via the A41 junction, thus avoiding Tilling Road and Tempelhof Bridge. Traffic from the A41 south similarly benefits. On the southern side shopping centre traffic using Tempelhof Bridge has a more direct route to and from Claremont Road avoiding Tilling Road altogether.

The Tempelhof Bridge and its approach roads on both sides of the A406 are being widened to provide additional capacity, particularly for pedestrians, cyclists and buses. In addition the junctions of Tilling Road with Brent Terrace North, Tilling Road with Claremont Avenue, Market Square / Claremont Avenue / School Lane and Claremont Road / Claremont Avenue / Claremont Park Road / Orchard Lane are all being provided with new traffic signals, and the roundabout west of the Holiday Inn is being improved. Southern development plots will be accessed in due course off Brent Terrace North, Tilling Road, School Lane and Claremont Park Road. There are also improvements for pedestrians, cyclists and bus users, as discussed later in this report.

Nearly all the new Phase 1A (North) roads and junctions are being designed with enough capacity to accommodate the predicted traffic levels for the whole development at end-state. The only exception is Tilling Road west of the Holiday Inn which is only being improved to accommodate the early phases. Otherwise, the network created in Phase 1A (North) provides all necessary connections to facilitate the rest of the northern development and in addition, some of the key roads south of the A406. Key elements of the road network for the Southern development are provided for, including the western end of School Lane (which will eventually connect to Whitefield Avenue when complete); the eastern end of Claremont Park Road (which will extend in a future phase to link with Spine Road North); and the corner forming Market Square / New Tempelhof Avenue which will be the point at which High Street South joins. The RMA for sub-phase 1A South is anticipated soon and will include some additional roads in the southern area to further facilitate the southern development.

The Phase 1A (North) part of the pedestrian and cycle network is also provided. The new diverted Prince Charles Drive will provide vehicle links to the new bus station and basement servicing between the western and eastern roundabouts. The bus station and basement servicing area will be provided as part of the BXSC in sub-phase 1B North.

It should be noted that the phasing of improvements in the vicinity of the A41 requires an interim highway layout until the A41 / Whitefield Avenue junction is built in phase 2. Traffic from the A41 south will be unable to access the dedicated left turn slip lane onto the A406 as the existing connection to Tilling Road will need to be maintained until the left turn at the new Whitefield Avenue junction is available. Traffic will be required to travel via the main signalised roundabout. For traffic travelling in the opposite direction, from the eastern part of the development to the A41 south, until the A41 / Whitefield

Avenue junction is built the route will be Tilling Road, Brentfield Gardens and via short parts of Highfield Avenue, Hamilton Road and Woodville Gardens.

A plan showing the location of the key roads and junctions that are part of this RMA, together with the gateway junctions is provided in Appendix 2.

The proposed River Brent diversion works will, in broad outline, create a meandering river course that will re-connect at the eastern and western ends of BXC into the existing river course. The River Brent diversion runs east to west from the A41 Hendon Way, following the route of the A406, North Circular to its junction with the M1. The opportunity arises to create a naturalised parkland riverside environment with marginal and wetland landscaping throughout the river channel, improved biodiversity throughout and ultimately improved water quality. The riverside park elements are provided in three separate reaches. The reaches reflect the different character of the adjoining development areas. Reaches 1 and 2 are situated adjacent to the Shopping Centre and this is reflected in their narrower width and desire to create a landscaped setting for the diverted river as well as the Shopping Centre. Reach 3 on the other hand is in an area that is closer to residential property and the opportunity here is to create a more expansive ecological habitat area where the public has greater access to both banks of the river.

The reserved matters submission under consideration in this report therefore provides details of site layout, scale, appearance, access and landscaping in for infrastructure including roads, bridges and River Brent diversion works comprised in Phase 1A (North) of the Brent Cross Cricklewood regeneration scheme.

The application is supported by the following documents:

- Application Covering Letter
- Explanatory Report
- Reserved Matters Transport Report
- Environmental Statement Further Information Report
- Landscape Infrastructure Design Development Report
- Bridge Design Development Report
- Temporary Bus Station Design Report
- Flood Risk Assessment Addendum
- Illustrative Reconciliation Plan
- Statement of Community Involvement.

4. MATERIAL CONSIDERATIONS

4.1 Key Relevant Planning Policy

In this case, the Development Plan comprises the London Plan (Consolidated with Further Alterations since 2011) (March 2015) at the strategic level and, at the local level, Barnet's Local Plan (Core Strategy (2012)) and the Saved UDP Policies GCRICK and C1-C11, which apply to the application site and are supplemented by the Cricklewood, Brent Cross and West Hendon Regeneration Area Development Framework (2005).

The Council's Development Management Policies DPD (2012) states at paragraph 1.4.3 that it will not apply to planning applications for comprehensive development in the Brent Cross unless and until the Core Strategy is reviewed in accordance with Policy CS2 and Section 20:13 of the Core Strategy.

Detailed consideration of the application against key London Plan and London Borough of Barnet policies can be found in Appendix 5. In summary the application is considered to be in accordance with relevant planning policies. The application is for matters reserved following the grant of the outline planning permission under the 2014 permission and as such the policy considerations and principles have previously been considered and have been found to have been met.

National Planning Policy Framework

The 'National Planning Policy Framework' (NPPF) was published on 27 March 2012. This is a key part of Government reforms to make the planning system less complex and more accessible, and to promote sustainable growth.

In March 2014 the National Planning Practice Guidance was published (online) as a web based resource. This resource provides an additional level of detail and guidance to support the policies set out in the NPPF.

4.2 Public Consultations and Views Expressed

Public Consultation

4,177 local residents were consulted by letter. The application was advertised in the local press on 11th June 2015 and 17 site notices were put up on site on 11th June 2015. The consultation letters allowed a 5 week period to respond. 5 letters of objection were received in response to this initial consultation period. Statutory consultees and other interest groups were also consulted on the application.

Following the receipt of amendments and clarification to the submitted plans and documentation; 4,177 local residents were re-consulted by letter on 21st July 2015. The letters allowed a three week period for the re-consultation

period expiring 12th August 2015. The application was re-advertised in the local press on the 22nd July 2015 and 18 site notices were put up on site on this date. Statutory consultees and other interest groups were also re - consulted.

10 Letters of objection from residents were received in response to this second consultation period.

A summary of the objections and comments received and officer comments in response can be found under Appendix 6 of this report.

The consultation process carried out for this application is considered to be appropriate for a development of this nature. The extent of consultation exceeded the requirements of national planning legislation and the Council's own adopted policy.

The S73 Consent includes a requirement under Condition 1.23 for submission of a Public Consultation Strategy This was submitted to the Council and considered under planning reference 14/07891/CON. It was approved on 31 March 2015. Details of the developer's own consultation process are set out in the Statement of Community Involvement submitted with the Phase 1A (North) Reserved Matters Submission and accorded with the requirements of the approved strategy.

Consultation Responses from Statutory Consultees and Other Bodies

A summary of the comments received from statutory consultees and other bodies and officer comments in response can be found under Appendix 6 of this report.

Environment Agency:

The Environment Agency have assessed the Infrastructure proposals in conjunction with the parallel RMA for the Brent Riverside Park and have provided comments that respond jointly to both applications given the interrelationship of the river diversion with the riverside park landscaping. The EA have reviewed the applications from the perspectives of the impacts on the ecological environment, the Water Framework Directive (WFD), flood risk and the risks of surface water pollution. More detail on their comments is provided in Appendix 6.

The EA have confirmed that they do not object to the approval of the Infrastructure RMA or the Central Brent Riverside Park RMA subject to securing a S106 financial contribution towards procurement of Ecological Environment and Water Framework Directive improvements to a stretch of the Mutton Brook within Barnet; and subject to the imposition of a number of conditions relating to the construction of the river channel, installation of furniture within the riverside park, and construction method statement to demonstrate how pollution to surface water will be prevented. Appropriate conditions and informatives are included in Appendix 1 to this report.

Transport for London:

TfL does not object to the approval of this Reserved Matter Application; subject to the comments and advice summarised in Appendix 6 of this report.

London Fire Brigade:

No objection Raised

Highways Agency:

No objection Raised

Brent Council:

Whilst Brent Council have raised concerns with regards to the A5 Corridor Study (14/07402/CON) and the Area Wide Walking and Cycling Study (14/08105/CON); no objections have been raised to the approval of this Reserved Matters Application.

Greater London Authority:

No objection Raised

Thames Water:

No objection Raised

Consultative Access Forum (CAF):

The Consultative Access Forum has assessed the Infrastructure proposals in conjunction with the parallel RMA for the Brent Riverside Park and have provided comments that respond jointly to both applications given the interrelationship of the river diversion with the riverside park landscaping. The Consultative Access Forum does not object to the approval of the Central Brent Riverside Park RMA and have provided comments regarding inclusive access to the Riverside Park including in relation to the pedestrian/cycle ramp and lift access to the lower level riverside walkway. Their comments are summarised in Appendix 6.

Internal Consultation responses**Environmental Health:**

The EH officers have reviewed the Infrastructure RMA information and have recommended conditions in relation to the hours of operation of the proposed temporary bus stops on Plot 113. Subject to the imposition of these conditions EH have no objections to the approval of the Phase 1A (North) Infrastructure RMA.

4.3 Application under Conditions 2.4 and 2.5 for the variation to the provision of open spaces (ref: 15/05040/CON)

Conditions 2.4 and 2.5 of the 2014 permission allow for amendments to be made to the DSF, DAS and DG where changes brought about, through for example a Reserved Matters Application, necessitate such amendment.

An application (reference 15/05040/CON) has been submitted under condition 2.4 and 2.5 to vary a number of provisions relating to the principal Infrastructure RMA and also to the Central Brent Riverside Park RMA. Full detail of the changes proposed is provided in a separate report also before this committee.

In relation to the Infrastructure RMA, the application proposes the following changes:

- Reducing the size of Brent Riverside Park to 3.07ha (from 3.1ha);
- Reducing the size of Nature Park 5 to 0.13ha (from 0.2ha) with a corresponding increase in the size of Nature Park 4 (from 0.2ha to 0.27ha);
- Alterations to the width measurement for roads to back of pavement to back of pavement (rather than building line to building line);
- Minor changes to scale thresholds (length, width and height) for the River Brent Bridges;
- Minor changes to the access ramp in the river corridor;
- Reduction in the minimum width for the pavements to 2.5m (save for Tempelhof Link Road);
- Reduction in the number of intermediate piers at Bridge B1 (Tempelhof Bridge); and
- Confirmation that the lift access from the Living Bridge will connect to the lower ground bus station level and the lower riverside walkway level.

In addition, a related Section 96a application (reference 15/04734/NMA) has also been submitted to the local planning authority which seeks to amend definitions to the Planning Permission as a result of this submission under Condition 2.4 and 2.5.

The changes are minor in nature and a number are described in more detail as follows.

The Revised Development Specification and Framework (“RDSF”) includes a number of approved Parameter Plans including those that relate specifically to infrastructure. Parameter Plan 002 relates to Transport Infrastructure and the supporting text requires a maximum building line to building line width of 28m and a minimum width of 13m to include a pedestrian footpath of 3m minimum width. In addition Parameter Plan 003: Public Realm and Urban Structure requires a minimum distance of 10m between buildings. The requirements reflected assumptions that in all cases buildings would face Primary Routes.

The design process however has shown that this will not always be the case. With regard to the Living Bridge southern approach for example buildings will not frame both sides of Claremont Avenue and the road width consequently exceeds the maximum building line to building line threshold. The change in Parameter Plan 002 supporting text consequently seeks to provide a

threshold relating to 'back of pavement to back of pavement' measurement width of 28m and a minimum width of 13m with pedestrian footways a minimum of 2.53m ,as opposed to a building line to building line measurement. The exceptions are Tempelhof Avenue which will have a maximum width of 34m, Claremont Avenue which will have a maximum width of 31m and Tempelhof Link Road which will have a minimum pedestrian width of 2m.

Parameter Plan 011 relates to the development within Brent Riverside Park and defines for example the general location of road and pedestrian/cycle bridges, the height threshold of the bridges and bridge length. Due to an increase in the width of Prince Charles Drive, to ensure that the highway meets the requirements of the relevant Highways authority, the width of Riverside Park has reduced within its central section. The riverside park width continues to accord with the 2014 permission however the Parameter Plan definitions relating to bridge length and head height have been amended to reflect the reduction in bridge span due to the reduction in overall width in the riverside park and a marginal increase in height. In addition minor design changes amend the general location of some of the road bridges. The detail of the Environment Agency access ramp within Riverside Park has also altered to reflect these design changes.

The detailed design process for the road infrastructure has resulted in a roadway being driven through the western internal roundabout. This roadway is to be delivered as an embankment structure and cuts through an area defined in the 2014 Permission as Nature Park NP5 as defined on Parameter Plan 003. In order for the proposed section of road to be accommodated it is necessary to reduce the area of NP5 from 0.2ha to 0.13ha. This is addressed in the Central Brent Riverside Park RMA report (reference 15/03315/RMA) also before the Members at this committee.

In order to compensate for the loss of 0.07ha from NP5 it is proposed to increase the area of Nature Park NP4 within the western section of the River Brent from 0.2ha to 0.27ha. In addition to this minor amendment, as a result of the widening of Prince Charles Drive, the river corridor has been slightly reduced in size by 0.1ha from 3.1ha to 3.07ha. This reduction is compensated via a financial contribution of £200,000 which has been agreed with the Environment Agency and is discussed in more detail in the Central Brent Riverside Park RMA report (15/03315/RMA) also before the Members at this committee. In light of the above changes it is proposed that Table 5, within page 43 of the RDSF, as well as Table 4 in Appendix 2 will be amended to reflect these changes.

The nature of the changes proposed under conditions 2.4 and 2.5 are all considered acceptable by officers. These changes are formalised through application 15/05040/CON which is also before Members at this Committee for approval. Approval of the application will ensure that the RMA application is consistent with the development's supporting documents.

5. PLANNING AND TRANSPORT ASSESSMENT

5.1 Assessment Against Parameters of the 2014 Section 73 Consent

The 2014 S73 Consent for Brent Cross Cricklewood is a 'hybrid' permission, in that planning permission has been granted in outline for the majority of the proposed development, whilst detailed permission has been granted in relation to the key gateway access junctions, namely:-

1. M1/A406 and A5/A406 (Staples Corner)
2. A41/A406
3. A5/A407 Cricklewood Lane and Chichele Road
4. A407 Cricklewood Lane/Claremont Road/Lichfield Road
5. A406 Brent Cross Ingress/Egress Junction
6. A41/Whitefield Avenue
7. A5/Humber Road/Waste Handling Facility
8. A5/Rail Freight Facility
9. A5/Dollis Hill Lane/Oxgate Gardens/Midland Mainline Bridge.

The first 5 of the above list of junctions fall within Phase 1A (North) of the development, whilst the remainder are associated with later phases of the (southern) development.

A series of Parameter Plans were approved under the Section 73 Consent which in conjunction with the conditions, Section 106 legal agreement and approved application documents, in particular the Revised Development Specification and Framework (RDSF), revised Design Guidelines (RDG), revised Public Realm and Open Stage Strategy (PROSS) and a revised Design and Access Statement (RDAS), provide the framework to control and assess reserved matters. They also control the nature and timing of reserved matters applications to accord with the assessments undertaken within the EIA and the TA of the likely significant environmental and transport impacts of the Scheme.

The relevant Parameter Plans approved in the RDSF, in relation to Infrastructure are:

Parameter Plan 002: Transport Infrastructure;
Parameter Plan 003: Public Realm and Urban Structure; and
Parameter Plan 011: River Brent.

Parameter Plan 002 identifies various infrastructure elements proposed as part of the comprehensive site development at BXC including defining the location of existing and proposed junctions and highways, defining primary routes and the approximate location of secondary and tertiary routes including building and development zone access points, vehicle and pedestrian bridges, transport interchange points and highways circulation.

Parameter Plan 003 identifies the network of new and existing public spaces

and routes between them for pedestrians and cyclists including primary circulation corridors for pedestrians and cyclists and secondary and tertiary routes.

Parameter Plan 011 defines the proposed corridor zone for the re-aligned River Brent including connections into the existing river. The plan identifies the new riverside park and access points including crossing points for highways infrastructure as well as for pedestrians and cyclists. The plan also defines three distinct reaches within the realigned River Brent corridor.

The 2014 S73 Consent is tied to those key parameters and principles in order to ensure that that proposed development is carried out, used and occupied in accordance with the assumptions which underpin the EIA Process. The current RMAs have been considered under the requirements of the EIA Regulations and it has been agreed that the assessment of the proposals under the Regulations can be dealt with by way of an Environmental Statement Further Information Report (ESFIR). This is covered in more detail in section 6 of this report.

The RDSF sets out the updated physical and other parameters and principles to guide and govern the subsequent design and approval of details in accordance with conditions attached to the 2014 S73 Consent. The RDSF identifies aspects of the proposed scheme that fall within the parameters and principles approved under the 2014 S73 Consent, and those that do not (the Reserved Matters) which are therefore subject to application to obtain Reserved Matters approval.

As with other large-scale redevelopment schemes, the need for flexibility was anticipated in framing the 2010 and 2014 permissions in a way which was specifically designed so as to allow the BXC Development to evolve and respond to market forces and opportunities, as well as to enable improvements to be made to the design and delivery of the development in accordance with relevant development plan policies and other guidance. Conditions 2.4 and 2.5 of the 2014 S73 Consent therefore allow applications to be submitted to vary the content and development parameters set down in the DSF, DAS and DG documents.

An assessment of conformity with the RDS F including the relevant parameter plans is provided in Appendix 7 of this report.

It is considered that the infrastructure proposals for new roads and bridges and the diversion of the River Brent conform to the Parameter Plans and 2014 S73 Consent documents, as modified by the parallel application under conditions 2.4 and 2.5 (reference 15/05040/CON – see Section 4.3 above). It should be noted that there are a range of other conditions attached to the Section 73 Consent that require the submission and approval of further detailed information prior to the commencement of any work starting on site. These include, but are not limited to, delivery programme, drainage, construction environmental management plan, construction transport management, site waste management plan, noise and vibration monitoring,

dust monitoring and control, air quality monitoring, archaeological investigation, detailed design of bridges etc.

5.2 Pre-Reserved Matters Conditions

The planning conditions require a number of transport strategies, reports and feasibility studies are submitted to LBB prior to submission of the first RMAs for the Development. These conditions are known as Pre-RMA conditions, and relate to overall transport strategies affecting the whole development, as well as information required that relates to the whole of Phase 1 and information required for just Phase 1A (North). The relevant transport Pre-RMA conditions comprise the following:-

Condition 1.9	Construction Consolidation Centre Feasibility Study
Condition 1.20	Area Wide Walking and Cycling Study (AWWCS)
Condition 1.21	Framework Servicing and Delivery Strategy
Condition 1.22	Phase 1A (North) Servicing and Delivery Strategy
Condition 2.7	A5 Corridor Study
Condition 2.8	Pedestrian and Cycle Strategy for Phase 1A (North)
Condition 7.1	Estate Management Framework
Condition 11.1	Car Parking Management Strategy
Condition 11.2	Phase 1 Parking Standards and Strategy
Condition 37.3	Phase 1 Transport Report
Condition 1.17	Illustrative Reconciliation Plan

With the exception of the A5 Corridor Study which is the subject of a separate report before this committee, each of these conditions are only briefly discussed below, but further detail is set out in Appendix 4.

Condition 1.9 Construction Consolidation Centre Feasibility Study

This condition relates to the review of the Construction Consolidation Centre provision and / or alternative logistics facilities to provide a management method for enabling the safe and efficient flow of construction materials and equipment from suppliers to relevant development sites within the Northern Development and / or Southern Development.

The key objective of the study is to reduce the number of construction vehicles on the highway network surrounding the development by identifying measures to provide a managed flow and the distribution of construction vehicles onto the existing main road network.

The review includes potential use of existing facilities and any new facilities which may be required to reduce the impact on the local infrastructure and the environment. The review also looks at the associated vehicle movements during Phase 1, and how these can be managed and reduced, especially in the peak periods.

The key findings from the study are:

- A Construction Consolidation Centre is unlikely to be required and certainly not for Phase 1 as it is envisaged that very few of the vehicles attending site prior to the final stages of fit-out arrive less than 90% full.

A potential rail based solution is problematic as there is minimal storage land adjacent to Hendon Rail Transfer Station, whilst the Euro Storage Site on the opposite side of the railway would require all vehicles to travel through the already congested Staples Corner and A5.

A number of locations for a new logistics centre and off site supporting facilities (warehousing / storage / existing commodities and batching) have been identified indicating a number of feasible options, which will be reviewed in more detail as the development progresses.

The full logistics plan for the construction of Phase 1 will form part of the Construction Transport Management Plan which will be produced by the developer and submitted for approval prior to the commencement of works on site.

Condition 1.20 Area Wide Walking and Cycling Study (AWWCS) and Condition 2.8 Pedestrian and Cycle Strategy for Phase 1A (North)

The Area Wide Walking and Cycling Study reviews the pedestrian and cycle connections between the development site, key destinations and local residential areas. The study forms the basis of a delivery programme of potential schemes for improvements to pedestrian and cycle facilities adjacent to or beyond the site boundary, improving access both to and from the Brent Cross site.

The key proposals of the AWWCS are a series of improvements to integrate the development into the existing network, secured via a developer contribution of £1,250,000 towards routes providing connections to Brent, Camden and other locations in Barnet including Hendon (Brent Street), Hendon Central tube station, Hendon railway station, West Hendon, Temple Fortune, Golders Green and Childs Hill.

The London Borough of Brent objected to the AWWCS, key concerns being that the measures proposed were too limited to provide a sufficient incentive for people to walk or cycle and that the measures did not mesh with LB Brent's own cycle route plans and proposals. However, officers consider that the agreed measures arising out of the AWWCS does provide sufficient proposals to comprehensively improve key local walk and cycle routes in the vicinity of the BXC development opportunity. The £1,250,000 mentioned above includes a £300,000 contribution towards a separate cycle route requested by LB Brent between BXC and Wembley.

The key elements of the Phase 1A (North) Pedestrian and Cycling strategy are provision of key connection points including via Sturgess Park, Claremont Park, Clitterhouse Playing Fields (to the proposed Mayoral Cycle Superhighway 11 along the A41 corridor), Staples Corner and across the A41

via existing improved subways at Haley Road and Ridge Hill, and via the A41 / A406 junction. There is also improved access across the A406 for pedestrians and cyclists with the Living Bridge, a new bridge at Staples Corner and the improved replacement of Tempelhof Bridge.

Condition 1.21 Framework Servicing and Delivery Strategy and Condition 1.22 Phase 1A (North) Servicing and Delivery Strategy

The Framework Servicing and Delivery Strategy provides a structure for the consideration of servicing and delivery issues at subsequent stages in the planning consents process and during construction and operation of the development.

Each phase or sub-phase of development requires the preparation of a Servicing and Delivery Strategy and will be in operation from first occupation until all elements of the phase or sub-phase are fully developed.

As regards the Phase 1A (North) Servicing and Delivery Strategy there are only very limited servicing and delivery requirements for the development within this phase which relate to the residential development on Plots 53 and 54 (Brent Terrace) and the open space proposals for Clitterhouse Playing Fields which include a small kiosk style café and the park maintenance depot which will require some deliveries / servicing.

Condition 2.7 A5 Corridor Study

The A5 Corridor Study is one of the Conditions (2.7) that under the Section 73 2014 Section 106 agreement must be considered before or at the same time as the RMA that is the subject of this report. The A5 Corridor Study is subject of a separate report before Members at this meeting and includes an assessment of any local impacts of the BXC scheme, including in the adjoining areas within LB Brent and LB Camden, and builds on the outline scope contained in the 2010 Section 106 Agreement for the 2010 consented scheme. The full scope for the study has been agreed with the relevant transport authorities and encompasses a detailed model of the A5 and an assessment of all transport facilities along the corridor. The assessment of local impacts uses a new BXCDDM traffic model. The Developers are required under the Revised S106 Agreement to fund any supplementary mitigation measures in Barnet, Brent and Camden that may be identified as part of the study, and the separate report provides full details of the agreed measures and funds arising out of the study, including several junction improvement schemes and if monitoring of traffic flows shows increase due to the development, an additional capped contribution of £300,000 towards future Supplementary Transport Measures has been agreed with the Brent Cross Development Partners (letter dated 2nd September 2015) and will be secured via the existing clauses within the section 106 agreement.

It should be noted that the LB Brent and LB Camden both objected to the A5 Corridor Study (although this was prior to the funding of the study proposals being agreed with the Developers). LB Brent expressed concerns about a lack

of mitigation measures along the A5 and in Brent itself, and parking pressures in the Dollis Hill area. LB Camden are also concerned about the identification of traffic and parking impacts in Camden. The full objections and detailed responses are contained in the separate report before Members, which explains how concerns about traffic and parking impacts, both predicted and potential, are proposed to be dealt with.

Officers are satisfied that the study has been carried out in accordance with the agreed and approved scope, and that the key impacts have been correctly identified and assessed. A package of supplementary mitigation measures and additional contributions has been agreed which is considered to adequately address expected impacts, as well as providing additional safeguards should additional supplementary mitigation be needed in future phases.

Condition 7.1 Estate Management Framework

The Estate Management Framework submitted in respect of Phase 1A (North) anticipates a structure for Estate Management Companies to be established for each phase of this development and sets out some general principles.

Adopted roads and structures (such as bridges) will remain the responsibility of the relevant highway authority. The EMF includes a plan which illustrates ownership assumptions at the time of preparing the report. It is anticipated that the final agreement and apportionment of responsibilities will form part of the detail S38 and S278 highways agreements and any such agreement is likely to include a commuted sum for future maintenance purposes.

An exception to this will be the management and maintenance of the surface of the Living Bridge which will be maintained by the Brent Cross Development Partners as it forms one of the principal entrances to the expanded shopping centre.

An acoustic barrier is required to ensure that the Brent Riverside Park is protected from the noise of the adjacent A406 (north circular) and can meet suitable standards expected for a publically accessible open space. It is anticipated that this noise barrier may be within the public highway and it is expected that the Highways Authority will either give a license to the Brent Cross Cricklewood development partners to allow them to maintain the structure or the structure will be adopted as part of the public highway by the relevant highway authority and a commuted sum will be paid by the Developer to cover maintenance costs.

In addition to the acoustic barrier, the proposed scheme includes for some areas of grass, landscape planting, trees and possibly other private apparatus within the public highway.

These will be either provided and maintained by the Developer under license granted by the Highways Authority or if appropriate, adopted by the relevant

Highway Authority on provision of relevant commuted sum to cover future maintenance.

Condition 11.1 Car Parking Management Strategy and Condition 11.2 Phase 1 Parking Standards and Strategy

The Car Parking Management Strategy provides an overarching framework to ensure the effective management of car parking across the scheme so as to assist in encouraging modal shift away from private transport to minimise environmental impacts. The strategy identifies:

- The parking tariffs to be introduced at the shopping centre;
- Provision of blue badge bays; parent/child parking; electric vehicle parking; motorcycle parking and customer collection points;
- Use of Variable Message Systems to assist drivers accessing and egressing the car parks;
- Use of car clubs;
- Provision of Controlled Parking Zones within and outside the regeneration area; and

For development within Phase 1A (North), the main parking demand will be generated from residential development on Plots 53 / 54 (Brent Terrace) as well as a small amount of parking associated with the open space improvements. Parking associated with the infrastructure works is temporary and is considered separately in the Construction Workers Travel Plan which is required to be submitted under condition 12.2.

Condition 37.2 Phase 1 Transport Report

The Phase Transport Report examines the end state of Phase 1 (2021) based on the preliminary design of the approved transport works for the phase. Further detail is to be provided within the reserved matters transport reports which will subsequently be produced for each sub-phase (1A North, 1A South, 1B North, 1B South and 1C).

This phase proposes to deliver numerous infrastructure improvements ahead of other aspects of the development.

The Phase Transport Report for Phase 1 has the following objectives:

- Safe operation of the TfL Road Network.
- Safe operation of the bus network that serves the site.
- Improved reliability of the buses that serve the site.
- Improved pedestrian and cycle links to site, including the Mayor's Cycle Super Highway along the A41.
- Mode shift from car use to public transport use, walking and cycling.

- Improved accessibility of the Phase 1 development for public transport users.
- Transport measures approved at Phase 1 enable subsequent phases of development to come forward, with a focus on Phases 1 to 4.

Based on a review of the existing site accessibility, the Phase 1 proposals are shown to provide improved pedestrian and cycle connectivity in the area. The Area Wide Walking and Cycling Study provides details of a number of proposed improvements which are designed to integrate the development with the existing pedestrian and cycle networks and to provide comprehensive connections to the surrounding areas. A cycle hub will also be provided to make cycling a more attractive mode of transport for the area.

Phase 1 will also improve public transport in the area by delivering a new bus station which will be situated to the north of Prince Charles Drive. A number of new bus stops and new bus services will also be provided as part of the Phase 1 development which will be utilised by existing services and will enable additional and more frequent services to be delivered during future phases. The provision of bus priority, such as on Tempelhof Bridge, and the delivery of new infrastructure, including a new junction on the A41 and improvements to the underpass under Tilling Road, will also benefit the existing bus services operating within the vicinity of the development during Phase 1.

The Phase 1 proposals are designed to encourage visitors to travel to and from the shopping centre on foot, by bicycle or by public transport, with no increase in associated parking provision.

Additional highway modelling has been undertaken to inform the Phase Transport Report for Phase 1 and to robustly identify the impacts of the Phase 1 proposals, the proposed benefits of the mitigation proposed for Phase 1 and to compare journey times and delays for the combined traffic impacts of phases 1 to 4. A number of tests have been undertaken for Phases 1A (North), 1B (North) and 2 (North), the Phase 1 highway network and for future phases to understand how Phase 1 fits in relation to the wider scheme. The results show that the Phase 1 highway network will perform as well as or better than the existing network in the majority of cases as a result of the new infrastructure which will be delivered.

The Phase 1 transport assessment work included checks that show that the overall development quanta and resulting trips do not exceed the boundaries presented in the 2014 S106 Agreement and that the transportation impact and mitigation proposed is as forecast in the S73 Consent.

Discharge of the Above Pre-RMA Conditions

At the time of writing this report, the necessary information needed to satisfy the above conditions, except for the A5 Corridor Study which is the subject of a separate report before Members, has been submitted and agreed with Officers. An update on the formal discharge of these conditions will be

provided in the Addendum.

5.3 Phase 1A (North) Reserved Matters Transport Report

The Section 106 (S106) legal agreement that is attached to the 2014 S73 Consent along with conditions 37.5 and 37.6, requires a Reserved Matters Transport Report (RMTR) to be provided for each phase or sub-phase of the development that contains plot development. This section of the report is based on officers' review of the submitted Phase 1A (North) RMTR, and so examines the detailed transport issues relating to this phase of the BXC Regeneration scheme.

The S106 agreement attached to the S73 Consent includes 29 schedules, several of which are directly transport related. Schedule 17 relates to the Matrix and Transport Reports schedule, Annex 5 of which includes the draft scope for Reserved Matter Transport Reports. The RMTR scope is also controlled through Condition 37.1 and a modified and expanded RMTR scope has been approved under this condition.

Since the Section 73 application was approved in 2014 there has been on-going dialogue with the development partners and their advisors in relation to the transport infrastructure. The Transport Advisory Group (TAG) has been set up and constituted, as per schedule 12 of the S106, and there has been liaison with the neighbouring boroughs of Brent and Camden. The Phase 1A (North) RMA proposals are described below, followed by section 5.4 that discusses the detailed assessment.

The BXC S73 Consolidated Transport Assessment Main Report (document BXC05) sets out the baseline traffic information upon which the development's impacts were assessed using the BXC Transport Model (BXCTM), which dates from around 2006.

To provide more up to date information to support the continued development of the design, a series of traffic surveys were undertaken in June and July 2013 with additional surveys undertaken in October and November 2013. The surveys provided additional information to help build an updated traffic model of the area which the development partners and the transport authorities agreed would be used as part of the detailed highway approval processes. The model, known as the BXC Detailed Design Model (BXCDDM) uses TfL's latest sub regional transport model as a basis, but has a greater level of detail in the BXC area. The transport authorities have approved the BXCDDM and its use is explained in more detail later under section 5.4 of this report.

RMA Infrastructure Proposals

Overview - Integrated Transport Strategy

The 2014 Section 73 Consent defines the multi-modal Integrated Transport Strategy (ITS) which was developed to satisfy the transport needs and

mitigate the impacts of the BXC site. At the core of the ITS is the provision of a fully integrated and, as far as is achievable, sustainable public transport network of appropriate travel modes. These modes will provide a good quality and attractive level of service provision for those wishing to access both the BXC site and the surrounding area whilst at the same time maintaining the same level of service on the highway network so that drivers are no worse off than had the scheme not been developed. Officers are satisfied that the RMA proposals are consistent with and will contribute positively to the overall ITS.

Proposed New Roads and Junctions

The majority of the infrastructure improvements associated with the scheme will be delivered during Phase 1A (North) in preparation for the proposed development due to come forwards during the subsequent phases.

The 2014 S73 Consent granted full permission for improvement works at the following junctions and thus no reserved matters application is necessary:

- (a) M1/A406 and A5/A406 (Staples Corner)
- (b) A41/A406 Junction
- (c) A5/A407 Cricklewood Lane
- (d) A407 Cricklewood Lane/Claremont Road
- (e) A406 Brent Cross Access/Egress Junction

Details of these approved junctions are included in Appendix 2.

The following areas of highway and junctions are also proposed to be improved and completed during Phase 1A (North), and therefore these are the schemes for which approval is being sought as part of this RMA:

(1) Tempelhof Avenue and Link Road

The new Tempelhof Avenue will provide a link with the southern end of the replacement A406 Tempelhof Bridge. Northbound vehicles will access the bridge via High Street South (East Works) and Tempelhof Avenue. Southbound vehicles exit the bridge onto Tempelhof Avenue and then either turn onto the Tempelhof Link Road to access Tilling Road or continue southwards towards Claremont Avenue. The Tempelhof Link Road will have a traffic signal controlled junction with both Claremont Avenue and Tilling Road.

A segregated cycle / footway will run along the western side of Tempelhof Avenue extending along Claremont Avenue and Tempelhof Bridge. A bus lane will be provided on the northbound carriageway, extending over Tempelhof Bridge. There is also a southbound bus lane over Tempelhof Bridge and footway on the east of the bridge.

(2) Diversion of Prince Charles Drive

The diverted Prince Charles Drive will lie between the western and eastern roundabouts for the shopping centre and between the A406 to the south and

the River Brent to the north. Prince Charles Drive will provide access to the basement service area, the new bus station and future plots which come forward as part of sub-phase 1B North. Prince Charles Drive is for vehicles only, with no pedestrian or cycle facilities proposed as these are accommodated on the adjacent river corridor.

(3) Claremont Avenue

The new Claremont Avenue will lie between Claremont Road to the south and Tilling Road to the north with junctions at Claremont Road / Claremont Park Road, Orchard Lane and Tilling Road / Tempelhof Link Road. The new Claremont Avenue will have a traffic signal controlled junction at High Street South (East Works) and School Lane. A right turn lane for vehicles travelling southbound along Claremont Avenue into Market Square will be provided, and a left turn lane for vehicles travelling northbound along Claremont Avenue into Market Square will be provided.

Off-road segregated pedestrian/cycle facilities alongside the northbound carriageway between the Claremont Road / Claremont Avenue / Orchard Lane / Claremont Park Road junction and the High Street South (East Works) and School Lane junction and alongside the southbound carriageway between the Tempelhof Link Road junction and the Claremont Road / Claremont Avenue / Orchard Lane / Claremont Park Road junction will be provided. A pedestrian only footway will be provided alongside the northbound carriageway between the High Street South (East Works) junction and Tilling Road.

(4) Claremont Avenue Junction with Tilling Road

A traffic signal controlled junction will be provided at the Tilling Road / Tempelhof Link Road / Claremont Avenue junction. Vehicles turn directly onto Claremont Avenue from Tilling Road to travel southbound towards Claremont Road. Vehicles travelling northbound to access Tilling Road will need to turn right onto Tempelhof Link Road before reaching the signalised junction.

Pedestrian crossings and advanced stop line markings provided on each arm (of the Tilling Road junction) will increase pedestrian and cyclist safety.

(5) Claremont Road North Junction

A traffic signal controlled junction will be provided at the Claremont Road / Claremont Avenue / Orchard Lane / Claremont Park Road junction. Toucan crossings and advanced stop line markings provided on each arm will increase pedestrian and cyclist safety. A right turn lane for vehicles travelling southbound along Claremont Avenue into Claremont Park Road will be provided. A dedicated left turn lane for vehicles travelling northbound along Claremont Road into Claremont Park Road without waiting at the signals is also proposed.

Clitterhouse Road will form the minor arm of a new priority junction with Claremont Road to the south of the Claremont Road north junction. Vehicles

travelling along Clitterhouse Road will need to give-way at the junction before turning onto Claremont Avenue to the north or Claremont Road to the south.

(6) Tilling Road West Realignment (Part 1)

The west section of Tilling Road is to be realigned to link with the newly configured M1 / A406 junction and provide a revised point of access to Brent Terrace North. A shared footway/cycleway at the western end from Brent Terrace North and a toucan crossing facility across Brent Terrace North is to be provided. A footway on the southern side (between Brent Terrace North and the roundabout) and the eastern end in the vicinity of the roundabout will continue to provide pedestrian access to two bus stops.

(7) High Street South (East Works)

High Street South (East Works) will provide a link between Claremont Avenue and Tempelhof Avenue forming a junction with Claremont Avenue at its eastern extent. A segregated cycle / footway and two new bus stops along the link (one on each side) will be provided on the southern side. High Street South will provide access to Market Square when this comes forward.

(8) Orchard Lane

Orchard Lane will be a new road linking with the new Claremont Avenue / Claremont Park Road / Claremont Road traffic signal controlled junction. Orchard Lane will form the eastern arm of this junction enabling a link with Prayle Grove and the associated residential areas served from Prayle Grove.

(9) Modifications to BXSC Perimeter Road

The western extent of the BXSC car park access road is to be realigned to join the new Prince Charles Drive western roundabout. The general alignment of the remainder of this section of the car park access road will remain the same as existing. A shared footway / cycleway facility will be provided alongside the eastern side of the carriageway linking with a Tiger crossing across the access road carriageway (close to the proposed emergency access through to Layfield Road). From the Tiger crossing a shared footway / cycleway will continue alongside the western side of the carriageway linking with Sturgess Park where a shared footway / cycleway will be provided with cycle parking. The two mini roundabouts will be retained and a taxi rank provided to the south of the east / west section of the access road.

The eastern extent of the BXSC car park access road will be realigned to join with the new Prince Charles Drive eastern roundabout. The general alignment of the remainder of this section of the car park access road, the mini roundabout and zebra crossing will remain the same as the existing arrangements with a pedestrian ramp provided from the car park access road at the zebra crossing to Spalding Road. An off road shared pedestrian / cycle facility alongside the access road and pedestrian crossing across the access road will allow access to the cycle parking at this location.

(11) A406 North Circular Road Coach Stops

The two coach stops located on the A406 on and off slip roads in the vicinity of the junction with the A41 will be displaced by the proposals for Phase 1A (North). An agreement in principle has been reached to relocate the stops to the following locations with final details to be submitted and approved as part of detailed design:

The northbound / westbound coach stop is to be located into an existing parking lay-by further along the slip road which joins the westbound carriageway of the A406. This location permits existing pedestrian facilities to be maintained to the coach stop and does not cause any service issues.

The southbound / eastbound coach stop is to be located onto Haley Road to the east of the A41. The location is on an area of land available alongside the slip road between the Shirehall Roundabout and the A41. This location requires the extension of double yellow lines to prohibit parking on both sides of the carriageway to maintain access to the A41 whilst a coach uses the stop. This location will also provide a safe walking route to the shopping centre and the surrounding residential areas. Coaches access this stop by using the A406 / A41 junction onto the A406 eastbound slip road and turning onto Cooper Road and Haley Road via the Shirehall Roundabout. Coaches access the A41 directly from the Haley Road / A41 slip road towards the A406 / A41 junction.

12) Acoustic Barrier

An acoustic barrier is proposed to be installed on the highway verge located between the A406 and the diverted Prince Charles Drive, to protect the riverside park from the traffic noise arising from the north circular. Although the A406 acoustic barrier will be delivered as part of the Phase 1A (North) the detailed design and specification of the barrier is subject to separate approval under condition 29.10 of the Section 73 Consent. The indicative location of the barrier is shown on the submitted RMA plans however the details are not considered by this application.

Bridges

(a) Bridge Structure B7 (Living Bridge)

A new pedestrian and cycle bridge over the A406 will be provided measuring between 17m and 25m in external width including the parapet. This bridge will form a key north/south link across the A406 between the Brent Cross shopping centre / new bus station and areas to the south including Claremont Avenue and Clitterhouse Playing Fields via the graded southern approach to the bridge.

Access to the Living Bridge from Claremont Avenue will be via a wide approach connecting with the segregated footway / cycleway provided alongside Claremont Avenue. A toucan crossing will be provided across Claremont Avenue to allow connections from High Street South (East Works) / New Tempelhof Avenue and other areas to the west. Access to the Living Bridge from the north (bus station) will be via steps / lifts / escalators. Access from the shopping centre will be at grade. The sloping southern approach to

the Living Bridge will provide access for pedestrians and cyclists. The bridge is proposed as a pedestrian and cycle bridge only although there will be controlled access available for maintenance and emergency vehicles, although fire tenders are not accommodated in the design.

The bridge will be an approximately 170 metres long elevated structure with an additional 70m ramp at the south and wide enough to accommodate landscaping forming a sequence of public gardens, as well as providing appropriate effective width for the flow of pedestrians and cyclists. Cycle parking and benches are to be provided along the bridge including 20 double sided cycle stands (40 spaces) within the north section of the bridge, four double sided cycle stands (8 spaces) within the south section of the bridge and eight double sided cycle stands (16 spaces) on the graded southern approach to the bridge.

A comfort level assessment for the pedestrian links within Phase 1A (North) identifies the two main crossings of the A406 will accommodate 7,590 pedestrian movements per hour. This adequately accommodates the pedestrian flows predicted, as contained within the original planning application. The Living Bridge alone will accommodate 87% of the forecast demand.

The bridge will be sheltered from noise and pollution associated with traffic on the North Circular Road through provision of a raised solid parapet. Lighting will be provided along the Living Bridge, complying with the lighting strategy. Various trees will be planted along the Living Bridge complying with the planting strategy.

The minimum effective width of the shared space path along the Living Bridge varies across its length, with passing places provided where necessary. The minimum effective width for pedestrians and cyclists will be seven metres. At the northern extent of the bridge a narrowing to six metres forms a gateway to signal the end of the cycle route. The gradient of the bridge will vary along its length. Various sections of the bridge comprise a flat gradient, with a nominal fall (1:40 slope and a maximum 1:30 slope). The cross fall across the bridge will be between 1% and 2.5% ensuring drainage provision is adequate.

The bridge is not designed to take vehicle loading other than planned maintenance vehicles and emergency vehicles (excluding fire tenders) and bollards will be installed at the south to prevent unauthorised access.

The Living Bridge rises above the road infrastructure beneath it to connect the southern developer area with Brent Cross Shopping Centre. The east and west elevations face the highway and are similar in design, constructed in 'Corten' and presenting a 'saw-tooth' face towards the highway that is solid and relatively flat facing towards the A406 but which increasingly gradates to permit angled views from the bridge through a lace pattern structure. Away from the highway infrastructure the bridge will be bound by development plots to the south, the detail of which will be defined in future RMAs and by the new shopping centre to the north.

The Bridge will be planted and landscaped whilst providing a clear minimum 6m wide pedestrian and cycle route between the southern and northern areas. The proposed landscape planting details have been the subject of lengthy discussion between the developers and officers to ensure that the planting mix, in terms of species, size, frequency of planting and maintenance regimes etc., are all agreed.

The graded southern approach will be planted with a double avenue of deciduous trees and hedging with a range of different seating options provided throughout. A row of 1100mm high stainless steel bollards will be set at 1200mm centres to control access to the southern approach and prevent vehicular access. Eight double sided cycle stands will be provided at this location. The surface material of the approach will be a combination of different types of granite stone paving setts including feature paving strips.

In order to build in access to the adjoining future development plot 93 to the west of the bridge a level area is formed using a stepped access. Moving towards the bridge structure a number of planters are proposed; these will be planted with conifer/deciduous tree planting and be provided with bench seating.

Moving onto the bridge structure the pedestrian/cycle route is more sinuous in nature with ornamental planting areas to the bridge edges; these will be planted with flowering bulbs. The planted areas will comprise a mix of conifer and deciduous tree planting, ornamental grass planting and areas of vertical wall planting interspersed with the inward facing 'corten' elevation. The area will be provided with an extensive mix of seating options, including bench seating around planters, bench seating to the grassed areas and bench seating to parapet areas. A number of double sided cycle stands are provided towards the southern and northern extent of the bridge, 8 double sided stands to the south and 19 double sided stands to the north.

The central area of the bridge structure opens out to a larger public space. The Landscaping Strategy ensures that a seasonally diverse planting regime will be implemented together with contrasting textural planting to provide all year round interest.

Overall the Living Bridge will be viewed externally as an enclosed green space with tree planting visible along the length of the bridge.

Details of an indicative lighting strategy have been submitted in the Design Development Report (section 6.5) that accompanies the application however the detail of the lighting is subject to the further consideration and written approval of the planning authority by way of a planning condition. The bridge will connect via stairs to the Bus Station level and riverside walkway levels below. The applicants have also agreed to provide a lift between the different levels. As the Shopping Centre layout design has still to be concluded the final details of the lift location require the further consideration and approval of

the planning authority. The applicants have however submitted indicative locational details of the lift.

Approximately 60 of the Whitefield Estate residential units are to be demolished to make space for construction of the Living Bridge. Reserved Matters consent has already been granted under application 15/00720/RMA for the residential development on Plots 53 and 54 which are intended to be used for the Whitefield Estate Replacement Units (Part 1). A Resident Relocation Strategy has also been prepared pursuant to condition 1.10.

(b) Bridge Structure B1 (Replacement A406 Tempelhof Bridge)

This bridge will form another key north / south link across the A406 between the Prince Charles Drive western roundabout and Claremont Road. The key dimensions are 28m wide and 171m in length (plus ramps) and a segregated footway / cycleway will be provided alongside the northbound carriageway of New Tempelhof Avenue linking with a segregated facility and toucan crossing at Claremont Avenue. To the north of the A406 the segregated footway / cycleway will link with a shared footway / cycleway facility connecting with the shopping centre car park access road and the shopping centre cycle parking provision. A toucan crossing will be provided across New Tempelhof Avenue in order to provide access to cycle parking located at the BXSC Tempelhof Circus entrance. A pedestrian only footway will be provided alongside the southbound carriageway between the toucan crossing across New Tempelhof Avenue and High Street south (East Works). In addition, a pedestrian only footway will be provided along the Tempelhof Link Road. It should be noted that some parking for the Holiday Inn hotel is proposed under the bridge on the southern side, whilst on the northern side north of Prince Charles Drive there is an enclosed void created below the bridge which the Developer would like to use in relation to the future development on the adjacent plot (101). To protect the bridge and highway above, the use of any space under the bridge will be controlled by, and subject to a lease from the relevant Highway Authority.

(c) River Brent Bridges

A total of eight vehicular bridges and two pedestrian bridges will be provided over the River Brent along the eastern, central and western stretches.

There are two bridges forming part of the western roundabout and two forming part of the eastern roundabout which are planned to be adopted by LBB as highway authority. These are approximately 20m to 30m long and of varying width.

There are an additional four river bridges between the two roundabouts with two providing access to the proposed bus station (in and out), and two more providing to the underground service access off Prince Charles Drive, and also access to the proposed residential plot located between Tempelhof Bridge and the western roundabout (plot 101). These vary but are typically 22m long by 12m wide.

These river bridges are all of the same style and design being concrete construction and the cross the diverted river, the proposed riverside park and footway/cycleway, in a single span.

Microclimate Assessment

The proposed development has been assessed in terms of its likely microclimate impact by an independent microclimate consultant. Following initial submissions and responses the development has been tweaked to address some potential microclimate issues. Overall however the development is now considered to be acceptable.

The most significant changes to the development to address initial concerns relate to the following:

- Living Bridge – further wind tunnel testing has been undertaken by the applicants to assess the potential impact on the basis of the indicative layout, particularly in relation to the southern approach to the Living Bridge and also taking account of the revised landscaping and bridge design and no significant impacts are anticipated; to some degree this has been achieved by re-defining the manner in which the applicants envisage the Living Bridge being used. The target conditions, in terms of proposed activities, have been amended to remove the potential requirements for long periods of outdoor sitting, such as for café outdoor seating. The ES justifies this by suggesting that the Living Bridge is primarily designed to be used as a thoroughfare for pedestrians and cyclists, with landscaping including benches and viewing areas to encourage visitors to linger and sit for short periods on the bridge.
- Further details have been provided regarding the potentially major adverse impact on the Tempelhof Bridge which is driven by the maximum height massing of the future plots alongside the A406. Whereas the developers suggest that the duration of impact is low and the safety threshold speed is exceeded for less than 5 hours in a year and that this represents a marginal exceedance, against a measurement criteria of 1 hour per annum officers consider this is not a marginal exceedance and there may be potential for high wind speeds only slightly below the threshold for considerably longer periods. Mitigation has not been recommended at this stage. As contingency, the applicants have stated that the addition of a 1.5m high solid barrier along the bridge sides would be effective at ensuring wind conditions on the bridge are suitable in terms of pedestrian safety. However, this does not appear to have been verified through wind tunnel testing. As there is some uncertainty at this time over how the development plots will be delivered it is problematic defining mitigation on the basis of an unknown scope. Rather than requiring mitigation at this stage it is recommended that this issue will need to be addressed through future RMA proposals relating to the adjoining development plots as the impact in terms of wind can only be fully assessed at that time.

- Concern had initially been raised regarding impacts on Central Brent Riverside Park, specifically in relation to some of the seating areas. Minor design changes however have now addressed this issue.

Other Structures

(a) Brent Cross Pedestrian Underpass Works

This refers to the existing pedestrian underpass just north of the A406/A41. Improvements include general maintenance such as painting, and an upgrade to the lighting where required. Further details will be provided at the detailed design stage.

Public Transport – Bus stops

The public transport improvements comprising Phase 1A (North) consist of improved bus stopping facilities. Waiting conditions at a number of existing bus stops are to be improved and new bus stops are also being provided in Phase 1A (North) in preparation to accompany new and altered services. Although the bus stop locations are illustrative, and their precise locations along the highways will be agreed with TfL during the S278 agreement process, their location on specific roads have been based on a review of likely need and these are set out in detail in a list in Appendix 2.

Temporary Bus Station

The new fully integrated high quality permanent bus station will come forward as part of Phase 1B (North) of the Brent Cross regeneration. In the interim, the existing bus station is planned to be moved to a temporary location during Phase 1A (North) to allow other proposed infrastructure to be delivered. Subject to the proposed Phase 1 construction programme a temporary bus station may be required for up to 4 years.

The temporary bus station will support the provision of the existing bus station (including all existing routes) and will operate until the new bus station is complete. Additional bus stops associated with pick-ups and drop-offs will be provided in the vicinity of the shopping centre, maintaining good bus passenger accessibility during the period when the temporary bus station is in place.

The temporary bus station facility is proposed to be located on the south west car park site (plot 114) comprising 8 bus drop-off and pick-up stops (4 each), 18 bus layover stands, driver facilities, an information kiosk and staff toilets. This bus station will operate 24 hours a day as the existing Brent Cross bus station does. The temporary bus station is located over 70m from the nearest residential properties in Brent Park Road to the north. The temporary bus station and bus stops have been designed with input from TfL to ensure that they meet their operational requirements.

Further temporary bus stops will also be located on part of the existing shopping centre car park to the rear of properties in Layfield Close, known as plot 113. This will comprise 8 stops for drop off and 8 stops for pick up with associated bus shelters along with an information kiosk and portable toilets. During daytime operating hours buses would set down and pick up passengers from the temporary stops on plot 113 which is closer to the BXSC, with the main bus station on the southwest car park being utilised to park up buses between shifts and provide bus services overnight.

The bus stops will be located approximately 38m from the rear elevation of the nearest residential properties in Layfield Close which are single storey bungalows. There is an existing substantial band of trees and hedge along this boundary in addition to an existing earth bund. Taking into consideration the proposed location of the bus stops, distance from neighbouring residential properties, the existing tree screen and earth bund, it is considered that the proposed bus stops will not be visually intrusive to the neighbouring residents.

A noise assessment report has been submitted with the application to consider the potential impacts of noise from the buses and their operation at Plot 113 on the neighbouring residential properties. The proposals include the provision of an acoustic screen along the edge of the bus stops to mitigate noise from the buses and associated operation. The Council's Environmental Health Officers have reviewed the submitted assessments and taking into account the site conditions and location of an acoustic screen. They consider that the daytime operating hours for the bus stops on Plot 113 should be limited to between 7:30am and 9:30pm on weekdays and Saturdays and between 9am and 9:30pm on Sundays and Bank holidays. A suitable worded condition is recommended to limit the hours of operation of the temporary bus stops on Plot 113 to these hours with the ability for them to be extended should noise monitoring over a 3 month period demonstrate to the satisfaction of the Local Planning Authority that the noise levels are within acceptable range and do not impact on the amenity of the occupiers of the neighbouring residential properties. A separate condition is also proposed to secure details and specification of the acoustic screen.

Coach Stops

Coach Stop J1: In the vicinity of the A406 / A41 junction

This 24 hour coach stop is to be relocated into an existing parking lay-by further along the slip road which joins the westbound carriageway of the A406. This location allows existing pedestrian facilities to be maintained and would not present any level of service issues.

Coach Stop J2: In the vicinity of the A406 / A41 junction

This 24 hour coach stop is to be relocated onto Haley Road to the east of the A41. This location would provide a safe walking route to the shopping centre and the surrounding residential areas.

The coach stop proposals are indicative and details will be submitted and approved as part of detailed design but they have both been discussed and agreed with TfL who oversee the coach operations, a shelter is proposed for waiting passengers and a new dedicated layby. There is expected to be associated drop off and collection of passengers at the coach stop by relatives/taxis in the vicinity.

Pedestrians

An assessment of the proposed footway widths relating to pedestrian comfort levels for pedestrian links within Phase 1A (North) has been provided. The assessment indicates the proposed widths are likely to comfortably accommodate predicted pedestrian flows.

Of the 32 pedestrian links assessed, widths vary between 1.7 metres (link from A41/A406 underpass to Cooper Road which is an existing footpath) to 7m (minimum width on the Living Bridge). In total, only 8 of the 32 links are less than 2.2m, with an average of 2.98 metres.

Existing pedestrian provision across the A406 due to the development is improved. In total six crossings will be provided:

- The Living Bridge: From Claremont Avenue via a 16 metre wide approach, the bridge will connect with the segregated footway/cycleway alongside Claremont Avenue. Access to the living Bridge from the bus station will be via steps / lifts / escalators. The bridge provides a friendly and relaxing environment for pedestrians to access and egress the shopping centre, with seating provision in a car free, environmentally friendly design.
- Tempelhof Bridge: A segregated footway/ cycleway adjacent to the northbound carriageway (2-2.5metres wide for pedestrians), with a pedestrian only footway (2.5-4metres wide) alongside the southbound carriageway.
- Pedestrian / Cycle Bridge (B6): At the eastern side of the Staples Corner / M1 / A406 junction providing a 4metre wide link in the pedestrian/cycle network.
- Replacement Pedestrian-only Footbridge at Staples Corner: the west of Bridge B6, the existing bridge will be replaced and a wider new ramp, to benefit mobility impaired users, provided on the northern side.
- Existing Pedestrian-only Footbridges: General refurbishment of the retained footbridges at Staples Corner
- A406/A41 Footbridges and Underpasses: General refurbishment of the retained underpasses and bridge (lighting / painting etc).

As part of the Phase 1 Strategy, the requirement for a 24 hour route through/adjacent to the Shopping Centre has been confirmed. The detailed arrangements are to be confirmed in sub-phase 1B North proposals.

A six metre minimum width shared footway / cycleway is proposed adjacent to the realigned River Brent.

With the 24 hour route through/adjacent to the Brent Cross Shopping Centre and the Brent Riverside Park link, no cycle / pedestrian provision is proposed along Prince Charles Drive.

For the area outside of the development site, the Area Wide Walking and Cycling Study assesses pedestrian routes to key destinations using the Pedestrian Environment Review System (PERS) methodology and identifies measures to improve the rating of those routes to integrate the development, and new facilities, with the existing pedestrian and cycle networks during Phase 1A (North). Funding of £1,250,000 has been secured towards pedestrian and cycle routes providing connections to Brent, Camden and other locations in Barnet under Condition 1.20.

Accessibility

The detailed design and landscaping of the central section of the Brent Riverside Park (reference 15/03315/RMA) is one of the suite of RMAs for Phase 1A (North) and is covered by a separate report also before Members at this committee. Please refer to this report for full detail of the design of the riverside park. Information is also provided under section 5.5 of this report in relation to the proposed river diversion works.

The proposed design of the infrastructure including the new Brent Riverside Park created as a result of the diversion of the River Brent has been developed in consultation with the Brent Cross Cricklewood Consultative Access Forum, to ensure accessibility for mobility impaired people has been properly considered.

In the Central Brent Riverside Park, on the northern side, there will be four locations where steps with a vehicle channel from the upper level path allow access to drop down to meet the lower level pedestrian and cycle path along the riverside walkway. The proposed steps from the bus station to the river corridor conform to the criteria for an Ambulant Disabled Stair.

In discussions with Officers and the CAF, concerns were raised in relation to the accessibility of the central reach of the new Brent Riverside Park for all users. At the London Borough of Barnet's request, a ramp for cyclists leading down to the riverside walkway to provide a safe connection for cyclists travelling between the Tempelhof Bridge and the residential areas to the East of the A41, has been included. This increases inclusive access to the river path to the west, via a graded slope of 1:21 with a dog leg and generous landings. The inclusion of a single lift to the west of the new Living Bridge from the proposed bus station level down to the lower riverside walkway level has also been agreed and is subject to condition requiring details to be provided.

The inclusive access point in the eastern reach of the Park is at the existing A41 underpass. Within the western reach of the Park a ramp is proposed. Both are defined on the Phase 1A (North) Infrastructure RMA highway drawings.

Cyclists

The proposed cycle networks for sub-phase 1A (North) will provide comprehensive connections to the development and the surrounding areas.

The development improves access across the A406 for cyclists with the Living Bridge close to the shopping centre (current proposals assume that cyclists will dismount at the northern end of the Living Bridge with cycle parking at both the northern and southern approaches to the bridge) and a new bridge at Staples Corner. Access over the A406 will be further enhanced by the improved replacement Tempelhof Bridge and the improvement of facilities at the A406/A41 junction. For cyclists, routes across the A406 provide a link to the proposed Cycle Superhighway CS11 along the A41 corridor via a mostly segregated cycle route (apart from the short length between Clitterhouse playing fields and the A41 at Ridge Road).

East - West cycling routes will be improved by the addition of a cycling link via the new 6 metre riverside shared footway/cycleway on the north bank of the newly created Brent Riverside Park. This will provide a pleasant, safe and high quality cycle and pedestrian connection and as a result no dedicated pedestrian and cycle provision will be made along the reconfigured Prince Charles Drive.

Further detail on the routes and cycle parking provision, including specific site and improvement details, is provided within the Pedestrian and Cycling Strategy for Phase 1A (North) (47065005-TP-RPT-010) submitted and approved under Condition 2.8. Details of the type and nature of pedestrian and cycle provision (i.e. segregated/shared use), footway and cycle path widths and type of crossing facilities are set out within the strategy.

In the area surrounding the outside of the development site the Area Wide Walking and Cycling Study assesses cycling routes to key destinations using the Cycling Environment Review System (CERS) methodology. This identifies measures to improve the rating of the routes, integration of the development, and the new facilities, with the existing pedestrian and cycle networks during Phase 1A (North). Funding of £1,250,000 has been secured towards routes providing connections to Brent, Camden and other locations in Barnet under Condition 1.20.

TfL policies on cycle and pedestrian provision have changed and evolved since the S73 S106 agreement was signed. During the detailed design, wherever possible within the constraints of the scheme, the implementation of the new guidance and standards is being incorporated.

Car Parking

The parking demand associated with Phase 1a North relates to the occupancy of Plots 53/54, the open space area at Clitterhouse Playing Fields and the temporary demand generated by construction workers.

For the 48 dwellings in Plots 53/54, a total of 48 parking spaces, of which 5 spaces will be for blue badge holders, will be provided. At Clitterhouse Playing Fields, 22 parking spaces and four for blue badge holders are proposed within a car park accessed via Claremont Road.

The parking associated with these two sub phases has already been approved as part of previous Reserved Matter Applications considered by this committee.

To date, the location and size of the car park for construction workers has yet to be identified. This is to be confirmed within the Construction Workers Travel Plan which will be submitted to the Council for approval prior to the commencement of the Phase 1A works. Initial indications are that at its peak, the car park will provide approximately 500 car parking spaces for construction workers during the development of Phase 1A (North). This is subject to consideration under Conditions 12.2.

Schedule 3 of the Section 106 requires the Development Partners to fund, prior to the commencement of development in any phase or sub-phase, the cost of developing and implementing CPZs in Barnet should the council reasonably determine that existing zones need to be extended or new ones introduced in relation to both construction activity as well as final occupation of development.

The decision to introduce new Controlled Parking Zones or extensions to existing ones needs careful consideration and will be subject to comprehensive monitoring. Under schedule 3 of the Section 106 (paragraph 11.3) the Developers are obliged to fund the reasonable costs incurred by the council in assessing the need for new or modified CPZs in the vicinity of the site.

In addition to the measures set out in the Car Parking Management Strategy submitted to discharge this condition, there are additional planning conditions to deal with phase specific car parking standards and strategy (Condition 11.2) and to deal with construction worker parking through the phase specific Construction Worker Travel Plans required under Condition 12.2.

Taxi Rank Provision

The existing 9 space taxi rank provision will be retained as per Condition 1 in Appendix 1. As part of phase 1B (North) a new 15 space taxi rank located at the northwest corner of the development to the north of the new western roundabout on Prince Charles Drive and a further taxi drop off provision accommodating up to seven taxis within the layby to the north of the new eastern roundabout on Prince Charles Drive, will be provided.

Signage

In addition to highway direction, regulatory and warning signs, signing throughout the development will be provided in accordance with the guidelines set out in Legible London, and contained in the Wayfinding and Inclusive Access Strategy (Ref: 031758).

Overall, officers are satisfied that the proposals comprehensively cover all the necessary transport aspects of the development that are relevant to Phase 1A (North). The next section discusses the assessment of the proposals.

5.4 Transport Assessment of Detailed Application

Introduction

The highways feasibility design of the new roads and junctions for sub-Phase 1A (North), including the gateway junctions, has been progressing in conjunction with the Developers, LBB, HE and TfL. To support the highway design and approval processes the authorities agreed that a new transport model (BXC detailed design model - DDM) should be built based on TfL's latest sub-regional model. It was further agreed that this model would be used in the A5 Corridor Study as the sub-regional model network was more detailed than the model used at outline application stage. The BXCDDM has been used to sense check the Phase 1A (North) highways scheme and junction layouts. The results of the junction feasibility design and modelling work (excluding the already approved gateway junctions) are set out in the Reserved Matters Transport Report (RMTR) and discussed below.

The 2014 Permission provides full planning consent in respect of the five Gateway Junction included within Phase 1A (North) (three on the A406 and two on the A407) for which reserved matters approval does not need to be sought, although it is worth noting that the junction assessment and modelling with the new BXCDDM has not resulted in any changes in the approved Gateway Junctions that require a further planning application.

Traffic Modelling

The strategic transport model used for all planning related work to date is termed the Brent Cross Cricklewood Transport Model (BXC TM) which is considered to be a robust tool to estimate future transport impacts of the development on the highway and public transport networks and to ensure the impacts of the Development remain within the overall envelope identified in the S73 Transport Assessment.

The Brent Cross Cricklewood Detailed Design Model (BXC DDM) has been developed for detailed design purposes and includes an increased level of detail of both existing and forecast traffic movements on the local roads within

the BXC study area. The model has been developed in line with TfL guidelines and meets the requirements of both TfL and LBB.

Junction models

Based on the BXCDDM traffic flows, detailed junction models have been utilised to rigorously test the proposed Phase 1A (North) junction layouts. Future year models for phase 1 (2021) and end state (2031) have been developed for weekday AM (08:00-09:00 hours), PM (17:00-18:00 hours) and Saturday (13:00-14:00 hours).

All junction designs, including traffic signal timings have been developed in collaboration with TfL, LBB and HE. The designs and associated traffic signal timings will be refined to improve performance through the detailed design process.

As part of the RMA the following junctions have been re-assessed using the BXC DDM and appropriate junction modelling tool to ensure that the designs originally developed at outline stage using the BXC TM remain appropriate:-

- Eastern Roundabout

This roundabout is now proposed to be partially signalised and the signal control will be linked to the signals controlling the exit of the adjacent bus station to help ensure efficient egress for buses onto the highway network.

The detailed junction capacity analysis for Phase 1 (2021) indicates the junction will operate within capacity in the AM and PM weekday peak periods and in the Saturday peak period it indicates queuing on the northern arm of the junction (from the BXC shopping centre perimeter road). Should such queues occur in reality, traffic is likely to re-assign or be re-routed via real-time signage to the western roundabout, where there is sufficient spare capacity.

- Western Roundabout

This roundabout is now proposed to be fully signalised with a 'cut-through' to facilitate traffic heading towards the shopping centre from Tempelhof Avenue.

The junction is forecast to operate during all time periods with no excessive queues or congestion in 2021 or 2031.

- Claremont Avenue / High Street South (East Works) / School Lane

The junction design has been amended from a 4-arm priority junction to a 4-arm traffic signal controlled junction with road space provided on both arms of Claremont Avenue to cater for right turning movements.

In phase 1 (2021) there will be negligible traffic demand entering and exiting School Lane.

The modelled results for both future years indicate this junction is forecast to operate satisfactorily during all time periods with no excessive queues.

- Claremont Road/Orchard Lane/Claremont Park Road
The modelled results for both future years show that this junction is forecast to operate satisfactorily during all time periods with no excessive queues.
- Claremont Avenue /Tilling Road/Tempelhof Link Road
The traffic signals at the Tilling Road / Claremont Avenue and Tempelhof Link Road / Claremont Avenue junctions will be linked to enable co-ordination between the two nearby locations.

The central island on Claremont Avenue allows for pedestrians to cross in parallel with traffic phases, resulting in only small impacts on highway capacity. The junction model assumes that the pedestrian phase occurs at all times.

The capacity analysis identifies significant queues only in the 2021 Saturday period and a reduction in the frequency with which the pedestrian phase is activated causes the overall modelled performance to be improved.

- Tilling Road / Brent Terrace North
This junction has been included in the traffic signal group which operates the re-modelled A5/A406/M1 Junction. The modelled results for both future years show this junction is forecast to operate satisfactorily during all time periods with no excessive queues.

VISSIM

In addition to the above conventional area-wide and individual junction models a microsimulation (“VISSIM”) model has been developed. Microsimulation models are capable of modelling individual vehicles and detailed vehicular interactions between junctions (whereas traffic models such as the BXC TM and BXC DDM normally model aggregate traffic movements across areas). The BXC VISSIM model encompasses the A5, Prince Charles Drive, Tempelhof Avenue, Claremont Avenue and the A406 and is being developed for 2021 in parallel to provide additional rigour in testing the detailed design of the planned transport infrastructure for Phase 1A (North). The VISSIM model has been utilised and reviewed as part of the A5 Corridor Study. Development of the model is on-going and transport authority audits to date have generally concluded that the model is acceptable.

Feasibility Design

Since the S73 submission the design has been progressed by the Developer as part of the technical approval process and in preparation of the RMA submission in consultation with the Technical Approval Authorities (LBB, TfL

and HE) and based on the output from the Detailed Design Model (BXC DDM).

The proposed highways have been designed to nationally recognised and approved design standards; however the nature of designing roads in an already constrained urban environment means that every requirement of all the design standards cannot be achieved.

Whilst the principles of the planned layout for Phase 1A (North) remain the same as those in the consented S73 scheme, the Developer has liaised with the relevant highway authorities and requested a number of relaxations and departures from standards. A number of these have been rejected following safety audits and after detailed consideration by the authorities. However, some have been agreed and this has resulted in minor changes to the S73 layout. Overall, the changes improve visibility, safety of the network and ensure that adequate traffic capacity is provided.

As part of the normal design process, further assessment and safety audits will be carried out on the design prior to construction and it can be expected that minor changes to the overall design may be required including items like, signage, lighting, drainage, utilities arrangements. This is normal for this stage of design of the scheme which can be recommended by officers for planning approval. The other highway authorities, TfL and HE, also support the approval of this RMA.

Road Safety Audit and Designers Response

A road safety audit and the associated designers response is produced at various stages of development of highway schemes in Barnet. For Planning Approval an acceptable stage 1 road safety audit is normally required, as was produced for the full highway layout in the S73 scheme in August 2013.

Subsequently a new Stage 1 Road Safety Audit was undertaken for the revised highway layout in November 2014 and a Designers Response followed in December.

The principles of the highway layout for the majority of Phase 1A (North) remain the same as those for the consented S73 scheme. However, following changes to the junction layouts due to the emerging DDM and following changes to alleviate safety concerns associated with non-supported departures from standards, a number of changes to the scheme have been introduced in agreement between the developers and the relevant highway authorities. A further series of stage 1 Road Safety Audits were carried out in July 2015 by TfL based on the most recent designs and departure reports. Four separate audits were carried out to cover the eastern and western roundabouts, London Borough of Barnet local roads and the A41 interchange.

As a result of some findings of the Road Safety Audit, and on-going discussions between the developers, their designers and LBB officers the layout of the eastern roundabout and some aspects of the local roads have

changed further since the plans were audited, and since the RMA consultation took place. However, having reviewed the latest submitted plans officers consider that the latest agreed changes are acceptable as they will improve both highway safety and performance, and will be subject to further Road Safety Audit at the appropriate stage in the detailed design.

A designer's response to the July 2015 Road Safety Audit has been submitted to the highway authorities and found to be generally acceptable.

It should be noted that the road safety audit process supports the RMA scheme development through helping to indicate what further work and changes should be considered as part of detailed design. Officers consider that the findings of audits to date are quite minor and normal for an application of this complexity. A further (Stage 2) Road Safety Audit will form part of the detailed design process and will probably be undertaken by the relevant Highway Authorities.

Public Transport Assessment

Phase 1A (North) sees the construction of infrastructure to enable the development of the new Brent Cross Bus Station, implementation of the temporary bus station and a series of bus lane measures at key locations, including gateway junctions, as part of the highway mitigation measures. In addition it allows a number of bus services to be re-routed, extended and altered.

One of the key aims of the BXC scheme is to establish and grow a pattern of increasing use of public transport for everyday activity in and around the local area that will be led by better interchange facilities and enhanced bus service provision. This will start to create capacity that will be continued during later phases of the scheme including a step-change in public transport provision associated with the new railway station. A crucial issue is the ability to maintain the capacity of the public transport networks such that they remain attractive modes of travel for new and existing travellers in the area.

As with the highway models the BXC DDM also includes public transport models for 2021 and 2031 AM, PM and Saturday peaks that has been similarly used to sense check the phased public transport proposals to ensure that the mitigation is sufficient and is introduced in the appropriate phase. The public transport forecasts continue to support the measures approved at outline stage and hence those proposed or enabled as part of this RMA, and officers have reviewed the public transport model and its forecasts and found all to be acceptable.

Construction Impact Assessment

The impact of construction of Phase 1 on the highway network is being managed through the following:

Construction Transport Management Plan

The Construction Transport Management Plan (CTMP) will set out traffic management procedures and processes to mitigate impacts of the development in terms of construction transport.

Condition 12.1 attached to the 2014 Permission requires a site-wide CTMP be submitted to and approved by LBB prior to development commencing. The CMTP is a detailed site wide plan setting out traffic management procedures and processes to mitigate any impacts which arise from the construction traffic travelling to and from and within the Site, including the appointment by the developers of a Traffic Management Officer.

Condition 12.1 also requires the CTMP to be revised at least every 3 years to reflect any changes in relevant best practice guidance or other relevant policy guidance, and so satisfactorily address any issues of concern or causes of complaints which might arise in relation to the operation of any approved version of the CTMP.

Construction Workers Travel Plan

Parking demand for construction workers will be managed via the Construction Workers Travel Plan (CWTP), within which sustainable transport options will be promoted. This will be coupled with the introduction of controlled parking zones to protect local residents from construction worker commuter parking on local streets. Their introduction is controlled through the section 106 for roads in Barnet and the developers have reached a separate agreement with LB Brent for the Dollis Hill area.

An initial outline of the Construction Workers Travel Plan Framework is contained in Paragraph 20 of Schedule 3 to the S106 agreement. An example of best practice, being the Olympics Construction Workers Travel Plan Framework, has been submitted. A Construction Workers Travel Plan will be issued to the Council for approval prior to the commencement of the Phase 1A (North) works.

On-street Car Parking Assessment

The process of introducing new controlled parking zones (CPZs) or extending existing ones will begin once it is established there is an increase in parking demand as a result of the Brent Cross Cricklewood development. It is proposed that the first survey will be undertaken prior to the start of construction and then periodically at a frequency to be agreed with TfL and all boroughs involved. The ultimate decision on CPZs would be taken by the relevant authority taking into account other factors such as feedback from residents.

Should significant concern arise regarding parking on certain residential streets, all local London Boroughs have the opportunity to be represented at the Transport Advisory Group, where the issue can be raised and addressed. The funding of new or extended CPZs in Brent is available through the Consolidated Transport Fund (CTF) and would need to be applied for either through the Transport Advisory Group (TAG) or by the London Borough of

Brent directly to the Transport Strategy Group (London Borough of Barnet and TfL). The Transport Strategy Group is required to take account of TAG's recommendations.

The requirement for Controlled Parking Zones within Brent has been raised and discussed at the Transport Advisory Group and the need for provision within the Dollis Hill area outside the scheme boundary in relation to any parking overspill related to construction activity has been agreed between Brent and the developer (as this is outwith the Section 106).

5.5 River Brent Alteration and Diversion Works

Included in the Infrastructure RMA is the diversion of the River Brent and the associated formation of a new river channel corridor running east to west. In granting planning permission for the BXC development in October 2010 and subsequently through the approval of the 2014 S73 Consent, reference F/04687/13, the existing River Brent was viewed as an obstacle to achieving full regeneration due to the spatial fragmentation that retaining the course of the river would result in. It was therefore established that river would be diverted, thereby opening up a greater regeneration opportunity. The diversion of the River Brent has therefore been established since 2010.

The River Brent diversion works are part of a host of infrastructure works designed to remove barriers and constraints to development and open up the wider area to ensure that the regeneration proposals capitalise on the excellent transport links and new open space areas that will be created.

The River Brent in its current form is a canalised concrete channel running east to west through the car parks to the south of the existing Shopping Centre. Diverting the river to the south, to reflect the line of the A406 North Circular Road, will enable a more deliverable regeneration development to be undertaken within Brent Cross. Diverting the River also affords an opportunity to create a more naturalised river course. The proposals, in broad outline, create a meandering river course that will re-connect at the eastern and western ends of BXC into the existing river course. The opportunity arises to create a naturalised parkland riverside environment with marginal and wetland landscaping throughout the river channel, improved biodiversity throughout and ultimately improved water quality. In the latter case, improved water quality will benefit the River Brent and the Welsh Harp Reservoir Site of Significant Scientific Interest (SSSI) that it leads into. Outwith the river channel, land is provided to form a parkland setting and public access. Overall the intention is to improve access and enrich biodiversity.

The River Brent diversion runs east to west from the A41 Hendon Way, following the route of the A406, North Circular to its junction with the M1. The riverside park elements are provided in three separate reaches. The reaches reflect the different character of the adjoining development areas. Reaches 1 and 2 are situated adjacent to the Shopping Centre and this is reflected in their narrower width and desire to create a landscaped setting for the diverted

river as well as the Shopping Centre. Reach 3 on the other hand is in an area that is closer to residential property and the opportunity here is to create a more expansive ecological habitat area where the public has greater access to both banks of the river.

The detailed access, landscaping, surface water management and biodiversity proposals for the Central Brent Riverside Park (Reach 2) elements are provided in the separate RMA reference (15/03313/RMA) for that development, the report for which is also before Members for consideration at this Committee. Whilst details have been provided for Reaches 1 and 3 to either side of the central reach these details are indicative only and have not been submitted for approval.

The existing Prince Charles Drive has similarly been realigned to follow the diverted river course and will sit between the Riverside Park and the A406 North Circular road. The provision of the diverted River Brent is dealt with under Parameter Plan 011 contained within the Design Framework Strategy approved under the 2014 S73 Consent. Due to the relationship with highways infrastructure and the realigned Prince Charles Drive there are a number of road bridges that will cross the diverted river; in addition two pedestrian bridges cross Reach 3 to the west. The Living Bridge pedestrian and cycle access linking the southern development area into the Shopping Centre crosses the river at an elevated level.

Within Reach 1 the river diverts from the existing channel into its more naturalised diverted course. Within the corridor and built into the new highways infrastructure is an Environment Agency maintenance route access which provides access for the EA to the river channel. The maintenance route will also serve as a pedestrian and cycle route through Riverside Park connecting west to east. For the majority of the route the course is shared by pedestrians, cyclists and the EA. Within Reach 1 however the EA maintenance route diverges from the through pedestrian/cycle route to provide access to the river channel whilst the through access route continues towards the A41. The whole river corridor passes beneath river bridges 7 and 8 which make up the eastern roundabout which will connect Prince Charles Drive back into the road network and access to the BX Shopping Centre. River bridge 6 provides an exit from the future Bus Station to be provided as part of the future Shopping Centre extension. Within the corridor, a stepped access point is provided from the walkway to the Bus Station.

Within Reach 2 the diverted river runs in meandering fashion to follow the route of the A406 North Circular with the new Prince Charles Drive sitting between the Riverside Park and the A406. The river is crossed by River Bridge 5, the Bus Station access as well as the Living Bridge at an elevated position above bridge 5. River Bridges 3 and 4 provide access off Prince Charles Drive across the river to development plots 101 and 102. The bridges associated with the western roundabout and Tempelhof Avenue mark the western boundary of Reach 2.

The EA maintenance route through this section is a combined route incorporating pedestrian and cycle access. Access is provided from the riverside park to upper levels, including a future phase upper walkway route, by way of stepped access adjacent to the proposed Bust Station and further west beneath Tempelhof Avenue. A graded ramp access and additional stair access down to the lower level riverside walkway are provided to the west of Reach 2. In discussion with the Development Partners it has been agreed that a lift access will also be provided to connect the riverside park level with the bus station level. This will be located to the west of the Living Bridge. The exact location has not been fixed at this stage on the grounds that the detailed design of the buildings on the adjacent plots is not known at this stage. A plan has therefore been provided to show the zone within which the lift will be installed and a condition is recommended to require details and specification of the lift prior to commencement of the River Brent Alteration and Diversion Works.

There is no public access to the southern bank of the diverted river within Reach 1 or 2. Underneath the western roundabout, the river course will form Nature Park 5 to its western flank.

Moving into Reach 3 the river continues in its meandering form before reconnecting with the channelled river on the western boundary. Further access is provided off the proposed road network for the EA maintenance vehicles to access the riverside park environment. Within this reach pedestrian access is also provided to the southern bank of the river with two pedestrian bridges proposed, connecting the north and south banks. These also connect into the proposed Nature Park 4 which is to be provided at the western extent of the altered river channel.

The approved Parameter Plan 011 within the Development Strategy Framework identifies the three different reaches for the proposed river diversion and recognises the different context of different Reaches. The requirements for a 6m wide EA maintenance route/pedestrian and cycle access has been met throughout the design as presented. The Parameter Plan requires both the north and south banks to be planted and for the formation of wetland habitat areas. It is only in Reach 2, through the Central Brent Riverside Park RMA (reference 15/03315/RM), where the details of planting and landscaping have been provided and submitted for approval. The detailed planting and landscaping for Reaches 1 and 3 will be provided in future RMAs to be approved by the Council.

The different nature and context of each Reach is reflected in the design of the river corridor. In the narrower Reaches 1 and 2 the southern bank is steep and retains the elevated Prince Charles Drive. The steeply sloping and planted southern bank leads down into the river channel which is made up of the river bed and its associated flood plain. The channel is designed to meet a 1 in 5 year flood event scenario. The northern bank rises to the pedestrian/cycle walkway which provides a corridor through the riverside park. To the north of the walkway additional landscaping and seating will be provided where feasible. In the eastern section the EA maintenance route

leads down from an access off the A41 entrance. To the west in Reach 3, the corridor is more level, with no need for the steep slope to retain Prince Charles Drive. The park environment is more open and spacious with access to both sides of the river.

The current RMA consequently seeks permission for the alignment of the diverted River Brent and the associated pedestrian/cycle and EA maintenance routes although Members are asked to note that the full details of these works are provided under 15/03315/RMA in respect of Reach 2. Future RMA will be submitted in respect of Reaches 1 and 3 which fall in later phases of the development.

There are some minor variations to Parameter Plan 011 which are addressed via a separate application (reference 15/05040/CON) under Condition 2.4 and 2.5 which enables the applicants to vary the agreed parameters approved in the Section 73 Consent, subject to certain criteria. The details within application 15/05040/CON are discussed separately under Section 4.3 within this report and are also the subject of a separate report also before Members at this Committee.

5.6 Landscaping and Drainage Works

The landscaping and drainage requirements for the Central Brent Riverside Park (Reach 2) proposals are dealt with under the separate RMA report, 15/03312/RMA also reported to Members at this Committee. Members will note that the RMA relates solely to Reach 2 and that the proposals relating To Reaches 1 and 3 are listed as indicative only.

The landscaping and drainage proposals relating to this Infrastructure application however accompany the current Infrastructure RMA, with the exception of the diverted River Brent corridor.

A series of plans covering the site wide development detail the removal and protection of trees, shrubs and other vegetation including identifying where these are removed due to the proposed development works or where they are recommended for removal or maintenance due to their arboricultural condition. Tree root protection areas are also defined. In addition the application is supported by extensive planting plans, including the provision of new tree and shrub planting as well as providing details of the general landscaping proposals such as grassed areas and grass sowing mix details. The application is supported by a Tree Survey, Arboricultural Impact Assessment, Arboricultural Method Statement and Tree Protection Plan.

The landscaping details submitted for approval cover the areas within the highways' infrastructure; these vary in nature and scale to small areas of grass planting between highways to larger areas of tree and shrub planting and grass seeding where the infrastructure allows.

In addition, within the larger areas of landscaping the applicants have defined areas that will address drainage through the provision of retention ponds and detention basins. For example the larger areas of land available between the highways infrastructure at the A406/M1 junction will be landscaped to include grass planting, tree and shrub planting and the inclusion of naturally formed retention ponds.

Full design details of proposed surface water management, drainage and the construction of SuDS ponds have not been submitted with the current application and will be assessed through pre-commencement planning conditions attached to the 2014 S73 Consent. The drainage strategy however will include a range of measures throughout the site including retention ponds, detention basins, cellular storage tanks with the use of proprietary filtration and separation measures to control potential contaminants.

At the Section 73 application stage the applicants had identified a potential flood issue on the eastern section of the A406 where it is close to the River Brent and the Flood Risk Assessment Addendum identified a potential raising in the left bank level of the River Brent to address this issue. The current proposals introduce additional road gullies to allow flood water to drain into the River Brent and have been developed in discussion with the Environment Agency. Whilst they will not address flood risk completely they will facilitate flood water drainage.

The applicants' surface water management strategy has been developed in consultation with the EA who have undertaken their own independent assessment of the available information including modelling the realigned River Brent. The EA offers no objection to the proposal although planning conditions are recommended in relation to elements of detail. The proposals have also been assessed by officers and any queries raised regarding the strategy have been adequately addressed by the applicants and/or the EA. Overall, officers are content that the surface water management strategy is acceptable. Where detail is still required to be submitted for officer approval it is recommended that this is dealt with by way of appropriate planning conditions.

6. ENVIRONMENTAL IMPACT ASSESSMENT

The EIA procedure in the UK is directed by the Town & Country Planning (Environmental Impact Assessment) Regulations 2011 (the 'Regulations'), EU Directive 85/337/EEC (as amended), as well as the National Planning Practice Guidance (2014).

Regulation 8 of the Regulations requires local planning authorities to consider whether or not the environmental information already before them (i.e. the ES submitted with the 2013 hybrid application F/04687/13 and any additional environmental information) is adequate to assess the environmental effects of the development:

To demonstrate the continued acceptability of the ES associated with application F/04687/13 in the context of the detailed reserved matters applications for Phase 1A (North) an Environmental Statement Further Information Report (the 'ES FIR') has been submitted.

Within the ES FIR it is necessary to consider the impact of all relevant aspects of Phase 1A (North). The report therefore considers all of the Reserved Matters submissions to date and their individual and cumulative effects.

The ES FIR was first submitted and based upon the proposals within the original Reserved Matters submissions made to the Council in January 2015 including the RMAs for Infrastructure and the Central Brent Riverside Park which were withdrawn prior to registration. Since that time, further changes to detail in relation to the Infrastructure and Riverside Park RMAs have resulted in a revised Further Information Report being submitted with the current RMAs in order to further assess these more recent changes to the development proposals.

The technical assessments within the ES FIR and Revised ES FIR include the whole BXC Scheme in outline (as per the 2014 Section 73 permission), whilst adding in the detailed design of Phase 1A (North) RMAs. The Further Information Reports therefore assess the development as a whole.

In relation to the Infrastructure and Central Brent Riverside Park RMAs submitted in June 2015, all minor changes to the BXC Scheme and therefore the assessment from that reported in the ES FIR were captured in the Revised ES FIR which reflects further design evolution and responses to consultee comments. Although some reported significant impacts did change from the ES FIR as assessed in the Revised ES FIR they did not result in an overall change to the outcomes of the environmental impact assessment for the Phase 1A (North) and/or the BXC scheme as a whole. The supporting statement therefore concludes that there are no new or different significant effects so as to warrant changes to the relevant parts of the ES FIR.

The ES FIR has assessed the potential cumulative impact arising from a number of committed development schemes in the wider area and has consequently identified where mitigation will be required to address any significant impacts arising from the proposed Phase 1AN development. In general, where mitigation has been identified as required the impacts have been designed out. The most critical area is that relating to noise impact from the A406 affecting the amenity of users of the Riverside Park. The incorporation of an acoustic barrier has been tested through different options within the ES FIR and a preferred solution identified that will address this matter.

The existing Environmental Statement associated with the 2014 Section 73 permission supplemented by the ES FIR and the other additional environmental information previously submitted satisfactorily assess and address the impacts of the development for the purposes of determining the

7. EQUALITY AND DIVERSITY ISSUES

Section 149 of the Equality Act 2010, which came into force on 5th April 2011, imposes important duties on public authorities in the exercise of their functions, including a duty to have regard to the need to:

- “(a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;*
- (b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;*
- (c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.”*

For the purposes of this obligation the term “protected characteristic” includes:

- age;
- disability;
- gender reassignment;
- pregnancy and maternity;
- race;
- religion or belief;
- sex; and
- sexual orientation.

Officers have in considering this application and preparing this report had regard to the requirements of this section and have concluded that a decision to grant planning permission for this proposed development will comply with the Council’s statutory duty under this important legislation.

The site will generally be accessible by various modes of transport, including by foot, bicycle, public transport and private car, thus providing a range of transport choices for all users of the site. Access to the Riverside Park is restricted to no motor based traffic, with the exclusion of Environment Agency maintenance vehicles.

This RMA has been prepared in consultation with the Consultative Access Forum (CAF) which has been set up specifically to advise on inclusive access requirements of the Brent Cross Cricklewood regeneration. Design aspects that incorporate inclusive design have been reviewed by the CAF to insure that the development proposal for the Central Brent Riverside Park is designed, delivered and managed in accordance with the Inclusive Access Strategy and the Way finding Strategy.

To provide inclusive, step free access to the Brent Riverside Park three facilities have been incorporated comprising two graded ramps and the provision of lifts from Living Bridge level to the bus station level and from the

bus station level down to the lower Riverside Park walkway. The first graded ramp option is the provision of a ramped access within reach 3 to the west and is essentially the EA maintenance access route but is designed to also be suitable for use by pedestrians. The other graded ramped access is located within reach 2 and is located between the point where Tempelhof Bridge crosses the river and crosses over Nature Park 5.

With regards to the lift from the bus station/Living Bridge level to the Riverside Park, the submitted plan alongside this RMA application is indicative. The exact location of the lift within the identified zone of the Riverside Park, along with the specification and design, will be controlled via condition and will be submitted to and approved by the Local Planning Authority in consultation with the Consultative Access Forum. The submitted scheme is considered to provide for inclusive access for all users.

It is considered that the road network has been designed to accommodate disabled users and sufficient width footways, dropped crossing points, and suitable gradients and crossfalls will be incorporated in the design, however the design organisation will need to complete a Non Motorised User Audit as part of the next stages of detail design, which will confirm the designs acceptability for non motorised users in detail.

It should be noted that the proposed Tempelhof Bridge has a maximum longitudinal gradient of 1:20 which is generally recognised by most standards as acceptable for most users, although many wheelchair users would be unable to proceed at gradients of more than 1 in 40. It is accepted that the proposals are a significant improvement on the existing situation where a stepped ramp is the only off carriageway connection between the north and the south of the A406 in this location. Whilst this is accepted as a significant improvement on the existing situation, some design guidance suggests flatter gradients to accommodate all manual wheelchair users would be more appropriate. Due to the constraints of the site including the need for adequate headroom over the A406 provision of gradients below 1 in 20 would be unviable. Recognising the constraints of the site and the provision of the Living Bridge close by with flatter gradients, it is considered an appropriate provision has been made.

8. CONCLUSION

BXC is a large and complex regeneration scheme and Phase 1A (North) provides the majority of the key enabling highway infrastructure to allow development to commence both north and south of the A406.

This application seeks reserved matters approval for the detailed road layout and network within Phase 1A (North); the diversion works for the River Brent to enable the future extension of the shopping centre; and the provision of bridges across the A406 and diverted River Brent, including the replacement Tempelhof bridge and new pedestrian/cycle Living Bridge. The proposals

complete the networks for the Northern development and provide sufficient connections across and to the south of the A406 to facilitate the start of the Southern development.

The works tie in with the parallel RMA (reference 15/03315/RMA) for the hard and soft landscaping, ecology and bio-diversity enhancements and pedestrian and cycle path access for the central reach of the Riverside Park to be developed along the route of the diverted River Brent, which is also before Members at this committee.

Officers have worked closely with the Developers and their advisors and have liaised appropriately with other key stakeholders to ensure that the impacts of the development on the network continue to be robustly assessed and the mitigation package remains appropriate. The detailed design of the highways will continue through the separate technical approval process under section 278 and 38 agreements with the relevant highway authorities. At this stage the feasibility design and junction analysis work completed to date is considered acceptable in planning terms to allow the RMA to be approved.

The application has been subject to statutory consultation and a small number of third party representations have been made. No significant issues have been raised that have not been addressed or which would move officers to recommend refusal.

The application is in accordance with Development Plan policies and accords with the conditions and parameters approved in the Section 73 planning permission for the wider Brent Cross Cricklewood regeneration scheme.

Overall, officers find the proposals acceptable and recommend that the application is approved subject to conditions as outlined in Appendix 1.

LIST OF APPENDICES

APPENDIX 1 – CONDITIONS

APPENDIX 2 – DEVELOPMENT PROPOSALS INCLUDING S73 APPROVALS

APPENDIX 3 – EXISTING ROAD AND TRANSPORT CONDITIONS

APPENDIX 4 – PRE RESERVED MATTERS CONDITIONS

APPENDIX 5 – POLICY COMPLIANCE

APPENDIX 6 – OBJECTIONS AND OFFICER RESPONSES

APPENDIX 7 – CONFORMITY WITH REVISED DEVELOPMENT SPECIFICATION AND FRAMEWORK

APPENDIX 8 – JUNCTIONS APPROVED UNDER THE SECTION 73 PLANNING APPROVAL

APPENDIX 9 – SITE LOCATION PLAN

APPENDIX 1

Conditions and Informatives

Conditions:

1	Approved plans		
	The development hereby permitted shall be carried out in accordance with the following approved plans unless minor variations are agreed in writing after the date of this reserved matters consent with the Local Planning Authority:		
	Central Brent Riverside Park Plans:		
	Highways Alignment - General Arrangement Phase 1A North - Sheet 1	BXCR-URS-AH-RM-DR-CE-00001	P06
	Highways Alignment - General Arrangement Phase 1A North - Sheet 2	BXCR-URS-AH-RM-DR-CE-00002	P08
	Highways Alignment - General Arrangement Phase 1A North - Sheet 3	BXCR-URS-AH-RM-DR-CE-00003	P06
	Highways Alignment - General Arrangement Phase 1A North - Sheet 4	BXCR-URS-AH-RM-DR-CE-00004	P10
	Highways Alignment - General Arrangement Phase 1A North - Sheet 6	BXCR-URS-AH-RM-DR-CE-00006	P08
	Highways Alignment - General Arrangement Phase 1A North - Sheet 7	BXCR-URS-AH-RM-DR-CE-00007	P09
	Highways Alignment - General Arrangement Phase 1A North - Sheet 8	BXCR-URS-AH-RM-DR-CE-00008	P08
	Highways Alignment - General Arrangement Phase 1A North - Sheet 13	BXCR-URS-AH-RM-DR-CE-00013	P04
	Highways Alignment - General Arrangement Phase 1A North - Sheet 14	BXCR-URS-AH-RM-DR-CE-00014	P08
	Highways Alignment - General Arrangement Phase 1A North - Sheet 15	BXCR-URS-AH-RM-DR-CE-00015	P08
	Highways Alignment - General Arrangement Phase 1A North - Sheet 16	BXCR-URS-AH-RM-DR-CE-00016	P04
	Highways Alignment - General Arrangement Phase 1A North - Sheet 30	BXCR-URS-AH-RM-DR-CE-00030	P04
	Highways Alignment - General Arrangement Phase 1A North - Sheet 32	BXCR-URS-AH-RM-DR-CE-00032	P07
	Living Bridge (B7) - General Arrangement	BXCR-URS-B7-HS-AP-SE-0001	P05

Living Bridge General Arrangement 1	1065-09-001	F	
Living Bridge General Arrangement 2	1065-09-002	F	
Living Bridge Section 1	1065-09-003	D	
Living Bridge Section 2	1065-09-004	D	
Living Bridge Section 3	1065-09-005	C	
Living Bridge Section 4	1065-09-006	C	
Living Bridge Section 5	1065-09-007	D	
Living Bridge Planting Plan 1	1065-09-201	D	
Living Bridge Planting Plan 2	1065-09-202	C	
Living Bridge Planting Plan 3	1065-09-203	D	
Living Bridge Tree Pit in Hard Landscape	1065-09-401	C	
Living Bridge Tree Pit over Structure	1065-09-402	C	
Living Bridge Paving Detail 1	1065-09-403	B	
Living Bridge Paving Detail 2	1065-09-404	B	
Living Bridge Seating Type 1	1065-09-405	B	
Living Bridge Seating Type 2	1065-09-406	B	
Living Bridge Seating Type 3	1065-09-407	B	
Living Bridge Seating Type 5	1065-09-409	B	
Living Bridge Seating Type 6	1065-09-410	B	
Living Bridge Seating Type 7	1065-09-411	B	
Living Bridge Seating Type 8	1065-09-412	B	
Living Bridge Green Wall to Parapet	1065-09-413	C	
Living Bridge Planter Edge	1065-09-414	B	
Living Bridge Cycle Stand	1065-09-415	C	
Living Bridge Bollards to Southern Approach	1065-09-417	B	
Living Bridge Steps to Future Plot 93	1065-09-418	C	
Living Bridge Context Plan	SK-1372	09	
Living Bridge Plan	SK-1373	08	
Living Bridge Abutment Plan	SK-1374	04	
Living Bridge Soffit Plan	SK-1375	06	
Living Bridge East Elevation A	SK-1376	06	
Living Bridge East Elevation B	SK-1377	06	
Living Bridge Cross Section 01	SK-1401	06	
Living Bridge Cross Section 02	SK-1402	06	
Living Bridge Cross Section 03	SK-1403	07	
Living Bridge Cross Section 04	SK-1404	05	
Living Bridge Parapet Setting Out	SK-1405	07	
Living Bridge Long Section 05	SK-1406	06	
Southern Approach Balustrade Detail	SK-1407	05	
Living Bridge Parapet Detail	SK-1408	06	
River Bridge 01 - General Arrangement	BXCR-URS-C1-XX-AP-SE-00001	P05	
River Bridge 02 - General Arrangement	BXCR-URS-C2-XX-	P07	

	AP-SE-00001		
River Bridge 03 - General Arrangement	BXCR-URS-C3-XX- AP-SE-00001	P06	
River Bridge 04 - General Arrangement	BXCR-URS-C4-XX- AP-SE-00001	P07	
River Bridge 05 - General Arrangement	BXCR-URS-C5-XX- AP-SE-00001	P09	
River Bridge 06 - General Arrangement	BXCR-URS-C6-XX- AP-SE-00001	P08	
River Bridge 07 - General Arrangement	BXCR-URS-C7-XX- AP-SE-00001	P07	
River Bridge 08 - General Arrangement	BXCR-URS-C8-XX- AP-SE-00001	P06	
River Bridge 09 - General Arrangement	BXCR-URS-D1-XX- AP-SE-00001	P04	
River Bridge 10 - General Arrangement	BXCR-URS-D2-XX- AP-SE-00001	P03	
Tempelhof Bridge (B1) - Tempelhof Avenue Level General Arrangement	BXCR-URS-B1-HS- AP-SE-00001	P05	
Tempelhof Bridge (B1) - North Abutment Details	BXCR-URS-B1-HS- AP-SE-00002	P04	
Tempelhof Bridge (B1) - Pier 1 Details	BXCR-URS-B1-HS- AP-SE-00003	P04	
Tempelhof Bridge (B1) - Pier 2 Details	BXCR-URS-B1-HS- AP-SE-00004	P04	
Tempelhof Bridge (B1) - Pier 3 Details	BXCR-URS-B1-HS- AP-SE-00005	P04	
Tempelhof Bridge (B1) - South Abutment Details	BXCR-URS-B1-HS- AP-SE-00006	P04	
Tempelhof Bridge (B1) - South Approach Embankment & Tempelhof Link (Sheet 1 Of 3)	BXCR-URS-B1-HS- AP-SE-01001	P03	
Tempelhof Bridge (B1) - South Approach Embankment & Tempelhof Link (Sheet 2 Of 3)	BXCR-URS-B1-HS- AP-SE-01002	P03	
Tempelhof Bridge (B1) - South Approach Embankment & Tempelhof Link (Sheet 3 Of 3)	BXCR-URS-B1-HS- AP-SE-01003	P03	
Tempelhof Bridge - General Arrangement - North Approach Embankment	BXCR-URS-B1-HS- AP-SE-02001	P04	
Pedestrian Footbridge (B6) - General Arrangement	BXCR-URS-B6-HS- AP-SE-00001	P02	
Pedestrian Footbridge (B6) - Spiral Approach Ramp Details	BXCR-URS-B6-HS- AP-SE-00002	P03	
River Brent - General Arrangement Plan - Sheet 1	BXCR-URS-RB-XX- DR-HD-00008	P05	
River Brent - General Arrangement Plan - Sheet 2	BXCR-URS-RB-XX- DR-HD-00009	P05	

River Brent - General Arrangement Plan - Sheet 3	BXCR-URS-RB-XX- DR-HD-00010	P05	
Infrastructure Landscape GA	1065-06-001	K	
Infrastructure Landscape GA	1065-06-002	J	
Infrastructure Landscape GA	1065-06-003	I	
Infrastructure Landscape GA	1065-06-004	L	
Infrastructure Landscape GA	1065-06-005	L	
Infrastructure Landscape GA	1065-06-006	J	
Infrastructure Landscape GA	1065-06-007	J	
Infrastructure Landscape GA	1065-06-008	I	
Infrastructure Landscape GA	1065-06-009	J	
Infrastructure Landscape GA	1065-06-010	J	
Infrastructure Landscape GA	1065-06-011	H	
Infrastructure Landscape GA	1065-06-012	H	
Infrastructure Landscape GA	1065-06-013	J	
Infrastructure Landscape GA	1065-06-014	F	
Infrastructure Landscape GA	1065-06-015	F	
Infrastructure Landscape GA	1065-06-016	F	
Infrastructure Landscape GA	1065-06-017	H	
Infrastructure Landscape GA	1065-06-018	F	
Infrastructure Landscape GA	1065-06-019	G	
Infrastructure Landscape GA	1065-06-020	G	
Infrastructure Landscape GA	1065-06-021	F	
Infrastructure Landscape GA	1065-06-022	F	
Tree Retention and Removal	1065-06-050	H	
Tree Retention and Removal	1065-06-051	H	
Tree Retention and Removal	1065-06-052	H	
Tree Retention and Removal	1065-06-053	H	
Tree Retention and Removal	1065-06-054	H	
Tree Retention and Removal	1065-06-055	H	
Tree Retention and Removal	1065-06-056	H	
Tree Retention and Removal	1065-06-057	H	
Tree Retention and Removal	1065-06-058	H	
Tree Retention and Removal	1065-06-059	H	
Tree Retention and Removal	1065-06-060	H	
Tree Retention and Removal	1065-06-061	H	
Tree Retention and Removal	1065-06-062	H	
Tree Retention and Removal	1065-06-063	H	
Tree Retention and Removal	1065-06-064	H	
Tree Retention and Removal	1065-06-065	H	
Temporary Bus Station General Arrangement Option G	BXCR-URS-T5-HW- SK-CE-00017	P08	
Temporary Bus Stop Node North West Proposed Layout Level 02 Plan	SK-1466	05	
Temporary Bus Station South West Proposed Layout Level 02 Plan	SK-1470	05	
Temporary Bus Stop Node North West Proposed Information Kiosk (Reuse	SK-1482	01	

	Existing) Layout & Elevations		
	Temporary Bus Station South West Proposed Information Kiosk Layout & Elevations	SK-1495	02
	Temporary Bus Station South West Proposed Mess Room Layout & Elevations	SK-1496	02
	Temporary Bus Station South West Tfl Standard Shelters Plans And Elevations	SK-1497	01
	Temporary Bus Station South West & Temporary Bus Stop Node North West Proposed Sections	SK-1498	01
	<p>Reason: For the avoidance of doubt and in the interests of proper planning and so as to ensure that the development is carried out fully in accordance with the application as assessed in accordance with policies CS1, CS4, CS5, of the Barnet Local Plan and policy 1.1 of the London Plan.</p>		
2	<p>Prior to commencement of the development within Phase 1A (north) full details of the access ramp between Brent River Corridor and Templehof Avenue shall be submitted to and approved in writing by the Local Planning Authority. These shall include though not be limited to:</p> <ul style="list-style-type: none"> • Gradient • Handrails • Surface Materials <p>Reason: To ensure that the development attains the highest level of accessibility in accordance with Saved UDP Policies C4 and C7, Core Strategy policy CS9; London Plan (FALP) 2015 policies 3.1, 6.9, 6.10, 7.1 and 7.2 and with the Inclusive Access Strategy approved under condition 1.26 of Planning application F/04687/13 approved on 23 July 2014.</p>		
3	<p>The operating hours of the Temporary Bus Stop Node Northwest on Plot 113 hereby approved shall be between 7:30am and 9:30pm on weekdays and Saturdays, and between 9:00am and 9:30pm on Sundays and Bank Holidays and no buses shall use the bus stops within the Bus Stop Node Northwest outside of these hours unless and until:</p> <p>a) it can be demonstrated following a 3 month period of monitoring from the date on which the Temporary Bus Stop Note Northwest shall first come into use that the level of noise emitted from the operation of the Temporary Bus Stop Node on Plot 113 is at least 5dB(A) below the background level, as measured from any point 1 metre outside the window of any room of a neighbouring residential property; and</p> <p>b) a satisfactory noise report shall be submitted to the Local Planning</p>		

	<p>Authority presenting the findings of the 3 month period of noise monitoring set out in part a) of this condition; and</p> <p>c) subject to Local Planning Authority's approval in writing of the noise report to be submitted under part b) of this condition, the maximum hours of operation for the Temporary Bus Stop Node shall be between 6:30am and 10:30pm on weekdays and Saturdays, and between 8:00am and 10:30pm on Sundays and Bank Holidays.</p> <p>Reason: To ensure that the proposed development does not prejudice the enjoyment of the occupiers of adjoining residential properties in accordance with Policy DM04 of the Development Management Policies DPD (adopted September 2012) and Policy 7.15 of the London Plan (2011).</p>
4	<p>Notwithstanding the details shown on the plans submitted and otherwise hereby approved, the Temporary Bus Stop Node Northwest on Plot 113 shall not be brought into use until full details and specification of all acoustic walls, fencing and other acoustic barriers to be erected on the site in accordance with the locations shown on plan SK 1466 Rev.05 have been submitted to the Local Planning Authority and approved in writing. The approved acoustic walls, fencing and barriers shall be implemented in accordance with the details as approved before the Temporary Bus Stop Node Northwest is brought into use.</p> <p>Reason: To ensure that the proposed development does not prejudice the enjoyment of the occupiers of adjoining residential properties in accordance with Policy DM04 of the Development Management Policies DPD (adopted September 2012) and Policy 7.15 of the London Plan (2011).</p>
5	<p>The Temporary Bus Station shall not be in operation for a period of more than 4 (four) years prior to the use ceasing in this location following the delivery and opening of the Permanent bus station unless otherwise agreed in writing.</p> <p>Reason: In the interest of the amenity of neighbouring residents and the ease of operation of TFL bus services.</p>
6	<p>Prior to any works to the designated Taxi rank on Prince Charles Drive adjacent to Brent Cross Bus station or any works that restrict access by the Taxi or passengers to this taxi rank provide details of alternative facility that is fully accessible for wheelchair users, sufficient capacity for at least 9 cabs to rank, and is clearly visible to customers exiting the shopping centre and appropriate sign posted within the shopping centre itself. The details of these facilities should be agreed with TfL and submitted to the LPA for approval, and the facilities operated in accord with the approved plans.</p> <p>Reason: To ensure the shopping centre and nearby facilities are fully accessible by Taxi during construction phase.</p>

7	<p>Prior to commencement of the development within Phase 1a (north) a detailed surface water pollution construction method statement shall be submitted to, and approved in writing by, the local planning authority. The construction method statement will demonstrate how pollution to surface water will be prevented during the alteration and diversion works to the river Brent..</p> <p>Reason: To prevent pollution of surface water from accumulated pollutants during the construction phase.</p>
8	<p>Prior to commencement of the development within Phase 1A (north) details shall be submitted to and approved in writing by the local Planning Authority of all retaining walls to be constructed in Phase 1A (North) the Retaining Walls which are currently indicatively shown. Plans Elevations Sections and details materials shall be submitted.</p> <p>Reason: In the interest of the visual appearance of the development and highway safety.</p>
9	<p>Until detailed information has been submitted to and approved in writing by the Local Planning Authority as a part of Phase 1B (North) the Coach/Taxi Drop off identified as 'Plot 109 COACH/TAXI DROP OFF' on plan'BXCR-URS-AH-RM-DR-CE-00007.Rev P09' shall be made inaccessible to motor vehicle access.</p> <p>Reason: In the interest of the safety of Pedestrians and other road users.</p>
10	<p>No development within Phase 1a (north) shall commence until the developer has secured the implementation of a programme of archaeological works in accordance with a written scheme of investigation which has been submitted by the developer in consultation with GLASS and approved in writing by the local planning authority. Thereafter the developer shall ensure that the programme of any archaeological works is fully implemented and that all recording and recovery of any archaeological evidence within the development site is undertaken to the satisfaction of the planning authority and GLASS.</p> <p>Reason: To enable archaeological investigation of the site and to minimise or mitigate harm to remains of archaeological interest.</p>
11	<p>Prior to the commencement of work, details of the proposed surface water management strategy and drainage network shall be submitted to the LPA for their written approval.</p> <p>Reason: To ensure appropriate drainage infrastructure to comply with policy CS13 of the Local Plan Core Strategy (Adopted September 2012) The Sustainable Design and Construction SPD (adopted April 2013) and Policies 5.13 and 5.14 of the London Plan FALP (March 2015).</p>
12	<p>Details of works to improve the environmental quality of Underpass U3 (as defined in BXC01 Parameter Plan 002) shall be submitted to the Local Planning Authority prior to work commencing on the eastern road</p>

	<p>infrastructure and such works as approved shall thereafter be implemented prior to the opening of Phase 1B(North) and shall include the following works (not exhaustive):</p> <ul style="list-style-type: none"> - Lighting - Signage - Surface improvements <p>Reason: In the interest of Pedestrian and Cyclist movement and security.</p>
--	--

Informatives:

1	<p>The applicant is advised that the costs of any associated works to the public highway, including reinstatement works, will be borne by the applicants and may require the applicant to enter into a 278 Agreement under the Highways Act 1980. Detailed design will have to be approved by Traffic & Development Team – Development and Regulatory Services.</p>
2	<p>The Environmental Statement Further Information Report accompanying this RMA has highlighted a potentially major adverse wind impact on Tempelhof Bridge which is driven by the maximum height massing of the future plots alongside the A406 and which may result in high wind speeds only slightly below the safety threshold for considerably longer periods than those identified in the ES FIR. The ES FIR advises that mitigation is not required at this stage as the maximum build heights are unlikely to be delivered. The ES FIR also makes reference to contingency mitigation that could be introduced on Tempelhof Bridge, comprising of an additional 1.5m high solid barrier along the bridge sides. The inclusion of the barrier has not been verified in wind tunnel testing and the impact arising from the buildings to be constructed adjacent to the A406 cannot be confirmed until the design stage is concluded. The applicants' attention is therefore drawn to the need for future RMA development to be supported by a detailed wind study to ensure that wind impact is properly assessed and mitigation can be designed in if required.</p>
3	<p>Full details of the western Environment Agency access Ramp from Perimeter Road to the Brent River Corridor shall be submitted as a part of details to be submitted under Phase 1B (North). Such submissions shall include in relation to the EA Access Ramp details of controls or barriers of entry to cars and motorbikes; details of both pedestrian and cycle accessibility including any resting places proposed for wheelchair users, raised paving for pedestrians; gradients, hard and soft</p>

	landscaping.
4	With the submission of Phase 1B (North) reserved matters applications Details of the permanent Taxi Rank will be submitted. The Taxi rank will be required by TFL to be: fully accessible for wheelchair users; have sufficient capacity for at least 9 cabs to rank (and additional based on forecast of future demand for the shopping centre and other uses nearby); clearly visible to customers exiting the shopping centre with appropriate signage posted within the shopping centre itself. Such details shall have been agreed in discussion with TFL prior to their submission and the facilities operated in accord with the approved plans.
5	The commencement of works to the Brent River corridor will require application to the Environment Agency for 'Flood Defence Consent'. Any changes to the flood model as reviewed by the Environment Agency at the time of Reserved Matters application , including but not limited to changes to the modelled roughness values of the River corridor will be expected to be highlighted to the Environment Agency within the submission. It is important for the developer to engage early with the Environment Agency prior to submitting a Flood Defence Consent application as the statutory determination period for such applications is 2 months and issues may take longer than this to resolve, particularly if changes to the modelling found to be acceptable at Reserved Matters Stage are submitted.”
6	The Reserved Matters Application for 1B (North) shall include details of the 24 hour pedestrian through route for the shopping centre.
7	Notwithstanding the information submitted in the Environmental Statement Further Information Report, details of the design and construction of the acoustic barrier to be erected between the A406 and Riverside Park shall be submitted for the approval of the planning authority.
8	The adoption of any infrastructure by the Council or any other highways authority will be subject to an appropriate commuted sum to cover future maintenance costs.
9	<p>In accordance with Reg 3 (4) and Reg 8 (2) of the Town and Country Planning (Environmental Impact Assessment) Regulations 2011, it is considered that:</p> <p>this submission of reserved matters reveals , with regard to the subject matter of the application, that there are no additional or different likely significant environmental effects than is considered in the environmental information already before the Council (the Environmental Statement (ES) (BXC02) submitted with the Section 73 application (F/04687/13) and any further and/or other information previously submitted;</p> <p>and</p> <p>the environmental information already before the Council (the ES submitted with the Section 73 application, and any further and/or other information previously submitted) remains adequate to assess the environmental effects of the development.</p>

This page is intentionally left blank

APPENDIX 2

Description of Proposed Infrastructure including that Approved under the 2014 S73 Consent

Scale copies of the plans of the proposed road layouts are available on request from the Planning Service 020 8359 7548.

Overview - Integrated Transport Strategy

The S73 Permission defines the multi-modal Integrated Transport Strategy (ITS) developed to satisfy the transport needs and mitigate the transport impacts of the BXC regeneration site.

The ITS is comprehensively reviewed and detailed within the S73 Permission. At the core of the ITS is the provision of a fully integrated and, as far as is achievable, sustainable public transport network of appropriate travel modes. These modes will provide a good quality and attractive level of service provision for those wishing to access both the BXC site and the surrounding area whilst at the same time maintaining the same level of service on the highway network so that drivers are no worse off than had the scheme not been developed.

In keeping with the Development Framework vision, the ITS will assist in the delivery of:

- Regeneration, sustainability and minimising car use whilst recognising that the private car will continue to be a significant mode of travel in the BXC site;
- A new destination in outer London which will provide a focus for trip making as an alternative to the predominant movements into and out of central London;
- A mixed-use development to encourage shorter trips between local destinations and make use of alternatives to car travel; and
- Public realm improvements at a human scale that provide a barrier-free environment for pedestrians and cyclists.

The aim of the ITS is therefore:

'To make substantial provision for Public Transport and other, more sustainable modes, whilst making suitable provision for appropriate, additional car travel.'

The implementation of Phase 1 of the ITS is an important first step in this process and Phase 1A (North) begins the process to ensure the appropriate level of infrastructure is delivered in line with the infrastructure triggers and levels of predicted demand. Phase 1 provides most of the infrastructure necessary to deliver future phases up to end state. The majority of the Phase 1 infrastructure is constructed as part of Phase 1A (North).

Overview - Cricklewood, Brent Cross and West Hendon Regeneration Area Development Framework 2005

The ITS represents a comprehensive and achievable phased plan for delivering the transport vision articulated in the Cricklewood, Brent Cross and West Hendon Regeneration Area Development Framework 2005. The key principles of the Transport Vision within this Development Framework are:

- Create a new outer London town centre that will reduce journeys into central London during periods of peak travel demand;
- Mixed-use development creating opportunities for short local trips and linked trips especially by non-car modes of travel;
- New and improved public transport services providing additional capacity, new links between areas and an overall higher standard of service;
- Integration between travel modes to facilitate access by public transport to the area;
- Improved accessibility and convenience to walking and cycling routes to make journeys easier and more attractive; and
- Limiting new parking associated with development to discourage the use of the car for non-essential journeys.

The Transport Vision recognises that: *'sustainable development on the scale envisaged within the Development Framework is only feasible if a high proportion of new trips can be made by non-car modes'*, albeit with the likelihood the car will remain an important element of travel in outer London.

Roads

The majority of the infrastructure improvements associated with the scheme will be delivered during Phase 1 in preparation for the proposed development due to come forwards during the subsequent phases.

The S73 Permission provides detailed permission for improvement works at the following junctions and thus no reserved matters application is necessary:

(a) M1/A406 and A5/A406 (Staples Corner)

The scheme proposes to alter and improve the configuration of the two existing M1 / A406 and A5 / A406 junctions to effectively operate as one large interchange and provide additional traffic capacity.

Pedestrian and cycle improvements at this junction include the following:

- Provision of a toucan crossing across the A5 on the northern side of Staples Corner
- Provision of continuous off-road shared footway/cycleway facilities alongside both the eastbound and westbound A406 carriageways linking from the A5 toucan crossings and Bridge B6 (pedestrian and cycle bridge);
- Bridge B6 (pedestrian and cycle bridge) over the A406 linking with shared footway/cycleway facilities on either side;
- Replacement of the pedestrian bridge to the west of Bridge B6 (west of the rail arches). The new ramp on the northern side of the bridge will be wider than the existing provision to benefit mobility impaired users; and
- General refurbishment of the remaining existing Staples Corner footbridges (lighting/painting etc.).

(b) A41/A406 Junction

The scheme proposes to modify and improve the A41 / A406 junction to provide additional traffic capacity. The works include the provision of a new slip road directly off the A406 eastbound slip road and the mid-level roundabout providing direct access into BXSC, together with the closure of the existing slip road from the A406 westbound into Brentfield Gardens. There will be a new wider bridge provided over the A406 on the eastern side of the mid-level roundabout.

Improvements to the pedestrian and cycle facilities include improvements to the underpasses on either side of the A406 (underneath the A41) to increase headroom clearances and the provision of off-road shared footway/cycleway facilities. Improvements on the northern side of the junction will allow pedestrians and cyclists to travel between Haley Road/Cooper Road and the River Brent Corridor via an off-road continuous facility. On the southern side of the junction, improvements will allow pedestrians and cyclists to travel between Brentfield Gardens and Tilling Road via a continuous off-road facility.

(c) A5/A407 Cricklewood Lane

Significant improvements are proposed. To enable the reconfiguration of this junction, demolition of a property on the south-east corner of the intersection of the A5 and A407 is required

Pedestrian and cycle improvements include provision of advanced cycle stop lines on all junction approaches to improve the visibility of cyclists to drivers, avoid conflicts with left turning vehicles, and allow cyclists to reach a safe position to turn right. In addition, a new controlled pedestrian crossing across the A407 Cricklewood Lane approach to the junction is to be implemented.

(d) A407 Cricklewood Lane/Claremont Road

The scheme improves the existing A407 Cricklewood Lane/Claremont Road junction, including widening of the southbound (Claremont Road) and eastbound (A407) approaches to enhance traffic capacity.

Pedestrian and cycle improvements include provision of cycle advanced stop lines on all junction approaches to improve visibility of cyclists to drivers, avoid conflicts with left turning vehicles, and allow cyclists to reach a safe position to turn right. Improvements to footway facilities at the junction will include localised footway widening (on the Claremont Road arm) and improvement to the existing tactile paving layouts. The remaining footways at the junction will stay as existing.

(e) A406 Brent Cross Access/Egress Junction

Access into the Brent Cross East Development Zone off the A406 is currently provided by a slip road from the A406 eastbound exit slip road from the M1 / A406 junction. The scheme proposes to improve this access arrangement by realigning the access and provide a new egress onto the A406 eastbound slip road at the same location.

Pedestrian and cycle improvements include provision of cycle advanced stop lines on all junction approaches to improve visibility of cyclists to drivers, avoid conflicts

with left turning vehicles, and allow cyclists to reach a safe position to turn right. Improvements to the footway facilities at the junction will include localised footway widening (on the Claremont Road arm) and improvement to the existing tactile paving layouts. The remaining footways at the junction will remain as existing.

(f) A41 Southbound On-Slip

A new southbound on-slip road will be provided directly from the junction gyratory. The slip road will run parallel to the A41 and merge just south of the Woodville Gardens.

Public Transport – bus stops

The public transport improvements comprising Phase 1A (North) consist of improved bus stopping facilities. Waiting conditions at a number of existing bus stops are to be improved and new bus stops are also being provided in Phase 1A (North) in preparation to accompany new and altered services. The bus stop locations along the highways are illustrative, and have been based on a review of likely need and these are set out in detail below:-

New and Modified Bus Stops

Provision	Location	Description
Bus Stop	A5 Edgware Road (NB) in the vicinity of Staples Corner	Replacement bus stop provided as part of the improvements to Staples Corner
Bus Stop	A5 Edgware Road (SB) in the vicinity of Staples Corner	Replacement bus stop provided as part of the improvements to Staples Corner
Bus Stop	Tilling Road West (EB) approximately 20m west of roundabout	New bus stop provided as part of the Tilling Road West re-alignment and diversion
Bus Stop	Tilling Road West (WB) approximately 60m west of roundabout	Replacement bus stop provided as part of the Tilling Road West re-alignment and diversion
Bus Lane	Tempelhof Avenue & Tempelhof Bridge (NB)	New bus lane provided on the western side of the new Tempelhof Avenue and replaced Tempelhof Bridge
Bus Lane	Tempelhof Bridge (SB)	New bus lane provided on the eastern side of the replaced Tempelhof Bridge
Bus Stop	Tempelhof Avenue (NB)	New bus stop provided on the western side of the new Tempelhof Avenue
Bus Stop	Tempelhof Avenue (SB)	New bus stop provided on the eastern side of the new Tempelhof Avenue
Bus Stop	A406 North Circular Road on-slip (WB) approximately 210m to the southwest of the A406 / A41 junction	Replacement bus stop provided on the on-slip for the A406

Bus Stop	Tilling Road East (NB) approximately 130m north of the Tesco access	Replacement bus stop provided on the western side of Tilling Road East
Bus Stop	Tilling Road East (SB) approximately 70m north of the Tesco access	New bus stop provided on the eastern side of Tilling Road East
Bus Stand	North of the Prince Charles Drive eastern roundabout	New bus stand provided to the north of the roundabout for the shopping centre
Bus Stop	Spalding Road (NB)	Replacement bus stop provided on the western side of Spalding Road as part of realignment works
Bus Stop	Brentfield Gardens (EB) approximately 40m west of Highfield Avenue	Replacement bus stop provided on the northern side of Brentfield Gardens as part of the extension works
Bus Stop	A41 Hendon Way off-slip (WB) approximately 200m to the southeast of the A406 / A41 junction	Replacement bus stop provided on the A41 Hendon off-slip for the A406 / A41 junction
Bus Stop	A407 Cricklewood Lane (EB) approximately 60m north of junction with A5 Edgware Road	Replacement bus stop provided on the northern side of the A407 Cricklewood Lane
Bus Stop	A407 Cricklewood Lane (WB) approximately 60m north of junction with A5 Edgware Road	Replacement bus stop provided on the southern side of the A407 Cricklewood Lane

Car parking

There is a requirement to review the Brent Cross Shopping Centre parking charges with the local planning authority every three years. The expectation is that blue badge holders will be able to park for an unlimited time and free of charge.

The off-street public parking areas serving the town, neighbourhood and community centres will be managed by the use of parking charges in such a way as to dissuade all day commuters parking.

In the residential areas, 1 space per residential unit for Phase 1 of the development is proposed, with an aspiration for a progressive reduction in provision towards 0.7 spaces per residential unit via review and monitoring.

The number of disabled spaces currently in the shopping centre car parks is approximately 3% of the total, which accommodates demand. An increase to 5% (380 spaces) is proposed (similar to Westfield) with an additional 5% of spaces provided as enlarged standard bays that could be converted at a later date. As the London Plan requires six per cent of the spaces to be designated blue badge parking

bays, there is a commitment to monitor and review the number of disabled parking spaces.

Staff employed within the shopping centre are expected to pay for parking and they will be managed by the Individual Travel Plans that will be prepared by the employers as each plot comes forward.

Parent and Child Parking will be developed through dialogue with the major retailers within the shopping centre.

The number of parking spaces with electric vehicle charging points will be provided in accordance with the discharge of Condition 39.7. For retail use, the London Plan identifies a need for 10% active provision and 10% passive provision.

Within each car park, there will be dedicated motorcycle parking areas. Parking within these areas will initially be free of charge. The expectation is that the proportion of motorcycle parking spaces will at least match the existing level of provision.

Computer controlled variable message signs, linked to the parking management system, will direct drivers to car parks with spaces available. These signs will be utilised to manage the exit arrangements from the car parks.

A site-wide Car Club will be established prior to occupation of 200 dwellings.

Motorcycles

Within each Brent Cross Shopping Centre Car Park there will be dedicated motorcycle parking areas. Parking within these areas will initially be free of charge. Motorcyclists parking in standard parking bays will be charged the standard customer parking tariff.

The expectation is that the proportion of motorcycle parking spaces will at least match the current level of provision and will be confirmed at the appropriate design stage. In the event that Automatic Number Plate Recognition is the preferred payment system, it is likely motorcyclists will eventually be charged to park at the shopping centre although the tariff is likely to be lower than the charge for cars.

Coach Stops

The coach stop on the southern side will be relocated further along the slip road, whilst on the northern side, due to implementation of the new eastern roundabout and slip roads, the authorities and operators have agreed to relocate the coach stop to Haley Road between the Shirehall Lane roundabout and the slip road onto the A41. This results in slightly longer travel distances for coaches in the vicinity of the A406 / A41 junction, but is considered in the circumstances to be an acceptable solution, and provides good passenger connections to residential areas, the BXSC and nearby bus stops.

APPENDIX 3

Existing Site Context and Road Conditions

Highways

The consented development site is located in the southern part of the London Borough of Barnet. The site is bounded by Park Road to the north, the A41 to the east (including Brent Cross Underground Station), the A5 and the border with London Borough of Brent to the west and the A407 Cricklewood Lane to the south.

The following existing local roads are of importance to the BXC Phase 1A (North) RMA, have a 30 mph speed limit and are the responsibility of the London Borough of Barnet except where indicated:

Prince Charles Drive provides access to BXSC from the A41 and from the existing urban areas on the eastern side of the A41 Hendon Way. It is a single carriageway two lane road. It also connects to the A406 entry slip road and to Tempelhof Avenue, at a roundabout in the west area of BXSC.

Claremont Road is a key north-south distributor for local traffic south of the A406. It passes between Tilling Road to the north and the A407 Cricklewood Lane to the south. It is a single carriageway two lane road. Claremont Road provides access to Marble Drive, The Vale and Clitterhouse Crescent.

Tilling Road is a key east-west distributor road for local traffic and existing commercial developments to the south of the A406, including via Brent Terrace North. It also provides important access to the Eastern Lands area of the site. It runs parallel to, and to the south of, the A406 between its intersections with the A41 in the east and the M1 in the west. It is a single carriageway two lane road.

Tempelhof Avenue provides a vital link across the A406 between Tilling Road and BXSC. It is a single carriageway, two lane link road on a narrow bridge over the A406.

Chichele Road provides access to/from southwest of the development area for local traffic, as well as from Willesden Green and Dollis Hill to the southwest of the scheme. It is a single carriageway two lane road. On-road parking is present on both sides of the carriageway. The road links with the A5 and A407 via the Chichele Road / Cricklewood Broadway (A5) / Cricklewood Lane (A407) junction. Responsibility for the road lies with the London Borough of Brent.

Edgware Road (A5) links central London to towns in Hertfordshire and beyond. It is predominantly single carriageway in the vicinity of the BXC site, with its width varying between 10m and 13m. The A5 is a dual carriageway with two lanes in each direction south from Staples Corner to the junction with Humber Road. South from this point the road becomes a single carriageway with two lanes in each direction.

Between its junction with Gladstone Park Gardens and the junction with the A407 Cricklewood Lane/ Chichele Road, the character and provision of the A5 varies, consisting of a mix of single lane sections, single lane sections (with bus lanes) and two lane sections in both directions.

The A5 intersects with the A406 North Circular Road via slip roads from the main carriageways of both roads, which meet at an at-grade roundabout at Staples Corner which is immediately to the west of the M1/A406 gyratory. The main carriageway of the A5 passes on a flyover above this roundabout and below the main carriageway of the A406, which is on a further flyover above the A5. It has a speed limit of 40mph in the vicinity of the site.

Responsibility for the A5 in the vicinity of the BXC site is shared between the London Boroughs of Barnet and Brent, as the borough boundary passes down the centreline of this road. TfL has oversight as it forms part of the Strategic Road Network (SRN) as well as direct responsibility for bus operations and traffic signals.

A407 Cricklewood Lane provides access to the existing Cricklewood Station. It also provides access to the BXC site from Cricklewood, Golders Green and Willesden. It intersects with both the A5 and A41 via at-grade traffic signal controlled junctions. It is a single carriageway two lane road.

Brentfield Gardens is a single carriageway two lane road which links with Tilling Road, Highfield Avenue and the A41. This road provides links to the residential areas to the south of the existing shopping centre and the south of the A406, as well as to the east of the development and the wider Brent Cross area.

Pennine Drive is divided by a roundabout at its centre point. The single carriageway two lane road has junctions with Claremont Road to the west and Hendon Way (A41) to the east. It also forms junctions with Cheviot Gardens, Cumbrian Gardens, Cleveland Gardens and Purbeck Drive. This road connects this residential area to the south of Clitterhouse Playing Fields with the A41 and subsequently the A406.

Lichfield Road links the south of the development with the West Hampstead area. The road is a single carriageway two lane road with parking along both sides of the carriageway. Lichfield Road forms a junction with the A407 Cricklewood Lane, and also forms junctions with a number of residential side roads.

Brent Cross Shopping Centre access is inbound to Prince Charles Drive from the A406 eastbound exit from the M1/A406 junction, inbound and outbound along Tempelhof Avenue over the A406 North Circular Road off Tilling Road, inbound and outbound along Prince Charles Drive under the A41 off Renters Avenue/Cooper Road providing access to the A406 and A41 and outbound via two exits: onto the A41 northbound carriageway, from Spalding Road; and a separate BXSC car park exit.

Road access to the area of the site to the south of the A406 is limited. Several sites to the north of this area, including the retail developments, are accessible only from Tilling Road. Tilling Road is accessible from the northbound A41 and westbound

A406 (at its eastern end). Tempelhof Avenue provides a link from Prince Charles Drive to Tilling Road (via Tempelhof Bridge over the A406).

Access to the Eastern Lands area is provided via Tempelhof Link Road and a mini roundabout on Tilling Road for the northern retail sites, via Claremont Road for the school facilities and Marble Drive / Prayle Grove (via Claremont Road) for residential and recreational land uses.

Brent Terrace North is a route into the west of this area from Tilling Road that serves the Claremont Way Industrial Estate and the Waste Transfer Station. There are several routes from the A41 into the area from the east between Tilling Road and A407 Cricklewood Lane, principally The Vale that forms a signal controlled all movement junction with the A41 and connects with Claremont Road. There are no connections from the A5 Edgware Road into the site between the A406 and A407 Cricklewood Lane.

Parking: On and Off Street

Parking in and around the shopping centre is provided by way of controlled on-street zones, charged on-street bays, charged public off-street car parks, free off-street car parks and extensive free on-street car parking particularly in residential areas south of the A406.

There is free parking at Brent Cross Shopping Centre and during the hours of opening access is uncontrolled. There are approximately 7,600 spaces provided in a number of separate car parks. The largest car parks are multi storey car parks each with a capacity of just over 2,000 spaces. There are approximately 237 disabled spaces which represent around 3.5% of the total. There are also 68 parent and child spaces and a small number of spaces set aside for customer collections.

The majority of car parking spaces are located to the north of Prince Charles Drive with two points of access at either end of the shopping centre. Both accesses are barrier controlled and during the week open at 08:45 hours. The barrier controls are in place to deter all day commuter and staff parking.

There are three car parks located to the south of Prince Charles Drive. The easternmost car park is not used for public parking. The other two car parks have a combined capacity of just over 1,000 spaces and remain open all day. As a result they tend to be used by staff at the shopping centre.

Car parking surveys were undertaken at the Shopping Centre on Wednesday 26th, Friday 28th and Saturday 29th June 2013.

A total of 7,429 spaces within nine car parks were surveyed. The shopping centre car park is at its busiest on a Saturday at 14:00 hours with the northern and then western car parks forming the key parking areas for vehicles. During the week a similar pattern is seen on both Wednesday and Friday, when the car parks reach their peak parking occupation at around 13:00 hours. Maximum occupancy on any of the three days surveyed did not exceed 4,500 vehicles.

The average duration of stay was just over an hour and a half. Almost one quarter of visitors to the shopping centre stayed for less than half an hour, more than two thirds stayed for less than two hours and 85% stayed for less than three hours. The vast majority of those staying for more than eight hours were parked in the car parks to the south of Prince Charles Drive.

There was a significant amount of spare capacity in most areas of disabled parking with the only area fully utilised being Level 1 of both multi storey car parks. Detailed monitoring of the use of disabled parking bays at the shopping centre is included in the BXC Monitoring Strategy.

Public Charged Off-Street Parking

There is limited public off-street parking in the adjacent area:

- Warner Close in West Hendon, just to the west of the A5: 72 spaces including two disabled bays;
- Brent Cross Underground Station: 25 spaces including two disabled bays; and
- Hendon Thameslink Station: 41 spaces including 10 disabled bays.

There are no parking facilities at Hendon Central or Golders Green Underground Stations or at Cricklewood Railway Station which is on the Thameslink line.

Private Off-Street Parking

There are a number of privately owned off-street car parks in the BXC area, the most significant car parks being:

- Tesco: 440 spaces and 20 disabled bays;
- Toys R Us: 127 spaces and 3 disabled bays;
- Holiday Inn Hotel: 142 spaces and 8 disabled bays;
- Brent Cross Retail: Park 280 spaces and 20 disabled bays;
- Hendon Leisure Centre: 36 spaces and 2 disabled bays; and
- Broadway Retail Park: 487 spaces and 13 disabled bays.

Controlled On-Street Parking Zones

There are many Controlled Parking Zones (CPZs) within and surrounding the Brent Cross Regeneration Area. Their primary function in the area is to restrict commuter parking in the vicinity of underground and mainline railway stations. The adjacent CPZs are:

Name Location Detail of Parking Controls

Brent Cross (B): Residential roads adjacent to shopping centre: 10:00-18:30 hours Monday-Saturday.

Brent Cross Station (BX): Residential area up to 600m from station: 11:00-12:00 hours Monday-Friday.

Cricklewood1 (C1, CW, CT): (CW,CT) Area bound by the A5, Cricklewood Lane and the Railway Lands, (C1) Area around Cricklewood Lane, to Finchley Road and The Vale/Cloister Road to the north: (CW, CT) All day zones – 09:00-22:00 hours Monday-Sunday (C1) One hour zone – 10:00-11:00 hours Monday- Friday.

Golders Green (G, H): (G) All day zone boundary extends from Elmcroft Avenue and railway tracks to the north. The Ridgeway to the south, Finchley Road to the east and Gresham Gardens to the west. (H) One hour zone boundary extends from The Ridgeway to the north, Wayside/Crewys Road to the south, Finchley Road to the east and Gresham Gardens to the west: (G) All day zones 08:00 -18:30 hours (H) Controlled parking zone – 11:00-12:00 hours Monday-Friday.

Hendon Central 1 (HC1): A41 Watford Way runs from the south-east boundary and then through the middle of the CPZ, Park Road to the south, A41 Hendon Way and Raleigh Close to the East, Brampton Grove to the north and Audley Road to the west: 11:00-12:00 hours Monday-Friday.

Hendon Central 2 (HC2): St Mary Crescent/Church Road to the north, Brampton Grove to the south, Brent Street to the east and The Burroughs to the west: 10:00-17:00 hours Monday-Friday.

Hendon Central 3 (HC3): Cheyne Walk to the north, Hendon Way to the south and Denehurst Gardens to the east: North (North of Beaufort Gardens) – 08:00-18:30 hours Monday-Friday. South (south of Beaufort Gardens) – 11:00-12:00 hours Monday-Friday.

West Hendon 1 (WH1): Park Road to the south, the A5 to the west and the M1 to the east: 09:30-16:30 hours Monday-Friday.

West Hendon 2 (WH2): A5 Hendon Broadway¹ to the east, Garrick Road to the north, Brent Reservoir to the west, Verulam Court to the south: 10:00-11:00 hours Monday-Friday.

West Hendon 3 (WH3): Station Road to the north, Park Road to the south, Audley Road to the east and the M1 to the west: Various parking time restrictions – majority 10:00-11:00 hours Monday-Friday.

Brent (GA, MA, GM): (MA) A5 Cricklewood Broadway to the north east, Chichele Road to the north west, Owain Lane to the south (GM) Wotton Road to the north, A5 Cricklewood Broadway to the east, Olive Road to the south, Gladstone Park to the West (GA) Olive Road to the north, Chichele Road to the east, Melrose Avenue to the south, Gladstone Park to the west: (MA, GM) 10:00-21:00 hours Monday – Saturday (GA) 10:00-15:00 hours Monday – Friday.

Camden (CA-P, CA-Q): (CA-P) University College Sports Ground to the north, Fortune Green Road to the east, Minster Road to the south, Westbere Road to the west (CA-Q) Richborough Road to the north, Fordwych Road to the east, Minster Road to the south, A5 Cricklewood Broadway to the west: (CA-P) 10:00-12:00 hours Monday – Friday (CA-Q) 08:30-18:30 hours Monday-Friday.

The introduction of parking charges at Brent Cross Shopping Centre is likely to result in some parking being displaced into surrounding areas in order to avoid the parking charges. Uncontrolled parking areas or areas that are controlled for a single hour in the morning might, for example, be susceptible to displaced parking from the

Shopping Centre. Observations on parking demand were undertaken on Wednesday 4th, Friday 6th and Saturday 7th of December 2013, from 10am to 12pm and 2pm to 4pm to identify areas which may be vulnerable to additional parking pressures as the redevelopment of the Regeneration Area progresses.

Uncontrolled Residential Car Parking Zones

There are several areas of uncontrolled parking within and surrounding the regeneration area. The largest area is immediately south of the A406 and surrounds Clitterhouse Playing Fields. There is another significant area to the west of the A5 in the Dollis Hill area and a further area to the north east of the shopping centre close to Hendon Park. These areas are all potentially vulnerable to displaced parking as the redevelopment of the regeneration area progresses.

Private Car Parking Zones

Private car parking zones are areas where parking is not subject to the parking regulations of the Local Authorities. There are several private parking areas within and surrounding the Brent Cross Regeneration Area:

Parking Controls Number of Bays

Private Resident Car Parks: 1842 spaces / 35 disabled bays / 9 visitor bays

Foyer Parking: 5 spaces

Private On-Street Parking (Kerbside): 176 spaces

Uncontrolled Kerbside Parking Within Private Road: 30 spaces

Pedestrians and Cyclists

As required by Condition 1.20 of the S73 Permission and defined in Schedule 1, 3, 15 and 17 of the S106 Agreement, an Area Wide Walking and Cycling Study (AWWCS) has been submitted to the council.

Since 2008 there have been no specific schemes in the immediate area which have upgraded either the pedestrian or cycle environment or provided additional facilities. There have been some improvements to pedestrian and cycle facilities in the Hendon Central area between 2010 and 2013. However the facilities for pedestrians remain poor in general. Facilities for cyclists are somewhat better, with a number of signed routes and areas of both shared surface facilities and cycle lanes. However cycle parking facilities in the Brent Cross area remain limited and cyclists still face traversing a number of busy junctions and highways.

Fourteen key areas to link the proposed Brent Cross site were agreed, including Neasden, Dollis Hill, Colindale, Golders Green, Willesden Green, Childs Hill and Hendon Town Centres / London Underground Stations.

As part of the AWWCS, PERS (Pedestrian Environment Review System) and CERS (Cycling Environment Review System) audits were undertaken along 16 walking and 17 cycling routes from the red line boundary to key destinations in the vicinity. In addition at TfL's request, the walking and cycling links alongside the A406 and A41 were assessed. Similarly, existing links along the A5 were reviewed as part of the A5 Corridor Study.

The findings for all 33 audits of the routes alongside the A406, A41 and A5 corridors were documented in the BXC PERS and CERS Audit Findings Report in terms of the

key characteristics of their individual elements as well as their apparent quality and resultant red, amber or green (RAG) rating.

The vast majority of both pedestrian and cycle routes received amber ratings, although 11 of the 16 pedestrian routes had features that received green ratings. Often these were limited to one or two links, formal crossing points and public transport facilities.

Only a very small number of areas scored red during the assessment located in the near vicinity of the BXC redline boundary. These include the areas around Brent Cross tube and Cricklewood train stations and Brent Park Road, which connects to Layfield Road and is one of the links directly accessing the BXSC. Two of the crossing points at the A41 Watford Way/Vivian Avenue/Queen's Road junction adjacent to Hendon Central Station were also rated red.

Routes along the A406 and A41 corridors included a large number of subways, footbridges and segregated footways. Few crossings are present along the two routes as most pedestrian movements are physically separated from vehicle movements. The PERS scores for the pedestrian environment were relatively low, with no single feature considered to be of a good standard and therefore green rating on the RAG scale. On average pedestrian facilities were predominantly considered to be of an amber standard; several links, pedestrian crossings and bus stops were rated red and considered to be of a poor standard.

As part of the CERS audit the A406 corridor was not assessed due to its unsuitability for cyclists. However cycle routes which follow the corridor were assessed (i.e. Tilling Road and other road sections with signed cycle routes, footbridges and segregated footpaths). All features were rated amber and were therefore considered to have average quality cycle facilities.

In addition to the severance which currently exists in terms of the barriers to the movement of pedestrians and cyclists by road, rail and river infrastructure, there are other baseline issues which provide a basis for the changes proposed through the BXC development, including:

- Existing traffic volumes;
- Air quality, noise and micro-climate;
- Local flooding problems within the highway;
- Legibility and way finding;
- Anti-social behaviours; and
- Shopping centre infrastructure encircling the centre buildings.

Road Safety

The review of Personal Injury Accident data collated between 1st March 2008 to 31st December 2013 indicates a total of 653 accidents in the vicinity of the development area, of which 89.8% resulted in slight injury, 9.0% in serious injury and 1.2% (8) fatalities.

A total of 258 accidents occurred at major junctions including Staples Corner (60 incidents), Staples Corner / M1 / A406 / Tilling Road (71 incidents), the A41/A406 (48 incidents), the A41/A407 (39 incidents) and the A5/A407 (40 incidents). As Staples Corner will be redesigned and rebuilt, the accidents record at this junction will be monitored to determine whether the safety of this junction improves in the future.

A total of 395 accidents occurred along the A5 corridor (between Staples Corner and the A4077 – 167 incidents), A41 (between the A406 and A407 – 80 incidents), A406 (Between Staples Corner and A41 – 59 incidents), A407 (between A41 and A5 – 39 incidents) and Claremont Road (between A406 and A407 – 50 incidents). As the A5 experiences approximately double the number of accidents to other links within the study area, monitoring is proposed.

Public Transport: Bus

A range of bus services, operated on behalf of TfL, pass through or close to the BXC site. The area is well served by the bus route network, with frequent services to a variety of destinations in London. The majority of the bus services start from or pass through the bus station at BXSC. This bus station serves the shopping centre and also operates as a local bus hub. Other bus hubs in the vicinity are located at North Finchley and Golders Green.

Routes that originate and terminate at Brent Cross Bus Station are: 16, 102, 112, 142, 143, 182, 186, 189, 2190, 226, 245, 260, 266, 316, 326, 332, 460, C11

Routes that pass nearby Brent Cross Bus Station are: 32, 113, 316, 332

Routes that pass through Brent Cross Bus Station are: 232

Routes added since 2010 consent are: 324

Existing bus stops are located approximately every 350metres along each road in the area where bus routes are present. The bus station adjacent to BXSC has four stops for bus services and space for approximately 15 buses to stand away from the stops.

Bus lanes are provided at a number of locations in the vicinity of the BXC site. A significant proportion (approximately 30% by length) of the A5 Cricklewood Broadway and Edgware Road between the A406 and A407 are specified as bus lanes.

For monitoring purposes and to assess if modal targets and the associated modal shift to sustainable travel has been achieved, details of existing use (based on Bus Origin and Destination Surveys (BODS) data and key point data, collected by TfL) have been submitted.

Public Transport: Rail

The Midland Mainline railway corridor passes through the western edge of the BXC site. The BXC site is served at the southern end by the existing Cricklewood Railway Station. Hendon Station is approximately 3km to the north, outside of the BXC site boundary.

The 'Thameslink' services are currently operated by Govia Thameslink Railway (GTR). These rail services serve Cricklewood Station and Hendon Station and provide four stopping 4-car trains per hour on the slow lines in each direction for the majority of the weekday from Luton/St Albans to the north through Kings Cross/St Pancras and beyond to the south. During the peak periods up to 15 Thameslink trains per hour pass each way on the slow and fast lines.

Information on train loading and the throughput of passengers on the national rail network has been provided to form the baseline for the Monitoring Strategy.

Public Transport: London Underground

The Edgware branch of the Northern Line passes to the east of the BXC site and the Jubilee Line passes to the southwest through Willesden Green and West Hampstead. Brent Cross Underground Station is nearest the site and is located to the southeast of the A406/A41 junction. The station is isolated from the development area by the A41. The existing pedestrian links through the A406/A41 junction to BXSC and the A41 underpass to the eastern lands of BXC site are not user friendly. To the north, Hendon Central is approximately 750metres from BXSC. The route to BXSC from Hendon Central is subject to less severance and is considered to feel safer than the route from Brent Cross station.

Both Hendon Central and Golders Green stations are now step free where Brent Cross is not. Whilst at a greater distance from BXSC than Brent Cross, Hendon Central station has higher frequency bus connections running in the direction of BXSC.

Public Transport Accessibility Level (PTAL)

Public Transport Accessibility Levels are a detailed and accurate measure of the accessibility to the public transport network of a locational point, which takes into account the walk access time and the service availability. The method is a way of measuring the density of the public transport network at a particular point.

The London Plan states that PTALs are used by TfL to produce a consistent London wide public transport access mapping facility to help boroughs with locational planning and assessment of appropriate parking provision by measuring broad public transport accessibility levels. There is evidence that car use reduces as access to public transport (as measured by PTALs) increases. Given the need to avoid over-provision, car parking should reduce as public transport accessibility increases.

The proposed regeneration area baseline is rated as having a range of between poor (2) and excellent (6) PTAL bands.

The areas of highest existing accessibility are clustered around Brent Cross Bus Station, Brent Cross London Underground Station and Cricklewood Railway Station. Public transport users can arrive/depart by train at Cricklewood Station, by

underground at Brent Cross Underground Station, or by bus with one of London's largest bus stations at BXSC. The largest area containing high PTAL scores is just outside the regeneration area around the Golders Green LUL Station.

Areas of particularly poor PTAL scores south of the A406 include the residential area to the north of Clitterhouse Playing fields and the area to the east of the Hendon Rail Transfer Station. North of the A406 the residential areas to the northwest and northeast of the BXSC also have particularly low PTAL scores.

Coaches

Coach parking for one vehicle is currently provided at BXSC, for shopping trips to the centre by organised coach parties.

There are no express or limited-stop coaches which stop at Brent Cross Bus Station. However, there are a number of Greenline express coach services (as well as other services) that stop on the A406 North Circular Road, which provide 24 hour connections to a number of destinations including airports.

Taxis

There are currently two informal and one formal taxi pick-up/drop-off points in the vicinity of the Brent Cross Shopping Centre.

The formal Hackney Carriage rank is located immediately outside and along the southern boundary of the bus station, on Prince Charles Drive. It is a one-way system accessed and egressed via Prince Charles Drive eastbound carriageway. The rank can accommodate up to nine waiting taxis. The taxi rank is currently located in an area affording poor visibility and accessibility to passengers from BXSC.

The two informal taxi pick-up/drop-off locations tend to be used by minicabs and other private hire vehicle pre-booked taxi services. One is located at the John Lewis Customer Collection Point on the western perimeter of the Shopping Centre, within the car park outside the John Lewis store. The pick-up/drop-off area within the car park for John Lewis Customer Collection contains 14 bays allocated for waiting vehicles. However taxis in general park directly outside the entrance doors when picking up/setting down.

The second informal pick-up/drop-off area is situated at a service area, located in the north-east corner of the shopping centre site along an access road outside the Waitrose store entrance. It is accessible from a mini-roundabout at the north-eastern corner of the shopping centre's internal road network. The area reserved for drop-offs/pick-ups is not formally marked.

A survey of taxi facilities was undertaken on a weekday and Saturday in July 2013 in order to establish the level of existing usage of the taxi ranks by passengers and taxis.

APPENDIX 4

Pre-RMA Conditions

Condition 1.9 Construction Consolidation Centre Feasibility Study

This is the review of the Construction Consolidation Centre provision and / or alternative logistics facilities to provide a management method for enabling the safe and efficient flow of construction materials and equipment from suppliers to relevant development sites within the Northern Development and / or Southern Development.

The key objective of the study is to reduce the number of construction vehicles on the surrounding highway network of the development by identifying measures to provide a managed flow and distribution of construction vehicles onto the strategic highway network.

The review includes potential use of existing facilities and any new facilities which may be required to reduce the impact on the local infrastructure and the environment, through reduced vehicle movements during Phase 1, especially in the peak periods.

Eleven sites were reviewed as a potential Logistics Centre (a vehicle holding facility enabling the flow of vehicles to a site to be controlled.) The highest scoring sites and associated distances from the site were:

- London Gateway Services Plot (M1 Junction 3, southbound: 5 miles)
- Wembley Industrial Park (Great Central Way, Wembley: 4 miles)

The same eleven sites were also reviewed as a potential a warehousing / storage site. The highest scoring sites and associated distances from the site were:

- Wembley Industrial Park (Great Central Way, Wembley: 4 miles)
- Park Royal Industrial Estate (Abbey Road, Park Royal: 4.5miles)

Nine sites were assessed as facilities / locations to support Phase 1. The highest scoring sites and associated distances from the site were:

- London Concrete and Yeoman Asphalt Great Central Way, Wembley: 3.5 miles)
- Aggregate Industries (Horn Lane, Acton: 7 miles)

The key findings from the study are:

- A Construction Consolidation Centre is unlikely to be required and certainly not for Phase 1 as it is envisaged that very few of the vehicles attending site prior to the final stages of fit-out arrive less than 90% full.
- A potential rail based solution is problematic as there is minimal storage land adjacent to Hendon Rail Transfer Station, whilst the Euro Storage Site (The Goods Yard, Railway Sidings, 400 Edgware Road: 1.5 miles.) would require all vehicles to travel through the already congested Staples Corner and A5.
- A number of locations for a new logistics centre and off site supporting facilities (warehousing / storage / existing commodities and batching) have

been identified indicating a number of feasible options, which will be reviewed in more detail as the development progresses.

The full logistics plan for the construction of Phase 1 will form part of the Construction Transport Management Plan which will be produced by the developer and submitted for approval under Condition 12.1 prior to the commencement of works on site.

Condition 1.20 Area Wide Walking and Cycling Study (AWWCS) and Condition 2.8 Pedestrian and Cycle Strategy for Phase 1A (North)

The Area Wide Walking and Cycling Study reviews the pedestrian and cycle connections between the development site, key destinations and local residential areas. The study forms the basis of a delivery programme of potential schemes for improvements to pedestrian and cycle facilities adjacent to or beyond the site boundary, both to and from the Brent Cross site.

The objectives of the Area Wide Walking and Cycling Study are to:

- Define the study area in agreement with the London Borough of Barnet and Transport for London in relation to key destinations
- Examine specific pedestrian and cycle routes connecting the site with key destinations, transport hubs and local residential areas to identify shortfalls in provision and areas for improvement on these routes
- Identify a set of identified improvements which could be provided along the above
- Identify for delivery a programme of potential schemes, to be funded by the BXC development partners (BXCDP) for improvements to pedestrian and cycle facilities adjacent to or beyond the site boundary, providing improved access to and/ or from the BXC site. The potential schemes are to be 'fairly and reasonably related in scale and kind to the Development' and will 'ensure that the Development is fully accessible by walkers and cyclists and that it is fully integrated with the surrounding pedestrian and cycle network in the interests of encouraging sustainable non-car modes of transport.'

In addition, the study has provided a baseline framework for the local boroughs and Transport for London to advise on any future improvements outside of the scope of the BXC development. The measures that are to be funded by the Development will be discussed within the Transport Advisory Group (TAG) and recommendations made to the Transport Strategy Group (TSG) for expenditure of the Consolidated Transport Fund (CTF).

As part of the scoping process a total of 14 key origins/destinations were identified as directly relevant to the Brent Cross development location. These are:

- Cricklewood Town Centre and First Capital Connect station
- Brent Cross London Underground station
- Hendon Central London Underground station
- West Hendon/Hendon First Capital Connect station
- Willesden Green Town Centre and London Underground station

- Golders Green Town Centre and London Underground station
- Dollis Hill Town Centre and London Underground station
- Childs Hill Town Centre
- Hendon Town Centre
- Colindale Town Centre and London Underground station
- Kilburn Town Centre and Kilburn High Road London Overground station
- Neasden Town Centre and London Underground station
- Temple Fortune Town Centre
- West Hampstead

A total of 16 walking and 17 cycling routes were identified, plus links alongside the A41 and A406. The A5 corridor and associated pedestrian and cycle facilities were audited as part of the A5 Corridor Study.

Following agreement of the routes, each was assessed using the street audit Pedestrian Environment Review System (PERS) and Cycling Environment Review System (CERS) guidance and software.

The resulting information was then examined to identify improvements which could be put forward for each route. These included a range of infrastructure improvements as well as measures to address maintenance and enforcement issues. For the cycle routes, improvements were considered between the Brent Cross red line boundary and the destination of each route. As the walking routes become very dispersed at distances greater than about 1km away from the site, improvement measures were only considered within 1km of the site boundary.

The development of the Area Wide Walking and Cycling Study has been conducted in close liaison with the London Borough of Barnet and Transport for London, as advised in Schedule 17. The proposed AWWCS improvement measures were presented and discussed at a consultation workshop held on Thursday 6th November 2014 with representatives of the London Borough of Brent, the Barnet Cycle Group and the Brent Cycle Group. Representatives from the London Borough of Camden; the London Cyclists Campaign and Touring Club; the Mayor's Cycling Commissioner; Living Streets and the Ramblers Association were invited to the workshop but did not attend.

The Area Wide Walking and Cycling Study has been reviewed by the London Borough of Barnet, Transport for London and the London Borough of Brent. Each local authority responded with suggested comments, amendments and additions. As well as improvements, minor route additions and diversions suggested by the local authorities were also included where considered relevant.

The final route improvements plan differentiates between 'proposed improvements to integrate the development into the existing networks' and 'potential improvements for consideration by local authorities'. The proposed 'potential improvements for consideration by local authorities' have then been graded in order to identify the forecast need for the implementation of the improvement measures in order to enable prioritisation in the future where necessary.

The indicative series of proposed improvements to integrate the development into the existing network have been secured via a contribution of £1,250,000. As the detail design progresses, the exact nature of the pedestrian and cycle improvements will evolve.

The Phase 1A North Pedestrian and Cycle strategy sets out the quantum and details of pedestrian and cycle links and facilities to be provided within this phase of the development.

The proposals have been based on an iterative design review process, based on consultation with Transport for London and officers of the London Borough of Barnet.

The strategy has been based on the version of the London Cycling Design Standards that were current at the time of the study. Segregated off-road pedestrian/cycle facilities have been proposed where feasible (where space and land allows). Shared off road facilities have been provided where width constraints do not allow for segregated off road facilities or where the quality of the environment consideration prevails, such as long the river route corridor.

At the Staples Corner/M1/A406 Junction, a 4m wide shared pedestrian/cycle bridge will be provided to form a key north/south link across the A406, to the east of the railway line. To the west of the railway line, 1.83m wide pedestrian bridges on both sides of the A5 will be provided.

The A406 will also be crossed via:

- The Living Bridge: A 15m wide bridge for shared use by pedestrians and cyclists. The minimum effective width (useable space for pedestrian/cycle movement) will be 7m.
- A406 Tempelhof Bridge: A segregated footway/cycleway will be provided alongside the northbound carriageway. This will consist of a 2.5m – 3m wide cycle lane and a 2-2.5m wide pedestrian element. Adjacent to the southbound carriageway, a 2.5-4m wide pedestrian only footway will be provided. A cycle ramp will provide access from New Tempelhof Avenue to the River Brent Corridor.
- A41/A406 Junction: The existing pedestrian bridge across the A406 at this location will remain and cyclists will have to dismount. Improved links to Brent Cross Underground Station from Brent Cross Shopping Centre will be provided for pedestrians and cyclists.

A 5m wide shared footway/cycleway facility with step free access will be provided on the River Brent Corridor providing a traffic free east-west connection.

A segregated footway/cycleway will be provided linking the proposed Cycle Superhighway 11 to Brent Cross Shopping Centre through Clitterhouse Playing Fields.

Proposed improvements to underpasses on the A41 and the M1/MML underpass will be undertaken, including general maintenance.

The proposed pedestrian and cycle links have been appraised using principles from the Pedestrian Environment Review System and the Cycling Environment Review System.

Although forecasts of pedestrian and cycle movements have not been calculated due to the lack of appropriate modelling tools, comfortable pedestrian flows for each footway width have been determined. These flows indicate that demand can be accommodated.

The London Borough of Brent objected to the AWWCS, key concerns being that the measures proposed were too limited to provide a sufficient incentive for people to walk or cycle and that the measures did not mesh with LB Brent's own cycle route plans and proposals. However, officers consider that the agreed measures arising out of the AWWCS does provide sufficient proposals to comprehensively improve key local walk and cycle routes in the vicinity of the BXC development opportunity. The £1,250,000 mentioned above includes a £300,000 contribution towards a separate cycle route requested by LB Brent between BXC and Wembley.

Condition 1.21 Framework Servicing and Delivery Strategy and Condition 1.22 Phase 1A North Servicing and Delivery Strategy

The Framework Servicing and Delivery Strategy provides a structure for the consideration of servicing and delivery issues at subsequent stages in the planning consents process and during construction and operation of the development.

Each phase or sub-phase of development requires the preparation of a Servicing and Delivery Strategy and will be in operation from first occupation until all elements of the phase or sub-phase are fully developed.

The implementation and overall success of the Servicing and Delivery Strategy for each phase or sub phase will be dependent on the end users of the development. Each end user will be required to prepare a Delivery Service Plan for their development as part of the reserved matters planning application. This will ensure individual plots work effectively under the Framework Servicing and Delivery Strategy and Servicing and Delivery Strategy for that phase.

The uses which have potential to generate a large number of servicing and delivery movements is split between:

- The Brent Cross Shopping Centre;
- The Rail Freight Facility;
- The Waste Handling Facility; and
- Other retail, notably High Street South, office, hotel, community, leisure, residential and industrial uses.

The overarching aim of the Framework Servicing and Delivery Strategy (and the Servicing and Delivery Strategies prepared for each phase or sub phase of development), is to reduce the impact of delivery and servicing activity generated by the development during the operation of the site on the human and physical

environment. This will be pursued through seeking to influence delivery and servicing activities to achieve the following objectives and targets:

- Objective 1: A reduction in the volume of delivery and servicing trips
 - Target - Achieve a 15% reduction in baseline site-wide delivery and servicing trips by year 5.
- Objective 2: A change in the pattern of delivery and servicing trips to avoid peak hours
 - Target - Achieve a 30% reduction in baseline peak hour delivery and servicing trips by year 5.
- Objective 3: A shift of mode from road to rail and other more sustainable modes in so far as is reasonably practicable
 - Target - Identify and implement mode shift from road to rail and other more sustainable modes through monitoring of mode split for delivery of goods.
- Objective 4: An improvement to delivery and servicing vehicles, equipment and technology.
 - Target - Promote improvements to vehicles, equipment and technology through use of registered logistics companies, benchmarking on fuel use, benchmarking on accidents, driver training schemes and emissions monitoring.

Proposed servicing and delivery management measures at Brent Cross Shopping Centre include a Vehicle Booking and Management System. This system will help manage deliveries away from peak hours, minimising congestion on site and ensuring deliveries can be managed according to the capacity of the loading facilities available. Working with tenants suppliers the aim is also to reduce the level of deliveries to the site by consolidating deliveries further up the supply chain.

Measures at the Rail Freight and Waste Handling Facilities are expected to include:

- Joining a Freight Quality Partnership;
- Moving deliveries outside of road network peak hours; and
- Minimising night time noise through the development of a noise abatement strategy.

As regards the Phase 1A (North) servicing and delivery strategy there are only very limited servicing and delivery issues. In the residential proposals on Plots 53 and 54 there are 48 homes. Within the open space proposals a small kiosk style café and the park maintenance depot will require some deliveries / servicing.

Condition 7.1 Estate Management Framework

The Estate Management Framework submitted in respect of Phase 1A (North) anticipates a structure for Estate Management Companies to be established for each phase of this development and sets out some general principles.

The longer term ownership of the northern element of the project will remain with the Brent Cross Partners whilst the southern part of the development will be largely

owned and managed by the LB Barnet and another development partner – likely to be Argent Related.

Adopted roads and structures (such as bridges) will remain the responsibility of the relevant highway authority. The report includes a plan which illustrates ownership assumptions at the time of preparing the report. It is anticipated that the final agreement and apportionment of responsibilities will form part of the detail S38 and S278 highways agreements and any such agreement is likely to include a commuted sum for future maintenance purposes.

An exception to this will be the management and maintenance of the surface of the Living Bridge which will be maintained by the Brent Cross Development Partners as it forms one of the principal entrances to the expanded shopping centre.

Different categories of open space are likely to have different management responsibilities although open spaces currently in the LB Barnet's ownership are likely to remain in council ownership such as the improved Clitterhouse and Claremont Open Spaces where the BX Development Partners will remain responsible for the costs of management and maintenance of the hard and soft landscape (including park furniture) for a period of two years with a longer period of 5 years agreed for maintenance of trees within the parks.

The Brent Riverside Park – the central section of which will be established in the RMA application for Phase 1A (North) - will be managed and maintained by the Brent Cross Development Partners in perpetuity whilst the river channel itself will remain the responsibility of the Environment Agency.

An acoustic barrier is required to ensure that the Brent Riverside Park is protected from the noise of the adjacent A406 (north circular) and can meet suitable standards expected for a publically accessible open space. It is anticipated that this noise barrier may be within the public highway and it is expected that the Highways Authority will either give a license to the Brent Cross Cricklewood development partners to allow them to maintain the structure or else will require a commuted sum to cover any maintenance costs should the structure be adopted as part of the public highway by the relevant highway authority.

In addition to the acoustic barrier, the proposed scheme includes for some areas of grass, landscape planting, trees and possibly other private apparatus within the public highway.

These will be either installed and maintained by the Developer under license granted by the Highways Authority or if appropriate, adopted by the relevant Highway Authority on provision of relevant commuted sum to cover future maintenance.

Condition 11.1 Car Parking Management Strategy and Condition 11.2 Phase 1 Parking Standards and Strategy

The Car Parking Management Strategy provides an overarching framework to ensure the effective management of car parking across the scheme so as to assist in

encouraging modal shift away from private transport to minimise environmental impacts.

Parking Standards approved through the Section 73 application

Parking standards set out in the S73 Permission may lead to 6,330 car parking spaces for residential use with an additional 11,565 spaces intended for non-residential uses.

7,600 spaces are allocated for retail and related uses with the BX East zone. The majority of residential spaces and spaces for office, industrial and community use are proposed south of the A406. The vast majority of parking proposed is on plot with only a small proportion located on street

Proposed car parking management strategy

A fundamental principle of the strategy for the control of travel to and from the area is the discouragement of those who have a reasonable alternative to travel by car. It is recognised that the availability of parking spaces both at the beginning and end of a journey has a considerable influence on mode choice. The car parking management strategy seeks to manage parking at a level where reasonable car use is accepted but unnecessary car travel becomes unattractive.

The key principles of car parking management within the strategy are:

- The introduction of car parking charges across the whole development including Brent Cross Shopping Centre;
- Shared use of parking provision;
- The non-allocation of parking spaces so that residents and businesses can “lease” parking to suit their needs;
- Parking provision, management and charges used to manage demand going forward;
- Potential for an evidence based, progressive reduction in residential parking standards from the London Borough of Barnet policy standards to those of the London Plan as the level of public transport accessibility increases;
- Use of travel plans/car clubs/cycle club to ensure the availability of alternatives to private car use and the need for parking provision;
- Protection of nearby residential areas from the impacts of parking displacement; and
- Restraint at Brent Cross Shopping Centre, with no additional retail or leisure spaces being provided through the proposals.

The following initial parking charges are proposed within the shopping centre, as set out in paragraph 2.1 of Schedule 16 of the S106 Agreement:

Less than 1 hour: £1
1-2 hours: £1
2-3 hours: £2
3-4 hours: £2.50
4-5 hours: £3.50

5-6 hours: £6.00
6-7 hours: £12.00
7-8 hours: £14.00
8 hours plus: £30.00

The expectation is that blue badge holders will be able to park for an unlimited time and free of charge. There is a requirement to review these parking charges with the local planning authority every three years.

The public off-street parking areas serving the town, neighbourhood and community centres will be managed by the use of parking charges in such a way as to dissuade all day commuter parking.

There will be no increase in the number of parking spaces serving the Brent Cross Shopping Centre; therefore there will effectively be a reduction in the ratio of floor area to parking spaces. In total there will be:

- 11,565 spaces intended for the no residential uses (including operational spaces) of which 7,600 are for retail (as per existing). The retail spaces will be with leisure uses as in general the peak demand for these land uses will occur at different times of the day.
- 6,330 residential car parking spaces

For the residential areas, 1 space per residential unit for Phase 1 of the development is proposed, with an aspiration for a progressive reduction in provision towards 0.7 spaces per residential unit via review and monitoring.

At present the number of disabled spaces in the shopping centre car parks is approximately 3% of the total. A survey of existing usage indicates that the current provision is more than adequate to accommodate demand. An increase to 5% is proposed (similar to Westfield) with an additional 5% of spaces (380 bays) provided as enlarged standard bays that could be converted at a later date. As the London Plan requires six per cent of the spaces to be designated blue badge parking bays, there is a commitment to monitor and review the number of disabled parking spaces. This will help to ensure that the correct number of spaces is being provided.

Staff parking will be managed by the Individual Travel Plans that will be prepared as each plot comes forward. The expectation is that staff will pay for parking.

Parent and Child Parking will be developed through dialogue with the major retailers. These demarcated spaces will be evenly distributed across all car parks and their usage will be monitored to ensure that the appropriate number of spaces has been provided.

The number of parking spaces with electric vehicle charging points will be provided in accordance with the discharge of Condition 39.7. For retail use, the London Plan identifies a need for 10% active provision and 10% passive provision.

Within each car park, there will be dedicated motorcycle parking areas. Parking within these areas will initially be free of charge. The expectation is that the

proportion of motorcycle parking spaces will at least match the current level of provision.

Computer controlled variable message signs, linked to the parking management system, will direct drivers to car parks where spaces are still available. Signs will be provided on the main approach routes to the Shopping Centre and on the internal circulation road within the Shopping Centre. Where a car park is full, this will help to discourage queues extending onto the internal access road and blocking the access to other car parks that have capacity. These signs will also be utilised to manage the exit arrangements from the car parks.

All the car parks at Brent Cross will meet the criteria to achieve The Safer Parking Scheme “Park Mark” award.

There are 80 car parking spaces proposed at the new Cricklewood Railway Station of which 42 will be “kiss and ride” with a maximum waiting time of 20 minutes. There will be provision made for approximately 40 taxis at the new station.

The developers will be required to ensure the early establishment of a Car Club. The site-wide Car Club will be established prior to occupation of 200 dwellings. The Travel Plan process will monitor the use of the car club and the level of car ownership to ensure that appropriate provisions are in place to accommodate the growing car club demand.

It is recognised that the stringent off-street parking controls within the area could increase parking in the surrounding residential and commercial areas. It is therefore proposed that the BXC site is included as a designated Controlled Parking Zone and that all residents and visitors within the area be given the opportunity to be issued with resident/visitor parking permits. Where new residents are provided with the opportunity for dedicated off-road parking, they will normally be excluded from applying for on-street residents parking permits.

On street parking surveys have been undertaken to identify areas that may be vulnerable to additional parking pressures as the redevelopment of the Regeneration Area progresses and parking charges at Brent Cross Shopping are introduced. Further parking beat surveys will be undertaken prior to the start of construction and then at intervals of 12 months to collect occupancy and length of stay in areas vulnerable to overspill parking. These surveys would continue for a period of three years after completion of the development to establish the final parking demand.

The decision to introduce new Controlled Parking Zones or extensions to existing ones will need careful consideration and will be subject to comprehensive monitoring. Under schedule 3 of the Section 106 (paragraph 11.3) the Developers are obliged to fund the reasonable costs of the council in assessing the need for new or modified CPZs in the vicinity of the site in Barnet in order to provide parking controls for the construction and / or operation of each phase or sub-phase.

In addition to the measures set out in the Car Parking Management Strategy submitted to discharge this condition, there are additional planning conditions to deal

with construction worker parking through the phase specific Construction Worker Travel Plans required under Condition 12.2.

The parking demand associated with sub-phase 1A North relates to the occupancy of Plots 53/54, the open space area at Clitterhouse Playing Fields and the temporary demand generated by construction workers.

For the residential areas, 1 space per residential unit for Phase 1 of the development is proposed, with an aspiration for a progressive reduction in provision towards 0.7 spaces per residential unit via review and monitoring.

For the 48 dwellings in Plots 53/54, a total of 48 parking spaces will be provided of which 5 will be for blue badge holders.

At Clitterhouse Playing Fields, 22 parking spaces and four for blue badge holders are proposed within a car park accessed via Claremont Road.

Following clarification, the location and size of the car park for construction workers has yet to be identified and will be confirmed within the Construction Workers Travel Plan (Condition 12.2) that will be submitted before work on the phase commences. The details of the location of any construction worker car parking and the detail of other measures to minimise construction worker parking are not known at this stage of the development process. As required by Condition 12.2 they must be confirmed before any work on Phase 1A North commences.

Initial indications are that at its peak the car park will provide approximately 500 spaces for construction workers during the development of Phase 1AN but this will be subject to consideration under the Condition.

It is anticipated that any construction worker car park will be served by a shuttle bus and the amount of available parking will be reviewed annually. Parking demand for construction workers will be managed via the Construction Workers Travel Plan, within which sustainable transport options will be promoted.

An initial outline of the Construction Workers Travel Plan Framework is contained in Paragraph 20 of Schedule 3 to the S106 agreement, with an example of best practice, being the Olympics Construction Workers Travel Plan Framework, being provided.

Schedule 12 of the s106 requires the Development Partners to consider, in liaison with the adjoining boroughs, whether Controlled Parking Zones should be extended around the site. It is proposed that the Controlled Parking Zone review will commence in early 2016, in order that any measures that are agreed are implemented pre-commencement.

In terms of cycle parking, each dwelling in Plots 53/54 will have access to covered and secure cycle storage facilities whilst within Clitterhouse Playing Fields a minimum of 30 cycle stands will be provided.

Condition 39 (2) confirms that the establishment of the site wide car club is not required as part of Sub-phase 1AN as this is triggered by occupation of the 201st residential unit.

Condition 39(7) of the 2014 permission confirms that electrical charging points are not required in sub-phase 1AN except within critical infrastructure.

Condition 2.7 A5 Corridor Study

The objectives of the A5 Corridor Study are to:

1. Ensure any local traffic impacts are identified in the adjacent boroughs of Brent and Camden, as well as any further impacts in the London Borough of Barnet by ensuring the traffic modelling for the design stage is sufficiently detailed in areas of interest.
2. Assess any identified impacts using appropriate junction modelling tools and produce outline designs of any mitigation measures where appropriate.
3. Develop an A5 VISSIM design model, in conjunction with supporting local area models using complementary modelling packages TRANSYT and LinSig to assist with the following:
4. The development of detailed designs for the new and improved junctions.
5. The testing of any temporary traffic management measures during the construction period on highway operations.
6. Define any new or improved facilities required in regard to multi-modal user requirements, e.g. walking and cycling and bus priority and setting out the identified interventions in line with the indicative phasing.

The microsimulation VISSIM traffic model (explained in section 5.4) has been extended from the A5 corridor to include the A406 corridor between Staples Corner and the A406/A41 junction. The model enables the interaction of differing junctions to be assessed and journey times for different modes, including buses, to be reviewed.

Where required, proposed improvements at junctions have been suggested within the study area. Analysis has been undertaken to identify the capacity, with development, at the following key junctions:

1. Staples Corner (Phase 1 and end state): A gateway junction with a previously approved junction design.
2. A5 / Humber Road / Geron Way (Phase 1 and end state): Creation of a four stage set of traffic signals with advanced cycle stop lines and a pedestrian controlled crossing on the southern arm of the A5.
3. A5 / Oxgate Gardens / A5 Link Road (end state): Creation of a 4-arm traffic signal controlled junction with Oxgate Gardens being one-way westbound.

4. A5 / Dollis Hill Lane / Residential Development (Phase 1 and end state): Conversion of this 3-arm traffic signal controlled junction to a 4-arm traffic signal controlled junction.
5. A5 /Ashford Road / Depot Approach (Phase 1 and end state): No changes proposed.
6. A407 Chichele Road / A5 Cricklewood Broadway (Phase 1 and end state): A gateway junction with a previously approved junction design. Compulsory purchase of the plot of land on the south east corner of the junction enables the arms of the A407 to be aligned and to operate together to improve capacity.
7. A407 Chichele Road / Anson Road (Phase 1 and end state): Linkage to the adjacent A5 Urban Traffic Control group is proposed, enabling improved co-ordination.
8. A407 Cricklewood Lane / Claremont Road / Litchfield Road (Phase 1 and end state): A gateway junction with a previously approved junction design. A flared approach on the A407 western arm and an additional southbound lane on Claremont Road increase capacity.

The results indicate at Staples Corner in 2021 the AM peak is within capacity but that three movements in the PM and Saturday peaks are overcapacity. In 2031 overcapacity is forecast for four movements. To reduce these queues, there is potential to adjust the signal times at the junction.

The A407 Cricklewood Lane / Claremont Road / Lichfield Road junction is forecast to be overcapacity in 2021 and 2031. However, deployment of a bespoke traffic signal control system at the junction will assist in mitigating the impact of additional traffic. Based on data collected by TfL, the deployment of such a system provides an average 12% reduction in delay.

The A407 Chichele Road / A5 Cricklewood Broadway is reaching capacity in 2031, with the A5 northbound movement predicted to be close to saturation in the PM peak period.

With regards to local roads, use of the BXCDDM has enabled changes in traffic flow to be assessed. The most significant increases in flow are predicted to be on the roads in Barnet, Brent and Camden tabulated below:

Link	Section	Direction	AM Peak	PM Peak	Sat Peak
A5	Layfield Road-Station Road	Northbound	*	*	
Highfield Avenue	A41-The Drive	Eastbound	*		*
Humber	A5-Coles Green Road	Westbound	*	*	*

Road					
Parsifal Road	A41–Fortune Green Road	N/Eastbound	*		
Chichele Road	Anson Road-Walm Lane	S/westbound	*	*	
A41	A406 NCR - A598 Finchley Road	Northbound		*	
Claremont Road	Somerton Road-Pennine Drive	Southbound		*	*
Cricklewood Lane	Farm Avenue – A41	Eastbound			*
Fordwych Road	Maygrove Road-Mill Lane	Northbound			*
Walm Lane	Lydford Road-A5	Eastbound			*

Analysis of the traffic on these local roads indicates the majority of the increases are not BXC development related trips, with changes in the performance of links and junctions leading to re-routing of existing trips which in turn is the main contributor to increased flows on these roads.

There are some links with increased traffic flow which is related to the BXC development, such as Claremont Road, where changes to the layout and the strategic junctions mean these routes, being in such close proximity to the development, inevitably attract development related trips. However, the infrastructure improvements to these routes are designed to account for this increased demand.

Overall, total flows within the BXCDDM model of the areas that fall within Camden and Brent increase with the approved BXC development. The total increase (based on passenger car units where a cyclist = 0.5, a car = 1 and a HGV/Bus = 3) in each peak period is summarised in the following table:

Time Period	Camden Total Link Differences	Brent Total Link Differences
	Peak hour PCUs	Peak hour PCUs
AM 2021	2,946	4,724
AM 2031	8,282	7,072
PM 2021	5,173	4,390
PM 2031	7,331	8,461
Sat 2021	7,601	6,347
Sat 2031	10,046	12,347

The results indicate that with the development, there is an increase in traffic in all peak periods across the highway network in the neighbouring boroughs. However, it is generally predicted that the increases on the non-strategic local roads is due to rerouting of non-development related traffic, rather than due directly to development related traffic. Despite this if monitoring of traffic flows shows noticeable increases in traffic flows on local roads in Brent or Camden due to the development, an additional

capped contribution of £300,000 towards future Supplementary Transport Measures has been secured.

It should be noted that the LB Brent and LB Camden both objected to the A5 Corridor Study (although this was prior to the funding of the study proposals being agreed with the Developers). LB Brent expressed concerns about a lack of mitigation measures along the A5 and in Brent itself, and parking pressures in the Dollis Hill area. LB Camden are also concerned about the identification of traffic and parking impacts in Camden. The full objections and detailed responses are contained in the separate report before Members, which explains how concerns about traffic and parking impacts, both predicted and potential, are proposed to be dealt with.

Officers are satisfied that the study has been carried out in accordance with the agreed and approved scope, and that the key impacts have been correctly identified and assessed. A package of supplementary mitigation measures and additional contributions has been agreed which is considered to adequately address expected impacts, as well as providing additional safeguards should additional supplementary mitigation be needed in future phases.

Condition 37.3 Phase 1 Transport Report

A Phase Transport Report has to be provided for each phase of the development and this one examines the end state of Phase 1 (2021) based on the preliminary design of the approved transport works for the phase. Further detail is to be provided within the reserved matters transport reports which will subsequently be produced for each sub-phase (1A North, 1A South, 1B North, 1B South and 1C).

The scope of this phase report has been discussed and agreed with the Highways England, the London Borough of Barnet and Transport for London. The numerous studies that form the basis of the Phase Transport Report have each been reviewed by officers of the London Borough of Barnet and Transport for London.

The specific objectives set within the Phase Transport Report for Phase 1 are:

- Safe operation of the local and strategic road network
- Safe operation of the bus network that serves the site
- Improved reliability of the buses that serve the site
- Improved pedestrian and cycle links to the site
- Mode shift from car use to public transport use, walking and cycling
- Improved accessibility of the Phase 1 development for public transport users
- Transport measures approved at Phase 1 to enable subsequent phases of development to come forward

The Phase Transport Report for Phase 1 details:

- Existing site information in relation to public transport, pedestrians and cyclists.
- Baseline transport data for buses, coaches, rail and London Underground, as well as a review of accident statistics.

A tabulated modal split breakdown of accidents within the study area has been provided for the A41, A406, A407 link and Claremont Road. Numbers and associated causes of accidents are reviewed across the study area, with on-going monitoring of accidents recommended on the A5, Staples Corner junction and the A41/A406 junction.

- A review of relevant national, regional and local policies.
- The Phase 1 proposals, which are split over five sub-phases and consist of:
 - Retail uses: 100,851 sqm
 - Office: 10,970 sqm
 - Hotel and Conference: 31,722 sqm
 - Community Uses: 9,338 sqm
 - Leisure Uses: 20,411 sqm
 - Residential: 171,150 sqm
 - Waste Handling: 24,700 sqm
 - Rail and Bus Station: 117 sqm**Total: 369,259 sqm**

The majority of the infrastructure improvements associated with the scheme will be delivered in Phase 1 so that they provide additional capacity and new routes early on. Junction improvements are proposed at:

- M1/A406 and A5/A406 Staples Corner
- A41/A406 junction
- A5/Diverted Geron Way
- A5/A407 Cricklewood Lane
- A407 Cricklewood Lane / Claremont Road
- A406 Brent Cross Access / Egress junction

Other infrastructure improvements include:

- Amended A406 North Circular Road Coach Stops
 - A41 Southbound On-Slip
 - Living Bridge
 - Realignment of the River Brent
 - Brent Cross Bus Station relocation
- Proposals in Phase 1 for improvements for cyclists, pedestrians and users of public transport.

This includes the provision of two new pedestrian bridges across the A406 and a segregated cycle route from Cycle Superhighway 11, through Clitterhouse Playing Fields to Brent Cross Shopping Centre. A pedestrian and cycle route network will be established both within the development red line area and outside, providing links into the London Boroughs of Brent and Camden as well as destinations such as Hendon, Colindale and Golders Green. A cycle hub will also be provided to make cycling a more attractive mode of transport for the area.

Improved pedestrian and cycle routes outside the red line boundary have been developed via the Area Wide Walking and Cycling Strategy, which has

been developed in consultation with Transport for London and the London Boroughs of Barnet and Brent. The strategy was based on the Cycle Environment Review System and Pedestrian Environment Review System analysis, carried out by trained members of staff.

The Area Wide Walking and Cycling Strategy has been submitted as a separate report and has been reviewed and revised based on comment from both Transport for London and the London Borough of Barnet. Specific proposals for cycle links within the London Borough of Brent have been submitted by the authority and included within the strategy at their request. The AWWCS proposals provide comprehensive walk and cycle connections to the areas surrounding the BXC site.

Based on a review of the existing site accessibility, the Phase 1 proposals are shown to provide improved pedestrian and cycle connectivity in the area.

The new bus station located north of Prince Charles Drive will provide 17 bus stands with 12 bus stops, the parameters and principles of which have been agreed with Transport for London. New bus stops and bus lanes are proposed across the network. Bus journey times will be monitored via TfL (iBus) data. The new stops will be utilised by existing services and will enable additional and more frequent services to be delivered during future phases. The provision of bus priority, such as on Tempelhof Bridge, and the delivery of new infrastructure, including a new junction on the A41 and improvements to the underpass under Tilling Road, will also benefit the existing bus services operating within the vicinity of the development during Phase 1.

- The Phase 1 proposals are designed to encourage visitors to travel to and from the shopping centre on foot, by bicycle or by public transport, with no increase in associated car parking. There is provision for motorcyclists, mobility impaired users and residents as well as the provision of Electric Vehicle Charging Points, in accordance with the relevant policies.

Car parking charging at Brent Cross Shopping Centre will be introduced at the following minimum gate rate tariff:

Time	Tariff	Time	Tariff
0-2 hours	£1.00	5-6 hours	£6.00
2-3 hours	£2.00	6-7 hours	£12.00
3-4 hours	£2.50	7-8 hours	£14.00
4-5 hours	£3.50	8 hours +	£30.00

The Phase 1 Transport Report identifies the requirement to protect nearby residential areas from the impacts of parking displacement. Further review is provided within the Car Parking Management Strategy and the Car Parking Standards and Strategy: Sub-Phase 1A North. A Construction Travel Management Plan is also required to be approved prior to commencement of construction. Controlled Parking Zones are proposed, with on-going

monitoring and associated reporting mechanisms in place to ensure further provision, should it be required, is provided.

- Servicing and delivery, with reference to the Framework Servicing and Delivery Strategy for the development and the Servicing and Delivery Strategy for each sub-phase.
- The Phase 1 transport assessment work included checks that show that the overall development quanta and resulting trips do not exceed the boundaries presented in the 2014 S106 Agreement and that the transportation impact and mitigation proposed is as forecast in the S73 Consent. This was undertaken through a review of the controlling Transport assessment Matrix, which is the agreed process to assess whether each phase falls within its predicted impact and that the overall impacts are within that forecast in the overall scheme Transport Assessment (BXC05).

As Phase 1 is the first phase, it represents the starting point for the whole development and therefore no cumulative impacts have been assessed. As no monitoring has been undertaken, just the predicted impact of Phase 1 is tabulated. Demand management measures and triggers are also identified.

- Additional highway modelling has been undertaken utilising the Brent Cross Cricklewood Transport Model to inform the Phase Transport Report for Phase 1 and to robustly identify the impacts of the Phase 1 proposals, the proposed benefits of the mitigation proposed for Phase 1 and to compare journey times and delays for the combined traffic impacts of phases 1 to 4. A number of tests have been undertaken as set out below to understand how Phase 1 fits in relation to the wider scheme:
 1. Sub-phases 1AN, 1BN and 2N highway network, with background traffic plus growth to 2021, and traffic flows generated by the development in sub-phases 1AN, 1BN and 2N.
 2. Phase 1 highway network with background traffic plus growth to 2021 and traffic flows generated by the development in Phase 1.
 3. Phase 1-4 highway network with background traffic plus growth to 2021 and traffic flows generated by the development in Phase 1-4.
 4. Phase 1-5 highway network with background traffic plus growth to 2021 and traffic flows generated by the development in Phase 1-5

The results show that the Phase 1 highway network will perform as well as or better than the existing network in the majority of cases as a result of the new infrastructure which will be delivered. The results also identify that the combined highway and junction improvements associated with each phase, or combination of phases, will mitigate the impact of the development.

- The integrated transport strategy for the development with regards to highways, public transport, walking and cycling with associated management measures.

APPENDIX 5

Analysis of Scheme Compliance with Regional and Local Planning Policy

Table 2.1: Analysis of the proposals compliance with London Plan (March 2015) Policies

Policy	Content Summary	Extent of compliance and comment
Policy 1.1 (Delivering the strategic vision and objectives for London)	Strategic vision and objectives for London including managing growth and change in order to realise sustainable development and ensuring all Londoners are able to enjoy a good and improving quality of life. Improving environments which are easy, safe and convenient for everyone to access.	Compliant: As a London Plan Opportunity Area, the approved BXC scheme seeks to make the most of brownfield land to meet wider growth requirements in terms of housing, retail and commercial activities in a location accessible by a range of transport modes. The provision of the proposed infrastructure will assist in achieving the wider sustainable aims of the London Plan and provide easy, safe and convenient access for all.
Policy 2.6 (Outer London: vision and strategy)	Work to realise the full potential of outer London and enhance the quality of life for present and future residents. Understand the significant difference in the nature and quality of neighbourhoods; improvement initiatives should address these sensitively and draw upon strategic support where necessary.	Compliant: The proposed development represents one of the most important opportunity areas in outer London. The BXC Opportunity Area represents a significantly underutilised area of accessible brownfield land in need of regeneration. More specifically, in relation to the current application, the infrastructure will be enhanced for visitors to the Brent Cross Shopping Centre and the Southern development. It will be an asset for the public and existing neighbourhoods.
Policy 2.8 (Outer London: Transport)	Enhance accessibility by improving links to and between town centres and other key locations by different modes and promoting and realising the key improvements. Work to improve public transport access, provide improved traffic management, road improvements and address and manage local congestion	Compliant: The majority of the infrastructure improvements associated with the scheme will be delivered during Phase 1 in preparation for the proposed development due to come forwards during the subsequent phases. The network created in Phase 1A (North) provides all necessary connections to facilitate the rest of the northern development and in addition, some of the key roads south of the A406 to facilitate the first plot of the Southern Development. Key elements of the road network for the whole development are therefore provided.
Policy 2.13 (Opportunity Areas and Intensification Areas)	Support the strategic policy directions for the opportunity areas, and where relevant, in adopted opportunity area planning frameworks Support wider regeneration, including in particular improvements to environmental quality, and integrate development proposals to the surrounding areas especially for regeneration.	Compliant: With regards to this current Reserved Matters Application, nearly all the new phase 1A North roads and junctions are being designed with enough capacity to accommodate the predicted traffic levels for the whole development at end-state. The new infrastructure is designed to support the wider Brent Cross regeneration. The A5 Corridor Study; Condition 2.7 is a pre-reserved matters condition of the s73 permission and the study identified that there would be additional impacts on local roads in Brent and Camden; to improve these conditions a capped contribution has been secured towards future Supplementary Transport Measures if monitoring of traffic flows indicates increases due to the development.

Policy	Content Summary	Extent of compliance and comment
Policy 2.14 (Areas for regeneration)	Boroughs should identify spatial areas for regeneration and spatial policies to bring together regeneration.	<p>Compliant: The BXC site is identified as a London Plan Opportunity Area (See Policy 1.1 above) in need of comprehensive regeneration and capable of accommodating significant housing, jobs and community infrastructure.</p> <p>This particular RMA provides the detailed layout and network for vehicles, cycles and pedestrians that provide connections to the Northern development and provides sufficient connections across and to the south of the A406 to facilitate the start of the Southern development. The Infrastructure RMA includes major highway works, including the M1/A406/A5, Brent Cross A406 Ingress/Egress, A406/A41, A5/A407 and A407/Claremont Road junction improvement schemes.</p>
Policy 2.15 (Town Centres)	Development proposal should promote access by public transport walking and cycling. Promote safety and security and contribute towards an enhanced public realm and links to green infrastructure.	Compliant: The development improves public transport by providing replacement of the existing bus station with a fully integrated new high quality facility within the extended Brent Cross Shopping Centre, improvements to pedestrian and cycle facilities; provides a new pedestrian and cycle bridge, known as the Living Bridge which will provide direct access from the southern development to the shopping centre situated to the north. Additionally, provide key connected pedestrian and cycle routes via Sturgess Park, Claremont Park and Clitterhouse Playing Fields.
Policy 3.1 (Ensuring equal life chances for all)	Development proposals should protect and enhance facilities and services that meet the needs of particular groups and communities. Proposals involving loss of these facilities without adequate justification or provision for replacement should be resisted.	Compliant: It is considered that the impact of the development on existing social infrastructure provision has been appropriately considered and that the proposed provision will be adequate to meet forecasted need in accordance with this policy.
Policy 5.12 (Flood risk management); Policy 5.13 (Sustainable drainage)	<p>Proposals must comply with the flood risk assessment and management requirements of set out in the NPPF and supporting Technical Guidance.</p> <p>Proposals should utilise sustainable urban drainage systems (SUDS) unless there are practical reasons for not doing so and should aim to achieve Greenfield runoff rates and ensure that surface water runoff is managed as close to its source as possible. Drainage should be designed and implemented in ways that deliver other objectives of the London Plan.</p>	<p>Compliant: The River Brent diversion works are part of a host of infrastructure works designed to remove barriers and constraints to development and open up the wider area to ensure that the regeneration proposals capitalise on the excellent transport links and new open space areas that will be created.</p> <p>Conditions 44.2 - 44.4 of the s73 permission relate specifically to the diversion of the River Brent and associated works. This proposal has been based on initial technical work around the content of the above planning conditions and therefore complies with the policy.</p>

Policy	Content Summary	Extent of compliance and comment
Policy 6.1 (Strategic approach);	The Mayor will work with all relevant partners to encourage a high quality public realm where appropriate, a corridor-based approach should be taken to ensure the needs of street users and improvements to the public realm are co-ordinated.	Compliant: The proposed development will see the provision of a network of connecting different green spaces and will primarily allow access to/from the Living Bridge which connects the southern development via the market square to Clitterhouse Playing Fields and Claremont Park; and access to the Brent Cross shopping Centre.
Policy 6.2 (Providing public transport capacity and safeguarding land for transport)	Development proposal should improve integration, quality, accessibility, frequency and environmental performance of the public transport system.	Compliant: The proposed RMA provides several improved and new accessible bus stops as well as facilitating the new high quality bus station at the shopping centre to be implemented as part of Phase 1B (North). The new road layout will also facilitate the modification and improvement of bus services in future phases.
Policy 6.3 (Assessing effect of development of transport capacity)	Development proposals should ensure that impacts on transport capacity and the transport network, at both a corridor and local level are fully assessed. Transport assessments will be required in accordance with TfL's Transport Assessment Best Practice Guidance for major planning applications.	Compliant: The RMA has been preceded by a Phase Transport Report (PTR) for Phase 1 (Condition 37.2) and includes a Reserved Matters Transport Report for Phase 1A (North). Both of these have been carried out in accordance with the Section 73 Planning Permission and accompanying 2014 Section 106 Agreement which included the draft scope for both these reports, as agreed with TfL. TfL also agreed with the final scope for the PTR and the RMTR. The reports both deal thoroughly with all aspects of the transport impacts of the BXC Development as appropriate; in particular ensuring that impacts are fully assessed and sufficient transport capacity is provided across the modes. In addition the A5 has been examined as part of the A5 Corridor Study and several routes for pedestrians and cyclists have been examined as part of the Area Wide Walking and Cycling Study.
Policy 6.4 (Enhancing London's Transport connectivity)	Proposals illustrate opportunities related to locations which will benefit from increased public transport accessibility. Improve the public transport system to support future development and regeneration.	Part Compliant: The BXC development is in an area which currently has the full range of possible Public Transport Accessibility Levels (PTALs), and the future phases include key public transport improvements designed to ensure that the site has much better future PTAL. These are partly facilitated through the new road network created by this Phase 1A (North) RMA, which provides for direct access off the re-aligned Prince Charles Drive to the new bus station. The RMA also provides some of the key roads including Market Street and the western end of School Lane. The latter is planned to be bus-only at end state, and these two roads will provide an east-west connection for new / diverted bus services across the southern development, and when Market Square is linked with High Street South as part of a future phase there will a direct connection to the

Policy	Content Summary	Extent of compliance and comment
		new railway station.
Policy 6.7 (Better streets and surface transport)	Development proposals should promote bus networks; allocating road space and providing high level priority on existing and proposed routes. Ensuring good access to and within areas served by networks, now and in future; and ensuring direct, secure, accessible and pleasant walking routes to stops.	Compliant: The RMA provides a significant amount of new roads to facilitate existing and future bus routes, including providing connections off Prince Charles Drive to the new bus station. Seven new and 10 replacement accessible bus stops are included, together with two modified accessible coach stops. There is a Phase 1A (North) Pedestrian and Cycle Strategy approved under Condition 2.8 which includes details of the proposed walking routes to stops.
Policy 6.9 (Cycling)	Proposals should identify and implement a network of cycle routes. Contribute positively to an integrated cycling network for London by providing infrastructure that is safe, comfortable, attractive, coherent, direct and adaptable and in line with the guidance set out in the London Cycle Design Standards (or subsequent revisions).	<p>Compliant: The approved Area Wide Walking and Cycling Study (AWWCS - Condition 1.20) and the Phase 1A (North) Pedestrian and Cycle Strategy (Condition 2.8) will ensure that a coherent network of cycle routes is provided on a phase by phase basis as part of the overall scheme, including as part of AWWCS appropriate links between the site and adjacent communities. The developers will be required to fund appropriate improvements and mitigation measures outside the red line scheme boundary in the light of the AWWCS.</p> <p>The key cycling elements of the Phase 1A North Pedestrian and Cycling strategy included in the RMA are provision of key connection points including via Sturgess Park, Claremont Park, Clitterhouse Playing Fields (to the proposed Mayoral Cycle Superhighway 11 along the A41 corridor), Staples Corner and across the A41 via existing improved subways at Haley Road and Ridge Hill, and via the A41 / A406 junction. There is also improved access along the river Brent corridor and across the A406 for cyclists with the Living Bridge, a new bridge at Staples Corner (bridge B6) and the improved replacement Tempelhof Bridge. The cycle routes were designed as part of the outline approved scheme as so pre-date the latest TfL Design standards; however, cycle routes will be implemented in accordance with the latest standards where practicable.</p>
Policy 6.10 (Walking)	Development proposals should ensure high quality pedestrian environments and emphasise the quality of the pedestrian and street space by referring to Transport for London's Pedestrian Design Guidance. Promote the 'Legible London' programme to improve pedestrian way finding. Encourage the use of shared space principles, such as simplified streetscape, de-cluttering and access for all.	<p>Compliant: The approved Phase 1A (North) Pedestrian and Cycle Strategy (Condition 2.8) provides key high quality pedestrian routes via Sturgess Park, Claremont Park, Clitterhouse Playing Fields, Staples Corner and across the A41 via existing improved subways at Haley Road and Ridge Hill, and via the A41 / A406 junction.</p> <p>Improved access across the A406 is provided for pedestrians with the shared space Living Bridge close to the shopping centre and a new bridge at Staples Corner (bridge B6). Access over the A406 will be further enhanced by the improved replacement Tempelhof Bridge and the</p>

Policy	Content Summary	Extent of compliance and comment
		<p>improvement of facilities at the A406/A41 junction. A fully accessible pedestrian route along the re-aligned river Brent corridor is also provided.</p> <p>Signing throughout the development will be provided in accordance with the guidelines set out in Legible London, and contained in the Wayfinding and Inclusive Access Strategy. A high quality public realm is proposed throughout.</p>
Policy 6.12 (Road network capacity)	Proposals should improve the road network taking into account: where it contributes to sustainable development, regeneration and improved connectivity, the extent of any additional traffic and any effects it may have on the locality, congestion impacts, the net benefit to the environment, how conditions for pedestrians, cyclists, public transport and freight users and local residents, as well as road safety, can be improved.	Compliant: The RMA includes the necessary road improvements as part of the overall BXC Regeneration scheme to mitigate the impact of the development whilst at the same time providing improved connectivity both within the development area, particularly north of the A406, and to the wider network. Local and strategic traffic impacts are assessed in the Transport Assessment for the outline approved scheme, and the A5 corridor has been further assessed as part of the A5 Corridor Study (A5CS), subject of a separate report before Members. The Phase 1 impacts have been examined in the Phase Transport Report (PTR), approved under Condition (37.2), and the RMTR, which is considered to be consistent with all these. The A5CS, PTR, RMTR and approved Pre-RMA Conditions together address the other policy issues, the PTR and RMTR in particular provide a multi-modal assessment of the Phase 1A (North) proposals.
Policy 7.1 (Lifetime Neighbourhood)	Development should enable people to live healthy, active lives; should maximize the opportunity for community diversity, inclusion and cohesion; and should contribute to people's sense of place, safety and security. Places of work and leisure, streets, neighbourhoods, parks and open spaces should be designed to meet the needs of the community at all stages of people's lives, and should meet the principles of lifetime neighbourhoods.	Compliant: A key objective of the BXC scheme is to create a sustainable and balanced community. The new and existing community will benefit from the enhanced new and improved infrastructure developments.

Policy	Content Summary	Extent of compliance and comment
Policy 7.2 (An Inclusive Environment)	Development proposals should explain how following engagement with relevant user groups and the principles of inclusive design has been integrated into the proposed development; and how inclusion will be maintained and managed.	<p>Compliant: With regard to inclusive access a lift will be added from the Bus station and Living Bridge level to provide step free access to the Riverside Park; the exact location of the lift within the proposed identified zone and the specification and design of the lift, will be submitted and approved by the Council subject to consultation and agreement with the Consultative Access Forum.</p> <p>In addition, ramped access is provided to the Riverside Park by way of a graded route connecting a proposed upper walkway to the riverside park walkway.</p> <p>The inclusive access point in the Eastern Park is at the existing A41 underpass. For those travelling between the Tempelhof Bridge and the residential areas to the East of the A41, a ramp is proposed. This increases inclusive access to the river path to the west, via a graded slope of 1:21 with a dog leg and generous landings.</p>
Policy 7.3 (Designing out crime)	Development proposals should reduce the opportunities for criminal behaviour and contribute to a sense of security without being overbearing or intimidating.	<p>Compliant: The proposed non-highways infrastructure development will be subject to a proposed lighting strategy which will assist in designing out crime. Whilst lighting strategy details have been provided in the Design Development Report that accompanied the application the final detail has still to be agreed and is subject to planning conditions requiring the submission of relevant details.</p> <p>In addition, the Estate Management Framework includes a section on Safety in the Public Realm and notes that the Shopping Centre CCTV strategy will extend to the Central Brent Riverside Park.</p>
Policy 7.4 (Local character); Policy 7.5 (Public realm);	<p>Buildings, streets and open spaces should provide a high quality design that has regard to the pattern and grain of the existing spaces in orientation, scale, proportion and mass. Contributes to a positive relationship between the urban structure and natural landscape features.</p> <p>Development should make the public realm comprehensible at a human scale, using gateways, focal points and landmarks as appropriate to help people find their way. Landscape treatment, street furniture and infrastructure should be of the highest quality, have a clear purpose, maintain uncluttered spaces and should contribute to the easy movement of people through the space.</p>	<p>Compliant: The urban design principles that underpin the BXC masterplan as set out in the Revised Design and Access Statement, Design Guidelines, DSF and Parameter Plans fulfil the key criteria of these policies to provide high quality buildings, streets and spaces. The development incorporates the principles of “Secured by Design”</p> <p>Compliant: There are a number of key gateways and focal points to the public realm being provided as part of the RMA, for example the Living Bridge has been designed to ensure that it is a high quality shared-space that is attractive, fully accessible and user friendly. The developer will maintain areas of highway verge and landscape areas under license and the riverside park to ensure the appropriate high standards are maintained post construction.</p>

Policy	Content Summary	Extent of compliance and comment
<p>Policy 7.13 (Safety Security and Resilience to Emergency)</p>	<p>Development proposals should contribute to the minimization of potential physical risks, including those arising as a result of flood and related hazards.</p>	<p>Compliant: The highway proposals are generally in accordance with established national design guidance, and where these standards aren't achieved, then detailed assessment, risk assessment, and departure from standards application submitted.</p> <p>The risk caused by flood is low as access to the river corridor in times of expected flood will be controlled by the Development partners as required under the Flood Management Strategy; Condition 3.</p>
<p>Policy 7.14 (Improving air quality)</p>	<p>Development proposals should minimise increased exposure to existing poor air quality and make provision to address local problems of air quality. Reconcile the importance of tackling air pollution and improving air quality.</p>	<p>Compliant: The matter of Air Quality was assessed in the s73 Permission Environmental Statement and this assessment has been revisited in relation to the current RMA within the ES Further Information Report.</p> <p>In summary, the updated air quality modelling of road traffic from the Development has taken account of improvements in technology and tighter emissions controls through the future pollutant emission rates. Although the modelling results showed that, for NO₂, there would be a Substantial Adverse impact on NO₂ at the junction of the A5 Cricklewood Broadway and Cricklewood Lane, and Moderate Adverse impacts on NO₂ locally along stretches of Cricklewood Lane and Claremont Road, south of the Development, the impact at the vast majority of sensitive receptors (representative of existing properties) is Negligible. Furthermore, Slight Beneficial impacts are also predicted at a number of receptors. The impact on particulate matter (both PM₁₀ and PM_{2.5}) is also Negligible, without any mitigation measures in place.</p> <p>By way of mitigation it is noted that a number of measures to encourage non-car mode travel will be implemented through the Framework Travel Plan (FTP) for the Development. A draft FTP accompanied the s.73 Application with its main objectives being to reduce reliance on the private car and encourage future site occupiers / users to travel in a more sustainable manner. Any reduction in traffic flows through the implementation of the FTP would reduce predicted impacts on air quality.</p> <p>The ES notes that air quality monitoring would be necessary at the locations described above following completion of the Development. Where exceedences of the NO₂ annual mean objective are confirmed and attributed to the Development, specific mitigation measures such as mechanical ventilation and NO_x filtration may be required to ensure appropriate indoor air quality.</p> <p>Assuming the above FTP is implemented and</p>

Policy	Content Summary	Extent of compliance and comment
		effectively leads to reductions in traffic flows compared to the current forecast flows used in this assessment, it is considered that the residual impacts at the properties where a Substantial Adverse impact is predicted without mitigation would likely reduce to Moderate Adverse, considering that only a small reduction in NO _x emissions would be required.
Policy 7.15 (Reducing and managing noise, improving and enhancing the acoustic environment and promoting appropriate soundscapes)	Development proposals should seek to mitigating and minimizing the existing and potential adverse impacts of noise. Separate new noise from major noise sources (such as road) through the use of distance or acoustic screening.	Compliant: An acoustic barrier is required to ensure that the Brent Riverside Park is protected from the noise of the adjacent A406 (north circular) and can meet suitable standards expected for a publically accessible open space. It is anticipated that this noise barrier may be within the public highway and it is expected that the Highways Authority will either give a license to the Brent Cross Cricklewood development partners to allow them to maintain the structure or else will require a commuted sum to cover any maintenance costs should the structure be adopted as part of the public highway by the relevant highway authority.
Policy 7.19 (Biodiversity and access to nature)	Proposals should: <ul style="list-style-type: none"> - Wherever possible make a positive contribution to the protection, enhancement, creation and management of biodiversity. - Prioritise assisting in meeting targets in biodiversity action plans and/or improve access to nature in areas deficient in accessible wildlife sites. - Be resisted where they have significant adverse impacts on the population or conservation status of a protected species, or a priority species or habitat identified in a biodiversity action plan. 	Compliant: One of the key principles of this proposal is to improve local bio-diversity within the Riverside Park environment. The creation of a marginal wetland environment associated with the river will help facilitate this aim. It is considered that the development will result in a net gain in biodiversity in terms of habitat quality and connectivity.
Policy 7.21 (Trees and woodlands)	Existing trees of value should be retained and any loss as a result of development should be replaced. Wherever appropriate the planting of additional trees should be included in developments.	Compliant: The proposed development includes extensive tree planting and landscaping throughout the development. The new planting will compensate for the loss of existing trees due to be removed to accommodate the new Shopping Centre and infrastructure.
Policy 7.28 (Restoration of The Blue Ribbon Network)	Development proposals should restore and enhance the Blue Ribbon Network by taking opportunities to open culverts and naturalise river channels, increasing habitat value and protect the open character of the Blue Ribbon Network.	Compliant: It is considered that the proposal enhances local habitat, ecology and biodiversity and makes a positive contribution to the area. The proposal is in accordance with this policy.

Policy	Content Summary	Extent of compliance and comment
Policy 7.30 (London's canals and other river's and water spaces)	Development proposals along London's canal network and other rivers and water bodies (such as reservoirs, lakes and ponds) should respect their local character and contribute to their accessibility and active water related uses, in particular transport uses, where these are possible.	Compliant: It is considered that the overall improvement of the habitat of rivers brooks and streams in accordance with this policy.

Table 2.2: Analysis of the proposals compliance with Barnet's Local Plan Policies
(September 2012)

Policy	Content Summary	Extent of Compliance and Comment
Core Strategy		
CS NPPF (National Planning Policy Framework – presumption in favour of sustainable development)	Take a positive approach to proposals which reflect the presumption in favour of sustainable development and approve applications that accord with the Local Plan, unless material considerations indicate otherwise. Where there are no policies relevant to the proposal or the relevant policies are out of date permission should be granted, unless material considerations indicate otherwise.	Compliant: the proposal is considered to constitute sustainable development in accordance with the NPPF and which complies with Local Plan taken as a whole. It is therefore recommended for approval.
CS1 (Barnet's place shaping strategy – the three strands approach)	<p>As part of its 'Three Strands Approach' the council will:</p> <ul style="list-style-type: none"> - Concentrate and consolidate growth in well located areas that provide opportunities for development, creating a high quality environment that will have positive impacts. - Focus major growth in the most suitable locations and ensure that this delivers sustainable development, while continuing to conserve and enhance the distinctiveness of Barnet as a place to live, work and visit. - Ensure that development funds infrastructure through Section 106 Agreements and other funding mechanisms. - Protect and enhance Barnet's high quality suburbs. 	<p>Compliant: the proposal is considered to show the influence of this policy and demonstrates compliance with its key objectives.</p> <p>As an Opportunity Area in the Mayor's London Plan, the BXC scheme has been developed with the consideration that the site has significant capacity for new housing, commercial and other development linked to existing or potential improvements to public transport accessibility.</p> <p>The proposed development relates to matters reserved following the grant of planning permission in 2014.</p> <p>The majority of the infrastructure improvements associated with the scheme will be delivered during Phase 1 in preparation for the proposed development due to come forwards during the subsequent phases. The network created in sub-phase 1A North provides all necessary connections to facilitate the rest of the northern development and in addition, some of the key roads south of the A406. Key elements of the road network for the Southern development are provided.</p>
CS2 – Brent Cross – Cricklewood	<p>The Council will seek comprehensive redevelopment of Brent Cross – Cricklewood in accordance with the London Plan, the saved UDP policies (Chapter 12) and the adopted Development Framework. The Policy makes provision for the following:</p> <ul style="list-style-type: none"> • It is considered likely that comprehensive regeneration will be achieved in accordance with the planning permission. If this is not achieved, the Council will 	<p>Compliant: The 2014 consent of the S73 application continues to ensure the comprehensive redevelopment of Brent Cross Cricklewood in accordance with this policy and the Saved UDP policies. The current Reserved Matters Application for the proposed for access and Landscaping for infrastructure including roads, bridges and River Brent diversion works is consistent with the 2014 permission.</p> <p>The infrastructure will be enhanced for visitors to the Brent Cross Shopping Centre and the Southern development. It will be an asset for the public and</p>

Policy	Content Summary	Extent of Compliance and Comment
	<p>consider whether in the circumstances the Local Plan needs to be reviewed.</p> <ul style="list-style-type: none"> • Specific monitoring indicators for Brent Cross – Cricklewood are set out in Appendix B of the Core Strategy On the basis of these indicators it is expected that comprehensive re-development will commence in relation to Phase 1 at some time between 2015 and 2017. • If these milestones are not achieved (or are not likely to be capable of being delivered) we will consider the possible need for a review of the Core Strategy Policy on Brent Cross – Cricklewood • The key milestone for the regeneration of Brent Cross – Cricklewood is likely to be the Phase 1 Compulsory Purchase Order (CPO). If by the end of 2014 any CPO that is required to deliver Phase 1 and commence the development has not been made and submitted for confirmation we will instigate a review of the policy framework for Brent Cross – Cricklewood. 	<p>existing neighbourhoods. The new infrastructure will support the wider Brent Cross regeneration.</p>
<p>CS9 (Providing safe, efficient and effective travel)</p>	<p>Promote the delivery of appropriate transport infrastructure in order to support growth.</p> <p>Ensure new development funds infrastructure to keep existing traffic moving and cope with new demands.</p> <p>Key elements include:</p> <ul style="list-style-type: none"> • Ensuring More Efficient use of the local road network • Taking a comprehensive approach to tackling the school run • Delivery of high quality transport systems in regeneration areas and town centres <p>Major proposals should incorporate Transport Assessments, Travel Plans, Delivery and Servicing Plans and mitigation measures and ensure that adequate capacity and high quality safe</p>	<p>Part Compliant: The RMA is a key element in the phased delivery of a high quality transport system at BXC and includes the appropriate level of Transport Assessment agreed by all key stakeholders, including a detailed Reserved Matters Transport Report (RMTR). The RMTR and associated parts of the submission demonstrates that the Phase 1A (North) infrastructure provides adequate and safe highway capacity and describes the proposals for a comprehensive range of improvements to all modes of transport. Major improvements to the A406 at Staples Corner and to the A41 interchange are part of the phase, as are the connections to the proposed new bus station are provided off Prince Charles Drive. The Phase 1A (North) road layout will also facilitate future bus service modifications and enhancements. The Rapid Transit Bus Service and improvements to Cricklewood station remain, although they are currently unfunded. Other proposals in CS9 related BXC are planned to be delivered as part of future phases or are being addressed through specific Conditions.</p>

Policy	Content Summary	Extent of Compliance and Comment
	<p>transport facilities are delivered in line with demand.</p> <p>Deliver with partners high quality public transport improvements along the A5, bus service enhancements as part of regeneration schemes, including a high quality bus station at BXC and a Rapid Transit bus service.</p>	
<p>CS10 (Enabling inclusive and integrated community facilities and uses)</p>	<p>The council will ensure that community facilities are provided for Barnet's communities and expect development that increases the demand for community facilities and services to make appropriate contributions towards new and accessible facilities.</p>	<p>Compliant: With regard to inclusive access a lift will be added from the Bus station and Living Bridge level to provide step free access to the Riverside Park; the exact location of the lift within the proposed identified zone and the specification and design of the lift, will be submitted and approved by the Council subject to consultation and agreement with the Consultative Access Forum.</p> <p>In addition, ramped access is provided to the Riverside Park by way of a graded route connecting a proposed upper walkway to the riverside park walkway.</p> <p>The inclusive access point in the Eastern Park is at the existing A41 underpass. For those travelling between the Tempelhof Bridge and the residential areas to the East of the A41, a ramp is proposed. This increases inclusive access to the river path to the west, via a graded slope of 1:21 with a dog leg and generous landings.</p>
<p>CS11 (Improving health and wellbeing in Barnet)</p>	<p>Will improve health and wellbeing in Barnet through a range of measures including supporting healthier neighbourhoods, ensuring increased access to green spaces and improving opportunities for higher levels of physical activity.</p>	<p>Compliant: Although the Infrastructure developments serves to enhance the redevelopment of Brent Cross shopping Centre. The development also provided key enhanced and improved pedestrian and cycle routes via Sturgess Park, Claremont Park and Clitterhouse Playing Fields. Additionally, a linear green corridor incorporating a new pedestrian and cycle access will enable residents to move west to east without having to access the shopping centre and remote from vehicular traffic.</p>
<p>CS12 (Making Barnet a safer place)</p>	<p>The Council will:</p> <ul style="list-style-type: none"> - Encourage appropriate security and community safety measures in developments and the transport network. - Require developers to demonstrate that they have incorporated community safety and security design principles in new development. - Promote safer streets and public areas, including open spaces. 	<p>Compliant: a lighting strategy is proposed by the development partners. Although indicative details have been submitted with the current application full design details are still required and will be subject to the requirements of a planning condition. The lighting strategy will ensure the environment is secure.</p> <p>In addition, the Estate Management Framework Agreement advises that a team of SIA (Security Industry Association) accredited CCTV operators (closed circuit television) will operate an extensive system within Brent Cross shopping centre 24 hours of every day supporting the Duty Assistant team in</p>

Policy	Content Summary	Extent of Compliance and Comment
		<p>providing a safe and secure environment.</p> <p>These systems will extend to the Central Brent Riverside Park and Living Bridge (surface).</p>
<p>CS15 (Delivering the Core Strategy)</p>	<p>The council will work with partners to deliver the vision, objectives and policies of the Core Strategy, including working with developers and using planning obligations (and other funding mechanism where appropriate) to support the delivery of infrastructure, facilities and services to meet needs generated by development and mitigate the impact of development.</p>	<p>Compliant: the Section 73 permission was subject to a S106 Agreement. The current RMA sits under the S73 permission and the requirements of the s106 are still applicable. Where required, the s106 will be varied in order to secure deliver of specific elements within the current application.</p>
<p>Adopted UDP (May 2006) Saved Policies (May 2009) as referred to in Core Strategy Policy CS2 – Chapter 12 of the UDP: Cricklewood, Brent Cross and West Hendon Regeneration Area</p>		
<p>GCrick Cricklewood, Brent Cross and West Hendon Regeneration Area</p>	<ul style="list-style-type: none"> - The Council seeks integrated regeneration in the Cricklewood, Brent Cross and West Hendon Regeneration Area. - All development to the highest environmental and design standards - Aim to develop a new town centre over the plan period. 	<p>Compliant: the development is consistent with this strategic aim, falling as it does within the terms of the S73 permission.</p>
<p>C1 Comprehensive Development</p>	<ul style="list-style-type: none"> - The Council seeks the comprehensive development of the regeneration area in accordance with the area framework and delivery strategy. - Development proposals will need to meet policies of the UDP and their more detailed elaboration in the development framework. 	<p>Compliant: The scheme accords with this policy. The proposals are Phase 1A (N) proposals within the scope of the S73 permission, the first phase of development within the wider Brent Cross Cricklewood regeneration project.</p>
<p>C7 Transport Improvements</p>	<p>Transport improvements - the following should be provided through planning conditions and/or Section 106 agreements:</p> <ul style="list-style-type: none"> i. Connections and improvements to the strategic road network. ii. Sufficient transport links to and through the development, to include at least one vehicular link across the North Circular Road (NCR) and one vehicular link crossing the railway to the Edgware Road. iii. A new integrated railway station and new integrated bus station at 	<p>Part Compliant: The RMA includes five of the gateway junctions providing connections to the strategic roads in the area and the new Tempelhof bridge across the NCR. It also facilitates the new bus station by providing the connections off Prince Charles Drive and as explained in the various other policy sections above provides compliant facilities for pedestrians including disabled people, cyclists and bus users. The other facilities set out in policy C7 are planned to be provided as part of future phases, except for the rapid transit system which, whilst still part of the proposals, is currently unfunded.</p>

Policy	Content Summary	Extent of Compliance and Comment
	<p>Cricklewood, linked by a rapid transport system to Brent Cross Bus Station and Hendon Central and/or Brent Cross Underground Stations on the Northern Line.</p> <p>iv. A new bus station at Brent Cross, to north of the North Circular Road, with associated improvements to the local bus infrastructure.</p> <p>v. An upgrade of the rail freight facilities.</p> <p>vi. Provision of an enhanced, rail-linked waste transfer station serving North London.</p> <p>vii. Priority measures for access to disabled persons, pedestrians, buses and cyclists throughout the Regeneration Area.</p>	
<p>UDP Site Specific Proposals (2006)</p>	<p>Parts of the BXC development site are subject to site-specific proposals as shown in the Proposals Map (2006) (as saved). The aspirations for these sites are set out below:</p> <ul style="list-style-type: none"> • Brent Cross New Town Centre (Site 31) – new town centre, comprising a mix of appropriate uses, improved public transport and pedestrian access, landscaping and diversion of the River Brent; • Cricklewood Eastern Lands (Site 37) – mixed use including residential, office, leisure, local and neighbourhood shops, education, community uses and open space; • New Railway Station Cricklewood (Site 38) – railway station and public transport interchange; • New Waste Transfer Station (Site 39) – waste handling facility. 	<p>Compliant: the proposal is consistent with the Proposals Map.</p>
<p>Cricklewood, Brent Cross, West Hendon Regeneration Area Development Framework SPD 2005.</p>		
<p>The Cricklewood, Brent Cross and West Hendon Regeneration Area Development Framework was adopted by the council and the Mayor of London as Supplementary</p>	<p>Compliant: Compliance in relation to the proposed Infrastructure RMA. : The parameters and principles of the BXC scheme are considered to be</p>	

Policy	Content Summary	Extent of Compliance and Comment
	<p>Planning Guidance in 2005. This Development Framework was produced in collaboration with the Mayor and the Greater London Authority, landowners and developers in order to guide and inform the design and delivery of the development with the aim of achieving high quality comprehensive redevelopment of the area around a new sustainable mixed use town centre spanning the North Circular Road.</p> <p>The London Plan and the UDP saved policies combined with the Development Framework establishes a series of strategic principles for the comprehensive redevelopment of the area to create a new town centre, the overall vision for which is set out in UDP Policy GCrick.</p>	<p>in accordance with the principles set out in the guidance contained in the adopted Development Framework (2005) generally and taken as a whole.</p> <p>The general proposals are consistent with the vision and aims of the Development Framework.</p>

Key relevant local and strategic supplementary planning documents

Local Supplementary Planning Documents and Guidance:

- Infrastructure Delivery Plan (2011)
- Sustainable Design and Construction (2013)
- Planning Obligations (Section 106) (April 2013)

Strategic Supplementary Planning Documents and Guidance:

- Accessible London: Achieving an Inclusive Environment (April 2004)
- Sustainable Design and Construction (May 2006)
- London's Foundations SPG (March 2012)

This page is intentionally left blank

APPENDIX 6

RESIDENTIAL CONSULTATION RESPONSES

First Consultation Responses for Consultation Period Ending 10/07/2015

Resident Comments	Officer Response
<p><u>Resident 1 Comments:</u> Objection to the planning and rejuvenation of the Whitefield Estate based on the S73 Contamination Assessment Report. (BXC 17 – Supplementary Section 73 Phase 1 Geo-Environmental Assessment & Geotechnical Development Report; Appendix F- Contamination Assessment Report and study.)</p>	<p>It is not clear from the letter of objection submitted what aspect of the Contamination Assessment is of concern in relation to the Whitefield Estate.</p> <p>A Contamination Assessment was submitted as part of the S73 Planning Permission Ref No: F/04687/13. Appendix F- Contamination Assessment Report forms part of the Supplementary Phase 1 Geo- Environmental Assessment, which was carried out to assess current ground investigations and summarises the contamination results obtained.</p> <p>Planning Conditions 31.1 – 31.7 of the Section 73 permission relate specifically to Remediation of Contamination, and are in place to protect human health, prevent any potential contamination and ensure necessary remediation at the site has been undertaken.</p> <p>Condition 31.1 is a Pre reserved matters condition which was submitted and approved for Phase 1A North, prior to submission of any reserved matters application. An initial framework and breakdown of areas of land for remediation along with schedules of earthworks and soil treatment activities relevant to each of these Remediation Zones or Sub-Zones have been provided for Phase 1A north.</p> <p>The Brent Terrace Triangles RMA in relation to Plots 53 and 54 has been determined and it is intended that the accommodation provided through this application will provide accommodation for Whitefield Residents displaced within Phase 1a (North). Planning Condition 1.10 (Residential Relocation Strategy) is currently under consideration. The aim of this Condition is to ensure the satisfactory relocation of Whitefield residents.</p>

Resident 2 Comments:

The development will encourage vehicles to enter Oxgate Gardens in Dollis Hill and will increase the existing flow of traffic even further. Oxgate Gardens is a residential road with a primary school located nearby; It is already used as a cut through to avoid existing traffic lights and the proximity of the BXC development would increase noise and disturbances.

Parking is extremely tight and on daily basis the majority of spaces are used by the existing factory workers, leaving residents of Oxgate Gardens to park on surrounding residential streets. Residents of Oxgate Gardens are going to approach Brent Council to request parking permits or restrictions.

When the car parks are full or when people are unwilling or unable to pay for parking visitors will park on surrounding residential streets close to the development; this would have a further detrimental impact on parking conditions. The installation of the traffic lights on Oxgate Gardens has caused a loss of privacy, and the proposed development will have a further impact.

Lorries during construction will use Oxgate Gardens to access the waste dump

The traffic impacts on Oxgate Gardens due to phase 1A North have been assessed as part of the A5 Corridor Study, and the plots in Appendix E of the study report shows that only very small increases are forecast.

The current parking situation is a matter for Brent Council.

Parking impacts of the BXC development are going to be managed through the Section 106 and under various planning conditions. It may be desirable to introduce Controlled Parking Zones on local roads but this will be subject to public consultation and (in the case of Oxgate Gardens) will be led by the LB of Brent. Through the Transport Advisory Group funding can be made available from the Section 106 Consolidated Transport Fund Other Boroughs Fund (maximum £1.25m).

The movement of construction vehicles will be fully considered in the Construction Transport Management Plan (Condition 12.1) which is a pre-commencement condition. It is planned that deliveries to the site will be controlled through a web based ordering system to ensure that only materials required in the short term are delivered and stored on site, and this will not include the waste dump referred to. Moreover the Construction Impact Assessment (BXC21), already submitted and approved as part of the S73 is based on the use of the main roads in the area, with the vast majority of construction traffic expected to use the M1 and A406. Controls are expected to be in place to ensure no lorries use any local roads.

<p>Resident 3 Comments: With reference to the newspaper article published in the Evening Standard on the 4th June 2015 titled ‘1300 people have died this year due to polluted air in the capital’; concerns have been raised whether the Brent Cross infrastructure development would create more pollution?</p> <p>Staples Corner and the surrounding areas currently suffer from large volumes of traffic. The proposed development would increase the traffic flows.</p>	<p>This matter was considered at the time of the outline application and is not a matter currently under consideration as part of this Reserved Matter Application. The applicant has committed to managing air quality and dust impacts during construction.</p> <p>There are a number of planning conditions proposed to control this aspect including the requirement to establish air quality monitoring stations before development commences.</p> <p>All traffic flow changes have been carefully considered as part of the development of the outline approved scheme, and have recently been re-examined through the BXC Detailed Design Model, as explained in the main report. The BXC development includes a comprehensive multi-modal package of phased mitigation measures, and a framework of control designed to ensure that traffic volumes are effectively managed and mode-shift away from cars encouraged as the scheme rolls out.</p>
<p>Resident 4 Comments: Documentation submitted for the Reserved Matters Application has led to confusion and is not clear.</p>	<p>It is appreciated that this is a complicated Planning application for residents to comment on. The planning case officer is available (on the telephone) to discuss and explain the planning application and Reserved Matters Applications to local residents.</p>

<p>Resident 5 Comments: Referring to the Phase 1a (North) Illustrative Reconciliation Plan and the location of the Waste Handling Plant, concerns have been raised whether the Waste Transport Lorries will use the following roads: Dollis Hill Lane, Coles Green Road, Oxgate Lane and Humber Road.</p> <p>Is waste going to be incineration from the Waste disposal unit? And what method will be used to dispose the waste once it enters the Waste Handling Facility?</p> <p>Have any solutions been considered to prevent any potential or eliminate smells travelling beyond the facility and prevent travelling to neighbouring residents? How will the Waste Handling Facilities eliminate avoiding any potential rodents or bugs? And will residents living nearby be inconvenienced by the waste collection facility?</p>	<p>The location of the proposed Waste Handling Facility was established under the 2010 Outline Consent and remained unchanged in the Section 73 application. Details regarding the Waste Handling Facility are not part of this current Reserved Matters Planning Application and will be dealt under a future RMA submission for Phase 1B South. The location and the principles of the Waste Handling Facility have not changed since the 2010 Planning approval and this has been included in the Section 73 Planning Permission Ref No: F/04687/13.</p> <p>Planning condition 41.1 of the S73 planning permission establishes the information that will need to be submitted before the Waste Handling Facility is constructed to ensure high standards of urban design; landscaping, environmental. Details regarding highway access and heavy goods vehicle routing will also be dealt under this condition.</p> <p>The fuel source for the Waste Handling Facility has not been determined at this stage. The emissions from such plants will be tightly controlled and monitored by the Environment Agency;</p>
---	---

Second Consultation Responses for Consultation Period Ending 12/08/2015

<p>Resident 6 Comments: The proposed plans fail to solve the main cycle route required for this area, in particular north-south on the A5 Corridor.</p> <p>The proposed routes through the development are not practical or safe for cyclists. Conditions would be worse after the development at Staples Corner.</p> <p>It is considered the best solution for the A5 is to demolish the existing A5 flyover. It is in a bad condition and only</p>	<p>The proposed sub-phase 1A North highway layout incorporates improvements for cyclists as set out in the approved Pedestrian and Cycle Strategy, including providing connections to existing cycle routes and local roads used as quiet routes by cyclists. There are currently few facilities for cyclists but the proposed facilities will provide a comprehensive network when the development is complete and key parts of the network are provided as part of sub-phase 1A North, including the route between the A41, Clitterhouse Playing Fields, Brent Cross shopping centre and local roads to the north, a route that is mostly segregated. The already approved Staples Corner scheme is considered to provide the best solution at this location and includes a new bridge (B6) for pedestrians and cyclists that together with other existing and modified</p>
---	---

<p>serves to move small volumes of traffic through Cricklewood to West Hendon. A new junction or crossroad at ground level with a cycle friendly roundabout would be practical. The space currently occupied by the A5 slip roads could therefore be used for segregated cycle paths, wide pavements for pedestrians and landscape which would provide urban realm benefits. This would also provide a pedestrian and cycle crossing from the new development to the Welsh Harp open spaces.</p> <p>Paths for cyclists and pedestrians need to be segregated; shared paths will only create potential conflicts and limit the uptake for cycling.</p>	<p>facilities will separate vulnerable road users from traffic. A cycle connection to the Welsh Harp is planned as part of the approved Area Wide Walking and Cycling Study, and will be via local roads to the north of the shopping centre and then West Hendon.</p>
<p><u>Resident 7 Response:</u> Have any designs or proposals been considered to include a pelican or zebra crossing on Highfield Avenue opposite the entrance of Brent Cross Underground Station?</p> <p>Highfield Avenue is notorious for speeding and there have been accidents; school children and adults cannot safely cross the road. Will speed cameras or road signs be introduced to slow down the traffic?</p>	<p>A crossing outside the station is being considered as part of the overall package of improvements to the station.</p> <p>Monitoring of traffic movements in the area will be undertaken as the development proceeds and if related traffic levels increase and are determined to be having local impacts funding will be available to introduce appropriate traffic calming measures.</p>
<p><u>Resident 8 Response:</u> Concerns have been raised regarding the development of the project and information provided to residents of the Whitefield Estate.</p>	<p>The detailed arrangements for residents of the Whitefield Estate are not the subject of this Reserved Matter Application for infrastructure necessary to deliver the first phase of the Brent Cross regeneration. However, the Brent Terrace Triangles RMA in relation to Plots 53 and 54 was approved earlier this year and it is intended that the accommodation provided through this application will provide accommodation for the Whitefield Estate Replacement Units (Part 1) displaced within Phase 1a (North) of the development. Planning Condition 1.10 (Residential Relocation Strategy) is currently under consideration. The aim of this Condition is to ensure the satisfactory</p>

<p>Objection to residents being relocated, losing the right to buy and future rent and service charges.</p> <p>Object to becoming a Housing Association tenant.</p>	<p>relocation of Whitefield residents</p> <p>Planning Condition 1.10 (Residential Relocation Strategy) is currently under consideration. The aim of this Condition is to ensure the satisfactory relocation of Whitefield residents.</p> <p>In addition a Residents Advisor has been appointed by the developer and the officers from the Council's Regeneration Service are continuing to meet with residents of the Whitefield Estate.</p>
<p><u>Resident 9 Response:</u> <u>A5 Corridor Study:</u></p> <p>Concerns Regarding the A5 Corridor study has been raised. There is no clarity whether the traffic impacts of the surrounding regeneration developments (West Hendon and A2 Dominion) have been taken into consideration.</p> <p>Existing traffic saturations have not been taken into consideration. Only the calculated baseline traffic; presuming with all the surrounding developments has been predicted. A comparison of queue lengths and journey times is essential.</p> <p>The visual report does not correlate with actual experiences of travelling on the A5; including significant delays to buses in the PM peak.</p> <p>We are not certain where Kara road is where there is unused cycle provision.</p> <p>Increases of 2-3% in saturation are considered insignificant, but such an increase on a road 95% saturated increases the risk of melt down by 100%. The congestion on the network will impact the buses, and</p>	<p>Two Reserved Matters Applications and the A5 Corridor Study condition 2.7 of the Section 73 approved application (14/07402/CON) are before this Planning Committee for consideration; and this particular submission specially relates to the Infrastructure RMA. Whilst the objection letter refers to both the Infrastructure RMA and the A5 Corridor Study condition 2.7 planning references, officer comments can be found under Appendix 6 in the committee report for the A5 Corridor Study condition 2.7 (14/07402/CON).</p> <p>Please see response above.</p>

<p>these journey times need improving.</p> <p>The mitigations included in the outline approval seem to be renegotiable. The A5 Corridor study submitted states that there is some unresolved traffic jam which will have to be resolved at Barnet Councils expense. Can you clarify this?</p> <p>It is proposed the New Railway Station was a planning gain and would only be built when the developer have sufficient revenue to justify. It is not proposed to bring new station works forward, which will be funded by the Central Government and not the developer.</p> <p>What is the turnaround for each bus stop and how many buses need to stop at each location at one time? This information has not been provided and will this be available?</p> <p>There is no drop off proposed for Cricklewood station and why has this been excluded? The outline application stated that there would be step free access for Cricklewood Station; has this been withdrawn?</p> <p>Widths of pavements are a concern particularly if the bus stops are to cope with huge crowds of people</p> <p>Servicing and delivery has not been adequately solved in this application.</p> <p>Housing still being shown on the plans in front of B&Q where roadside servicing is indicated in the outline approval.</p> <p>The A5 Corridor study should have outlined how traffic would be monitored through the development</p>	<p>Please see above</p>
---	-------------------------

<p>programme; to confirm the level of modal shift that would occur and how it would be adjusted. These aspects have not been taken into consideration.</p>	
<p><u>Resident 10 Response:</u> The Junctions at Claremont, Cricklewood Lane and Lichfield Road are currently very busy and difficult to navigate. When this junction gets busy it impacts the junction of Chichele Road, Cricklewood Broadway and Cricklewood Lane. Are there any proposals to improve these junctions?</p>	<p>These junctions are both proposed to be improved as part of phase 1A North. The schemes themselves have already received planning approval as they are 2 of the key gateway junctions to the BXC development, approved as part of the outline permission.</p>
<p><u>Resident 11 Response:</u> The roads currently are already heavily congested and at certain times of the day the traffic is standstill causing dangerous levels of pollution. Any development designed will increase the pollution or attract higher number of visitors to Brent Cross; which will have a negative impact to the area.</p>	<p>The overall impacts of the BXC development were considered at the outline application stage. The development includes a new railway station and other major improvements to public transport and non-car modes, and the section 106 and Conditions contain a robust framework of control to help ensure the future mode split of trips in the local area is more heavily weighted towards these modes. Increases in vehicular traffic are expected to be catered for through the delivery of a new and more efficient road network, including several key junction improvements, and a significant part of the new road network is the subject of the sub-phase 1A North infrastructure Reserved Matters Application.</p>

<p><u>Resident 12 Response:</u> The Cricklewood part of the Edgware Road is already heavily congested and will be unable to cope with the massive increase in traffic generated by the proposed development. The results estimated does not give confidence that the A5 will be able to deal with the proposed changed in traffic.</p>	<p>Two Reserved Matters Applications and the A5 Corridor Study condition 2.7 of the Section 73 approved application (14/07402/CON) are before this Planning Committee for consideration; and this particular submission specially relates to the Infrastructure RMA. Whilst the objection letter refers to both the Infrastructure RMA and the A5 Corridor Study condition 2.7 planning references, officer comments can be found under Appendix 6 in the committee report for the A5 Corridor Study condition 2.7 (14/07402/CON).</p>
<p><u>Resident 13 Response:</u> To improve the traffic impacts in the area a priority bus route across the railway and running West to East through the middle of the development needs to be created. The road should be situated half way between the North Circular and Cricklewood Lane; this would create a more practical and circumferential route.</p> <p>The submitted documents states there is no space or capacity in the area for more road traffic and congestion is probably limiting traffic growth; the developers should think outside the box and consider other options to increase the comfortable movement and invest in long term needs.</p>	<p>The bus priority route referred to is part of the overall scheme and by the time the final phase is complete buses will be able to use the proposed Midland Mainline bridge to access an east-west route across the site via Spine Road North, High Street South, Market Square, School Lane and Whitefield Avenue. School Lane is proposed to be bus only, all roads will have bus stops and bus lanes are proposed on the railway bridge, spine road and high street. Only Market Square and the western end of School Lane is provided as part of sub-phase 1A North, the rest of the route will be provided as part of later phases.</p> <p>The BXC development includes a comprehensive multi-modal package of lasting and fully accessible transport schemes including a network of new and improved cycle routes, substantial public transport improvements including a new railway station and a commitment to a range of travel planning measures including car clubs, electric vehicle charging points, subsidised public transport vouchers and cycle purchase discounts.</p>
<p><u>Resident 14 Response:</u> <u>This resident is promoting the re-opening of the Dudding Hill Freight Line which could provide an orbital route across Brent to the 'new' Thameslink Station at Brent Cross</u> In the November 2009 planning committee it was stated that Brent Council had no objections to the Planning Application; this is in breach of the Human Rights Act 1998 Section 6 Paragraph 1.</p>	<p>The Dudding Hill Freight Line is not under consideration as part of this Reserved Matter Application.</p>

<p>Why was the A5 Corridor Study (14/07402/CON) open to public consultation but not the Phase Transport Report for Phase 1A (15/00812/CON)? And What are the current Transport- related Planning applications?</p> <p>Concerns have been raised that the Living Bridge only goes to shopping centre; cyclists who use this bridge would have to carry their bikes down a flight of steps to access the River Brent footpath, or use the Tempelhof Bridge and the double- roundabout. This is in breach with the London Plan.</p> <p>The Mayors guidance regarding cycling standards with segregated paths and Barents SPG predicting over 29,000 extra cars per day in the Brent Cross area are reasons for rejection. Advance stop lines and white line down a few roads should not be considered as cycling infrastructure.</p> <p>The following comments are “questions to the Mayor of London” that have been raised separately through the</p>	<p>Brent Council did raise objections and these were recorded in the committee report presented at the 18 and 19 November 2009 Planning and Environment Committee.</p> <p>The Phase Transport Report for Phase 1 was submitted for consideration under Condition 37.2. It was open to public consultation under reference: 15/03312/RMA. The Reserved Matters Transport Report for sub-phase 1A North is part of the Infrastructure Reserved Matters Application. More detail is set out in the main part of the report.</p> <p>Cyclists wishing to access the river corridor will be signed via the Tempelhof bridge and then via a pedestrian and cycle ramp which provides direct access to the riverside pedestrian and cycle path. This facility is considered to comply with the London Plan and is supported by TfL. Notwithstanding this, there is a lift proposed from the Living Bridge down to the bus station that can be used by cyclists, or alternatively, slots for bicycle wheels to allow cyclists to use the alternative steps should they so wish. The consultation proposals did not provide a further lift from the bus station level to the river corridor, but following discussions with the Developers it has been agreed to now provide this which can be used by cyclists.</p> <p>The issue of 29,000 extra cars per day was addressed at the outline application stage, when it was clarified that it is expected to be far fewer (some 11,000). A comprehensive network of cycle routes was provided as part of the outline application, and this was developed consistent with the design standards at the time (2009). Where facilities are being provided as part of Reserved Matters Applications the aim is to provide them, where practicable, consistent with current design standards. It should be noted that the key route between the A41 and the shopping centre is virtually all segregated, apart from a very short stretch in the southeast corner of Clitterhouse playing fields.</p> <p>These questions have been responded to by the Mayor. Any new questions relevant to the RMA are underlined and a response provided.</p>
---	---

GLA and considered to be bought to the attention of the planning committee. Concerns have been raised regarding the Brent Cross regeneration and it has been questioned whether this RMA Planning Application is fully compliant with the Mayor's Standards.

Brent Cross Highways Infrastructure (1)
Question to the Mayor of London from Navin Shah (21-May-2015):

You have stated that you have been in detailed discussions over highways design at Brent Cross. Can you confirm that Brent Cross Phase One now totally conforms to your London Cycle Design Guidance of 2014?

If not, in which locations is it deficient?

Response from the Mayor of London (21-May-2015):

The Brent Cross Cricklewood project received planning permission in 2010. Since this time, cycling aspirations and standards have evolved significantly and TfL is supporting the developer to improve current proposals to ensure they align with today's expectations and are compliant with the London Cycling Design Standards 2014. This work is still underway.

Brent Cross Highways Infrastructure (2)
Question to the Mayor of London from Navin Shah (21-May-2015):

Thanks to your insistence, can you confirm that there are now no non-segregated cycle paths, or highway unprotected cycle lanes or advance stop lines in the Brent Cross Phase One plan?

If not, in which locations have you failed?

Response from the Mayor of London (21-May-2015):

Please see my response to Brent Cross Highways Infrastructure (1).

Brent Cross Highways Infrastructure (3)
Question to the Mayor of London from Navin Shah (21-May-2015):

Having concluding your discussions, will every part of Brent Cross Phase One plan exceed the standards that you illustrate and promote in your Better Streets Delivered Case Studies of 2013?

If not, which parts of Brent Cross are deficient?

Response from the Mayor of London (21-May-2015):

TfL has prepared its transport and highway requirements for Brent Cross Phase One which includes the application of Better Streets design principles to the highway network and is working with the developer and the London Borough of Barnet to ensure these requirements are adopted through the planning process. Discussions are on-going.

Brent Cross Highways Infrastructure (4)
Question to the Mayor of London from Navin Shah (21-May-2015):

Following your discussions, will Brent Cross Phase One be fully compliant with all aspects of your 2013 Roads Task Force Report, including the London Streets Family document, for streets and spaces design to accommodate active frontages, street greening, walking and cycling, inset on-street parking / loading and vehicle movement? If not, where does it fail?

Response from the Mayor of London (21-May-2015):

TfL continues to work productively with the developer and the London Borough of Barnet through the planning process to apply the principles of the Roads Task Force (RTF) to Brent Cross Phase One. This will also be the case for subsequent phases of the project.

This work includes the provision of TfL technical support to the developer and the London Borough of Barnet to

continue to improve the currently proposed highway designs, using the RTF toolbox of measures. This approach will further improve support for walking and cycling, while providing an appropriate highway network for vehicle movement and servicing activities at the expanded shopping centre.

With regards to the Mayor's responses above: Are you content that the following is accurate, including for pedestrians on the pavements of the North Circular Road, on the living bridge and at the "double roundabout" at the northern end of Tempelhof Avenue?

The responses are a matter of public record and the work referred to by TfL has been on-going and, assuming approval of this Reserved Matters Application, will continue as part of the detailed technical highways approval. The sub-phase 1A North proposals include shared facilities for pedestrians with cyclists at Staples Corner (already approved) on the North Circular Road, otherwise provision along the A406 is unchanged. At the Prince Charles Drive western roundabout a footway is proposed on the western side to provide a pedestrian connection from the south west car park plot to the wider network, and on the north eastern corner there is a footway shared with cyclists that is part of the north – south route through the site. Otherwise, the western roundabout is not intended to be for vehicular traffic and has been designed accordingly. The Living Bridge is proposed to be for both pedestrians and cyclists.

Major Brent Cross Roads (1)

Question to the Mayor of London from Navin Shah (21-May-2015):

You will be aware that on 29 April 2015, the government was ordered by the Supreme Court to take immediate action over its obligations under European law on air pollution limits. What action have you taken to ensure that proposed changes to all major Brent Cross Phase One roads help in the regard?

Response from the Mayor (21-May-2015):

The Brent Cross Cricklewood regeneration will create a new town centre, encouraging modal shift from highway to public transport. There will be good pedestrian and cycle access throughout the area and improved links to public transport services at Brent Cross Underground

station, the new main line station, the expanded bus station in the shopping centre and enhanced bus services. Under the terms of the planning consent, the developer is required to transform the road layout to provide more efficient movements, which will prevent traffic queues and reduce pollution. The high levels of walking, cycling and public transport use, together with the changes to the highway network and London-wide measures such as the Low and Ultra Low Emission Zones, will minimise the contribution of traffic in the area to air pollution. This work is on-going.

Brent Cross contract (2)
Question to the Mayor of London from Darren Johnson (25-Mar-2015):

Who will have ownership and responsibility for the maintenance and repair of the Living Bridge which is being built as part of the Brent Cross development?

Response from the Mayor (25-Mar-2015):
The final ownership of the Living Bridge remains under discussion. As with any other third party structure over the TfL road network, there will be effective measures in place to ensure that the bridge will be properly maintained to protect road users' safety and the standards required by TfL on its road network.

With regards to Majors comments above, who will own the living bridge?

Major Brent Cross Roads (3)
Question to the Mayor of London from Navin Shah (21-May-2015):

The ownership of the Living Bridge has yet to be agreed. It will be owned by LBB, subject to commuted sums to cover future envisaged maintenance costs being agreed with the Developers. It should be noted that for the Reserved Matters Application ownership is not a material consideration.

Are you satisfied that technical expertise exists in UK engineering companies to build all the planned highway bridges over the North Circular Road in-situ, rather than elsewhere and then pushing them across? Are you content with building them all in-situ?

If not, what are your technical reasons to favour building one or more bridge to the south of the North Circular Road, rather than to the north? In choosing between the south and the north for a bridge-building site, what effect would each have on the Brent Cross Phase One number of London homes?

Response from the Mayor (21-May-2015):

Brent Cross Phase One, which includes the highways infrastructure, is still at the concept design stage. The delivery strategy and construction methodologies are currently being prepared by the developer for agreement with the highway authorities.

In developing this project, TfL will ensure that the construction methodologies used to construct new bridges are safe, minimise the impact on traffic, local residents and businesses, and, comply with the Traffic Management Act and New Roads and Street Works Act.

No loss of homes is anticipated as a result of temporary bridge-building sites.

With regards to the Mayors comments above: "No loss of homes is anticipated as a result of temporary bridge-building sites?" And Does that mean that the bridges will not be built on ex-housing land, south of the North Circular Road?

There are no temporary road bridges proposed in this RMA. The Whitefield Estate is within the red line boundary of the 2014 S73 Consent for the BXC development. The Living Bridge, including the southern approach ramp, is a permanent bridge structure and will require land from the Whitfield Estate. The Living Bridge will be delivered in Phase 1A (North). A separate RMA for the Brent Terrace Triangles in relation to Plots 53 and 54 was approved earlier this year and it is intended that the accommodation provided through this application will provide accommodation for the Whitefield Estate Replacement Units (Part 1) displaced within Phase 1a (North) of the development. Different housing solutions will be required for Council tenants and residents who are long leaseholders or freeholders. Planning Condition 1.10

<p><u>A41 segregated cycling</u> Question to the Mayor of London from Andrew Dismore (15-Jul-2015): <i>What progress is there concerning a segregated cycle lane for the A41 between Swiss Cottage and Hendon?</i> Response from the Mayor (15-Jul-2015): <i>TfL is working on designs for Cycle Superhighway 11, which is proposed to run from Brent Cross to the West End, via the A41 and Swiss Cottage. Proposals are currently at feasibility stage with a range of options under consideration in collaboration with local stakeholders. A public consultation is planned to take place later this year.</i></p> <p><u>Brent Cross - through routes</u> Question to the Mayor of London from Darren Johnson (11-Jun-2014): <i>What through routes at the northern end of the proposed 'living bridge' do you suggest are available for cyclists use, rather than merely being able to get to and from the Brent Cross shopping centre?</i> Response from the Mayor (11-Jun-2014):</p>	<p>(Residential Relocation Strategy) is currently under consideration and will set out the options in more detail. The aim of this Condition is to ensure the satisfactory relocation of Whitefield residents. The Council has appointed an Independent Residents Advisor for the Whitefield Estate and has set up a Steering Group of representatives of the various tenure groups on the estate.</p> <p>The connection to the proposed Cycle Superhighway 11 is where the Clitterhouse Playing Field pedestrian and cycle network links with the A41, adjacent to Ridge Hill. As set out in the approved sub-phase 1A North Pedestrian and Cycle Strategy there is then a (virtually all) segregated route provided to the base of the Living Bridge ramp on Claremont Avenue, and the Living Bridge itself is a shared use facility.</p>
--	---

Please see my response to [\[the following question\]](#).

Brent Cross - 'living bridge' and cyclists
Question to the Mayor of London from Darren Johnson
(19-Mar-2014)

What routes at the northern end of the proposed 'living bridge' do you suggest that cyclists can use to get to and from the Brent Cross shopping centre?

Response from the Mayor (19-Mar-2014)

The indicative plans show that the Living Bridge proposed by the developers will be accessible to cyclists at the southern end along the bridge approach ramp leading up from the Market Square. Cycling will then be permitted on the Living Bridge as far as its northern side where 40 cycle parking racks are proposed. From there onwards, cyclists could use the proposed two-way shared 3.5m wide cycle and pedestrian route along the northern side of the diverted river Brent. This east-west route would connect to the Brent Park Road (extending under the M1 motorway) to the west and under the A41 flyover in the east to link with Prince Charles Drive and with TfL's proposed cycle superhighway route 11 along Hendon Way.

The general parameters for the development have been fixed as part of the outline planning permission recently granted by Barnet Council. TfL and the GLA are actively working with both the Council and the developer to maximise opportunities for walking and cycling in the detailed design, with a particular focus on links between Brent Cross and the wider area.

With regards to the Majors Response above what happens at the north end of the living bridge?

See detailed response above providing clarification on the latest position with connections for pedestrians and cyclists at the northern end of the Living Bridge. (It should be noted that the current proposals are for 19 cycle parking racks at the northern end of the Living Bridge and 8 at the southern end (although these can be

<p><u>Brent Cross - new access roads</u> <u>Question to the Mayor of London from Darren Johnson (19-Mar-2014):</u> <i>Do you support the building of certain new minor access roads under Phase One of the Brent Cross development, to allow for the expansion of the shopping centre there? Will they have proper segregated cycle lanes, particularly given the expected lack of safe through-routes for casual cyclists through the site?</i></p> <p><u>Response from the Mayor (19-Mar-2014):</u> <i>The road layouts in Phase One of the Brent Cross Cricklewood development include changes to the local road network to improve access into the new shopping centre.</i> <i>The general parameters for the development have been fixed as part of the outline planning permission recently granted by Barnet Council. TfL and the GLA are actively working with both the Council and the development partners to maximise opportunities for safe walking and cycling in the detailed design, with a particular focus on links between Brent Cross and the wider area.</i> <i>My response ... [above] gives more detail on proposed through routes for cyclists.</i></p> <p><u>With regards to the mayors response above, how many of the yet-to-be-built roads will not have segregated cycle paths?</u></p>	<p>increased if insufficient), and that the proposed shared route along the river corridor is 6m wide. The connection to the proposed cycle superhighway is explained above and the connection at the western end of the river corridor is along the side of the M1 retaining wall.)</p> <p>Plan BXCR-URS-ZZ-11-CE-00002 revision P06, as contained within the approved sub-phase 1A North Pedestrian and Cycle Strategy clearly indicates the planned cycle facilities, and clarifies which facilities are shared, which are segregated, where cycle parking facilities are being provided etc.</p>
--	--

<p><u>Resident 10 Response:</u> <u>Living Bridge:</u> In reference to Appendix M of the Phase 1 Transport Report, the Living Bridge has no cycle access from the North end of the Bridge. Why do Cyclists need to dismount in the middle of their journey? The Living Bridge is wide enough to provide a segregated route for pedestrians and cyclist.</p> <p>The developers have only addressed access points to the shopping Centre and have not considered easy access around the shopping centre.</p> <p>What is the purpose of the Living Bridge? And how is it useable green space when it spans across on one of the most polluted roads. People are unlikely to use this space.</p> <p><u>Infrastructure:</u> The cycle and pedestrian route across the Tempelhof Bridge is segregated but there is a complex path when travelling to the east; is this path necessary?</p> <p>What do the pink paths on the submitted plans for the RMA demonstrate?</p> <p>How wide are the shared paths around Staples Corner and why are these paths not segregated?</p> <p>Currently cyclists can cycle over the bridge that forms the underpass for the A406 & A41; however, once the development is complete this will not be possible. This is a simple route for cyclist traveling from the North East.</p>	<p>The Proposed Living Bridge provides a connection to the shopping centre which will be the start or end point to any journey. As explained at the start of the response to resident 9 above, cyclists will be able to continue their journey via the river corridor but if making a trip through the area the route will be signed via Tempelhof Bridge and the proposed facilities to the western side of the shopping centre. The proposed layout of the Living Bridge is not configured for segregated provision as it is intended to be a shared space scheme. Please see the main report for further discussion on the Living Bridge. There is provision for pedestrian access through the shopping centre, as well as around the outside and details of the shopping centre proposals themselves are planned as part of sub-phase 1B North.</p>
	<p>It is unclear which 'complex path' is being referred to, but once over Tempelhof bridge anyone wishing to walk or cycle to the east can do so via a nearby ramp that gives direct access to the river corridor, and then onwards via the A41 junction.</p>
	<p>The pink paths represent shared pedestrian and cycle routes.</p>
	<p>The off road shared paths are 4m wide across the new bridge (B6) and between 2.3m – 3.5m on the approaches from the A5 north and south and Brent Terrace North. The facilities were not required to be segregated when the scheme was designed and submitted, and then approved by the planning authority.</p>
	<p>There is no change to provision for cyclists across the A406 at the A41 junction, where formally cyclists are required to dismount. The modified road layout and structures at the junction include a shared use facility as part of the A41 underpass for cyclists travelling to / from the east or north east. A new cycle connection is proposed via</p>

<p>Why has this route been closed and not widened or developed?</p> <p>Footpaths and cycle routes do not meet at the north or south end of the shopping centre or to the south east side. This will result pedestrians walking on the road.</p> <p>The cycle superhighway stops in the middle of the A41 just south of the A406 Flyover; this route is unconnected to any BXC cycle route.</p> <p>With regards to consultation, not all residents received the notification. Two Reserved Matters Planning applications have been in public consultation, ending 5th and 7th August. Residents have previously requested that two planning applications should not be out to consultation simultaneously. There are several documents submitted and difficult to review.</p>	<p>Cooper Road to provide a link to the wider area via Shirehall Lane. There is also an improved connection to Brentfield Gardens as the A406 westbound off slip is closed and a new off road shared footway – cycleway will be provided. A new shared footway – cycleway is also provided on the western side connecting with Tilling Road. General refurbishment (lighting / signing / painting as appropriate) is also planned for the A406/A41 pedestrian and cycle links.</p> <p>There are a combination of footpaths and cycleways that provide direct connections to and from the shopping centre on its north, south and southeast sides. Moreover, north – south connections are made at the western side of the shopping centre. No pedestrians will have any reason to walk in a road.</p> <p>The proposals are for the A41 cycle superhighway to connect to BXC via the cycle route that extends from the A41 near Ridge Road to Tempelhof Bridge and beyond via Clitterhouse Playing Fields, Claremont Avenue, Market Square and New Tempelhof Avenue.</p> <p>Please see the main report for details of the number of residents consulted and re consulted. Consultation time periods comply with the statutory requirements and the consultation expiry dates are also available on the Councils website. The Central Brent Riverside Park RMA (15/03315/RMA) has been developed alongside the Infrastructure RMA (15/03312/RMA) and is part of Phase 1A North. Both RMA submissions were subsequently submitted and designed together. The planning case officer is available (on the telephone) to discuss and explain the planning application and documentation submitted.</p>
---	--

Statutory Consultees and other interest groups Consultation Responses

<p><u>Transport for London</u> Letter dated: 1st September 2015</p> <p>1. The planting of trees within TfL highway are subject to agreement with TfL and have provided specific advice direct to the developer, and will work the Council and developer to deliver trees and landscape improvements.</p> <p>2. TfL is responsible for bus stops that are altered by the proposed development. TfL will work with the developers and Council on the detail design and work to agree fixed position for the stops with reference to TfL’s published guidance. TfL recommend that discussions regarding these aspects are discussed during the Transport Advisory Group (TAG); as they involve all local authorities.</p> <p>3. The Estate Management Framework has been updated to clarify any interfaces with TfL highway and infrastructure; including day-to-day arrangements to assist with compliance with any legal agreements, leases or licenses that TfL maybe party.</p> <p>4. With regards to the temporary bus restrictions, TfL should have unrestricted access to the existing bus station or replacement facility at all times for buses and passengers including step free access from the shopping centre over a reasonable walk distance with appropriate security and safety measures.</p> <p>5. TfL understands the reason for restricted hours on the temporary bus stops on Plot 113 having visited the site with your Environmental Health officer on 19th August 2015 and welcomes the review mechanism. These</p>	<p>Noted</p> <p>Noted</p> <p>Noted</p> <p>Noted</p>
---	---

<p>restriction pose significant operational difficulties to TfL and potential inconvenience to our passengers – however, will work with the applicant to comply with this condition and expect them to help mitigate the impact on our passengers and operators including by providing facilities, pedestrian routes and where necessary additional bus subsidy during construction</p> <p>6. TfL favours a straight move to the new bus station location rather than posing a temporary bus station and bus stops on Plot 113; subject to the developer being able to create safe and accessible routes for buses and passengers during construction of the shopping centre. The developers have indicated that as part of their Phase 1B application they would consider options for a straight move to the new permanent bus station. TfL requests that the developers engage with them in formal pre-application discussions in relation to Phase 1B.</p> <p>Any temporary or permanent bus station must meet TfL’s operational needs and those of passengers.</p> <p>7. TfL has proposed draft conditions to define more precisely the relocation of the taxi rank during construction phase.</p> <p>Temporary Taxi facilities <i>Prior to any works to the designated Taxi rank on Prince Charles Drive adjacent to Brent Cross Bus station or any works that restrict access by the Taxi or passengers to this taxi rank provide details of alternative facility that is fully accessible for wheelchair users, sufficient capacity for at least 9 cabs to rank, and is clearly visible to customers exiting the shopping centre and appropriate sign posted within the shopping centre itself. The details of these</i></p>	<p>Noted. Permanent bus station location will be discussed part of Phase 1B.</p>
---	--

<p><i>facilities should be agreed with TfL and submitted to the LPA for approval, and the facilities operated in accord with the approved plans.</i></p> <p><i>Reason: To ensure the shopping centre and nearby facilities are full accessible by Taxi during construction phases</i></p>	
<p><u>Greater London Authority</u></p> <p>No objection Raised - The Mayor supports TfL in overseeing the transport aspects of the Brent Cross Cricklewood project and ensuring the scheme remains in accord with the London Plan and Mayor's Transport Strategy at each phase of sub-phase of the development.</p>	<p>Noted</p>
<p><u>Consultative Access Forum (CAF):</u></p> <p>Letter dated: 28th August 2015:</p> <p><u>Tempelhof Bridge ramp leading the River Corridor:</u></p> <p>Inserting a landing at the midpoint of each slope to provide space will improve the usability of the ramp. However, it is not possible from the drawings provided to assess the gradients and the handrails which are critical aspects of ramps.</p> <p><u>Lift to the Riverside Park:</u></p> <p>A lift from the Living Bridge/ Bus station level to access the Riverside Park would enhance access from the Lower Riverside Walkway, and benefit those who require step free access.</p>	<p>Officers response:</p> <p>The gradient for the ramp from Tempelhof Bridge ramp leading to the Riverside Park is 1 in 21 with a short 'dog leg' landing. Details and specifications regarding handrails and other details aspects will be considered at the detailed design stage.</p> <p>Noted.</p>
<p><u>Environmental Agency</u></p> <p>Letter dated: 25th August 2015</p> <p>The Environmental Agency reviewed both Reserved Mattered Applications from the perspectives of potential</p>	<p>A financial contribution of £200,000 towards naturalisation works to an area of Mutton Brook upstream of the application site to compensate for the loss of 1217m2 to The Brent River Corridor has been agreed with the Developers and will be secured through a Deed of Variation to the existing Section 106 agreement attached to the</p>

<p>impacts on the ecological environment, the Water Framework Directive (WFD), flood risks and the risks of surface water pollution. It was considered that the current proposal results in a reduction in ecological buffer zone of 1217m² compared to the proposals approved under the Section 73 application.</p> <p><u>Flood Risk:</u></p> <p>The EA considered that the flood modeled will not cause any unacceptable increases in flood risk. However, noted that the modeling demonstrates high sensitivity to channel roughness ,and if future design changes are made this would impact the modeled channel roughness; to prevent any potential flood risk they have proposed the following condition:</p> <p>Condition 1</p> <p><i>The river channel will be constructed in such a way that the roughness values used in the current flood model are representative of the physical channel. Any changes to channel attributes that would make the current modeled roughness values unrepresentative will not be permitted without the prior written permission of the local planning authority.</i></p> <p>Reason</p> <p><i>To prevent increased flood risk</i></p> <p>At the detailed design stage, the EA have requested that the development should demonstrate that assets/ street furniture are fixed appropriately to the ground or walls; to prevent entering into the river during a flood event. To minimise this they have requested the following condition:</p> <p>Condition 2</p>	<p>2014 Section 73 Consent as per the recommendations at the beginning of this report.</p> <p>Under the terms of the 'Necessary Consents' the Developers are already required to obtain approval from the EA for Flood Defense consent which covers the issues described. An appropriately worded informative is proposed to ensure that the Developers are aware of this requirement.</p>
---	--

<p><i>Prior to the installation of bins, benches and bird boxes, full details of the design of these items will be submitted to and approved by the local planning authority.</i></p> <p>Reason <i>To minimise blockages and pollution of the river during a flood event.</i></p> <p><u>Surface Water Pollution:</u> The Environmental Agency is satisfied with the evidence provided to allow natural continuity between the surface water in the new river channel and groundwater. However, had concerns that if the contaminated groundwater is not managed correctly this could enter the River system and effect the surface water; therefore proposed the following condition to request a detailed method statement to assess environmental safeguards.</p> <p>Condition 3 <i>The development hereby permitted shall not be commenced until such time as a detailed construction method statement has been submitted to, and approved in writing by, the local planning authority. The construction method statement will demonstrate how pollution to surface water will be prevented.</i></p> <p>Reason <i>To prevent pollution of surface water</i></p>	<p>Noted. Condition included in Appendix 1 of this report.</p> <p>Noted. Condition included in Appendix 1 of this report.</p>
<p>Brent Council Letter dated: 18th August 2015 Whilst Brent Council have raised considerable concerns with regards to the A5 Corridor Study (14/07402/CON) and the Area Wide Walking and Cycling Study (14/08105/CON); no objections have been raised to the approval of this Reserved Matters Application.</p>	<p>Noted</p>

<p><u>Thames Water</u> Letter dated: 2nd July 2015 No objections raised and does not affect Thames Water</p>	<p>Noted</p>
<p><u>London Borough of Harrow</u> Letter dated: 28th July 2015 No objection raised</p>	<p>Noted</p>
<p><u>Natural England</u> Email dated: 8th July 2015 No objection raised</p>	<p>Noted</p>
<p><u>Network Rail</u> Email dated: 12th August 2015 Network Rail is supportive of the proposed works but note is essential that operational railways and assets are protected.</p>	<p>Noted</p>
<p><u>Hertsmere Borough Council</u> Email dated: 6th August 2015 No objection raised</p>	<p>Noted</p>
<p><u>Sport England</u> Email dated: 24th July 2015 No objection raised</p>	<p>Noted</p>
<p><u>London Fire and Emergency Planning Authority</u> Letter dated: 14th August 2015 Satisfied with the proposal.</p>	<p>Noted</p>
<p><u>North London Waste Authority</u> No objection Raised with regards to this reserved matters application; however, noted proposals submitted part of the Section 73 planning application to build flats on the site of Hendon Rail Transfer (HRF) and would continue to be consulted.</p>	<p>Noted</p>

Appendix 7

Conformity with Revised Development Specification and Framework

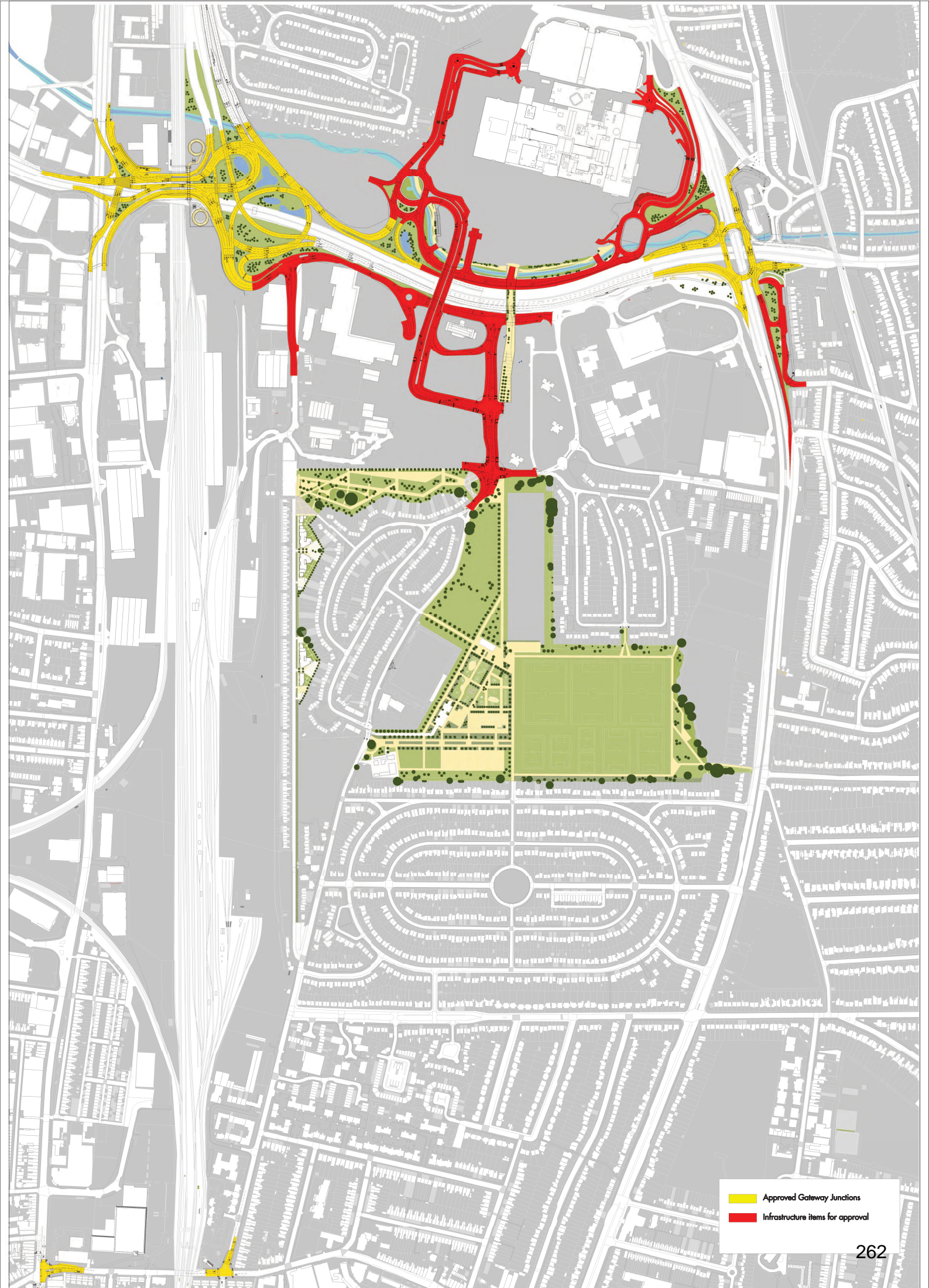
Revised Design Specification and Framework (RDSF) and Parameter Plans (PP)	Requirement relevant to Infrastructure Reserved Matters	Compliance
<p>Parameter Plan 002: Transport Infrastructure</p>	<p>PP2 identifies various infrastructure elements that are proposed as part of the site's comprehensive development including new highway junctions, primary routes and transport interchanges.</p> <p>More specifically the PP defines the parameters for road bridge clearance heights and the length of bridges.</p> <p>The general location of proposed road and pedestrian bridges is defined, including access points from the proposed Shopping Centre to the riverside walkway. Principal road bridges include Templehof Interchange/A406 Templehof Bridge, the western roundabout, and the eastern roundabout.</p> <p>The Living Bridge was included in the s73 permission PP002 and has been brought forward as a Phase 1A(North) element of critical infrastructure. The Living Bridge provides a pedestrian and managed cycle route from the southern development area into the Shopping Centre.</p> <p>The new Bus Station is proposed as one of the Transport Interchanges.</p> <p>Underpass U3, under the</p>	<p>The proposed development is generally in accordance with the parameter plan. A through pedestrian route is shown running west to east through Riverside Park.</p> <p>The principal western and eastern roundabouts provide access to the proposed Shopping Centre. The location of the road and pedestrian bridges is shown to generally accord with the PP. Changes to the PP through the separate application under Conditions 2.5 and 2.5 to vary the approved parameters ensure that the proposed development is compliant with regard to bridge clearance and bridge length as well as the location of a number of road bridges where these have deviated from the original scheme.</p> <p>The location and width of the Living Bridge is in compliance with the Parameter Plan.</p> <p>The current proposals do not provide details of the new Bus Station as this will be subject of a future planning application. The proposed development programme however necessitates the provision of a temporary bus station to address access issues during construction to the</p>

	<p>A41 Hendon Way and BXC is to be upgraded.</p>	<p>existing and proposed bus stations. The temporary bus station is addressed in the current application. The environmental impacts of the temporary bus station are assessed in the ES FIR.</p> <p>No details of the proposed upgrading have been provided although this can be addressed by condition.</p>
<p>Parameter Plan 003 Public Realm and Urban Structure</p>	<p>The PP identifies a network of existing and proposed open spaces.</p> <p>The general location of Nature parks 4 and 5 are shown on the PP and their respective area of NP4 is defined as 0.2ha</p> <p>The Living Bridge is included at City Garden 3 and its location defined.</p> <p>The new River Brent Corridor is defined as RBC and its area defined as 3.1ha.</p> <p>The development zones for the proposed road and pedestrian crossings are included in the PP.</p> <p>An 'optional' acoustic barrier location is shown.</p>	<p>The indicative locations of the Nature Parks are shown in the submitted plans however the details for each are not included in this RMA. Detail of NP 5 is included in the CBRP RMA, 15/03312/RM.</p> <p>Living Bridge is provided in accordance with the PP.</p> <p>With regard to the new Riverside Park, changes introduced through Condition 2.4/2.5 reduce the park area to 3.0ha thereby ensuring that the development currently proposed is compliant with the PP.</p> <p>The road and pedestrian bridge locations are compliant with the revised PP.</p> <p>The provision of an acoustic barrier has been confirmed as a detailed requirement of any planning permission, having been assessed under the ES FIR as mitigation to the proposed development in relation to the amenity of users of the Central Reach of CBRP. Whereas the barrier has been considered under the ES, and a number of options explored before a preferred option being</p>

		confirmed, no details are submitted with the current application. This is to be addressed by way of planning conditions.
Parameter Plan 011: River Brent	<p>PP 011 relates specifically to the diversion of the River Brent and associated works and defines the proposed river corridor and the indicative river bed.</p> <p>The river corridor is dealt with in three separate distinct reaches, Reach 1 and 2 relating to the retail development and reach 3 providing a more natural park environment.</p> <p>The PP includes an indicative river realignment plan.</p>	<p>The proposed river corridor is consistent with the PP.</p> <p>The development proposals identify the three reaches however only the provision of Reach 2 is provided in detail and that is dealt with under the separate application 15/03315/RMA for CBRP.</p> <p>The river realignment plan has been updated through changes made under Conditions 2.4 and 2.5 to accommodate design changes to the roads network.</p> <p>Although the river corridor has reduced in width as a result of widening of the realigned Prince Charles Driver (to meet Highways requirements) the river corridor is still compliant.</p> <p>The anticipated landscaping requirements within the PP are adhered to within the relevant RMA submission.</p>

This page is intentionally left blank

APPENDIX 8 – GATEWAY JUNCTIONS APPROVED UNDER THE SECTION 73
PLANNING APPROVAL



■ Approved Gateway Junctions
■ Infrastructure items for approval

Appendix 9

Red Line BXC Phase 1AN



This page is intentionally left blank

LOCATION: Central Brent Riverside Park, Brent Cross Cricklewood, London, NW2

AGENDA ITEM 9

REFERENCE: 15/03315/RMA

Received: 29 May, 2015

Accepted: 2 June, 2015

WARD: Golders Green

Expiry: 24 July, 2015

APPLICANT: Brent Cross Development Partners

PROPOSAL: Reserved Matters application within Phase 1A (North) of the Brent Cross Cricklewood Regeneration area, submitted pursuant to conditions 1.2.1A, and 2.1 and for the part discharge of condition 13.1 of Planning Permission F/04687/13 dated 23 July 2014 for the comprehensive mixed use redevelopment of Brent Cross Cricklewood, relating to Layout, Scale, Appearance, Access and Landscaping for the Open Space of the Central Brent Riverside Park. Application is accompanied by an Environmental Statement Further Information Report.

RECOMMENDATION

Resolution to approve subject to:

Part 1:

The completion of a satisfactory Deed of Variation to make the necessary amendments the existing Section 106 Agreement dated 22nd July 2014 attached to planning permission F/04687/13, to secure the following:

- 1) Amendment to introductory text to Schedule 1 to allow the minor amendment of defined terms in Schedule 1 of the S106 agreement to be agreed in writing between the Brent Cross Partners, CRL, the LPA and / or TFL where applicable;
- 2) Amendment to Definitions to accord with submissions against agreed definition changes under section 96A applications and changes resulting from conditions 2.4 and 2.5 and in accordance with approvals of Phase 1A (North) Reserved Matters Applications; and
- 3) A contribution of £200,000 (Index Linked) for naturalisation works to an area of Mutton Brook upstream of the application site to compensate for the loss of 1217m² to The Brent River Corridor.

Part 2:

That upon completion of the Deed of Variation specified in Part 1 of the recommendation above, the Assistant Director of Development Management and Building Control approve reserved matters application reference

15/03312/RMA under delegated powers subject to the conditions set out in Appendix 1 to this report and any changes to their wording and or deleting and or adding conditions as considered necessary by the Assistant Director for Development Management and Building Control.

1. APPLICATION SUMMARY

The proposal is a reserved matters application for the provision of enhanced open space and biodiversity associated with the Central Brent Riverside Park that will form the setting of the proposed diverted River Brent and the Brent Cross Shopping Centre.

The provision of enhanced open spaces is a matter reserved for further approval under the 2014 Section 73 outline planning permission (the '2014 S73 Consent') for the Brent Cross Cricklewood Regeneration Scheme, granted in July 2014. The proposed enhancements associated with the diverted River Brent will be provided in three distinct phases. The current phase within this RMA relates to the Central Brent reach of the river, 'Reach 2'. Details of the adjoining Reaches 1 and 3 will be subject to a future Phase RMA.

The central reach of the riverside park will be provided as part of Phase 1A (North). The provision of landscaping and biodiversity details under this phase is appropriate given the close linkage between the diversion of the River Brent, the provision of new highways infrastructure and the desire to provide a completed scheme to complement these works. The infrastructure requirements of the river diversion, a realigned Prince Charles Drive and the provision of the 'Living Bridge' have to be provided prior to the completion of the proposed landscaping, public access and ecology enhancements proposed within Riverside Park. The infrastructure works will be delivered under the separate reserved matters application reference 15/03312/RMA which is also reported to this Committee.

The open space RMA application includes indicative landscaping details for Reaches 1 and 3 that adjoin the planning application site. The submission of these indicative landscaping proposals demonstrates the wider landscape context of Riverside Park and how Reach 2 will sit within this context. The landscaping details for the adjoining Reaches are however indicative only and cannot be approved with this current application. Full details of the remainder of Riverside Park outwith Reach 2 will be delivered in subsequent phased RMAs.

The Riverside Park will form a new east-west cycle and pedestrian route creating new links to existing and proposed residential areas as well as providing an enhanced amenity space for visitors to the extended Brent Cross Shopping Centre. Riverside Park, once completed will also connect into the proposed sequence of open spaces lining different green spaces throughout

Brent Cross, primarily by way of access to/from the Living Bridge which connects south via Market Square to Clitterhouse Playing Fields and Claremont Park.

The application was submitted in June 2015 and is one of a suite of applications for Phase 1A (North) of the Brent Cross Cricklewood Regeneration Scheme. Four separate RMAs have been submitted covering the following matters:

- Development Plots 53 and 54, Brent Terrace, (reserved matters granted 9th June, 2015);
- Clitterhouse Playing Fields and Claremont Park Opens Spaces (reserved matters consent granted 10th July, 2015);
- Central Brent Riverside Park Open Space (submitted June 2015); and
- Infrastructure (submitted June 2015 and also before Members at this Committee).

Planning permission was granted by the Council on 9th June 2015 for Plots 53 and 54 for the redevelopment of Brent Terrace to provide housing under application reference 15/00720/RMA and similarly, planning permission has been granted for the provision of enhanced open spaces at Clitterhouse Playing Fields and Claremont Park under application reference 15/00769/RMA on the 25th June, 2015. The current application was submitted along with a reserved matters application relating to the proposed Infrastructure, which includes the provision of the Living Bridge and the diverted River Brent, the principal highways' infrastructure works and the provision of a temporary bus station.

The principle works in the RMA before Members comprise the provision of a combined Environment Agency maintenance access, footpath and cyclepath access along the diverted river corridor, detailed landscaping proposals and ecology and habitat improvements.

A separate application, which is also in front of Members at this Committee, has been made against conditions 2.4 and 2.5 of the Section 73 Consent (F/04687/13) to make changes to the content of the approved DSF, DAS and DG and Parameter Plan 011: River Brent. The changes sought under application reference 15/05040/CON primarily relate to the Infrastructure requirements however the following matters specifically fall under the Central Brent Riverside Park proposals:

- Reduction in the proposed area of Riverside Park from 3.1ha to 3.07ha due to an increase in the width of Prince Charles Drive and reduction in the width of Reach 2, with a marginal reduction within Reach 1;
- Reduction in the proposed area of Nature Park 5 adjacent to the Riverside Park from 0.2ha to 0.13ha to accommodate a change in design of the western roundabout to include a signalised roadway through the centre. The associated embankment structure cuts

through the area defined as Nature Park NP5 as shown on Parameter Plan 003; and

- In order to compensate for the loss of 0.07ha from NP5 it is proposed to increase the area of Nature Park NP4 within the western section of the River Brent from 0.2ha to 0.27ha.
- Change in the size definition of Nature Parks within the Open Space Hierarchy from 0.2-0.5ha to 0.1-0.5ha to reflect these proposals.

The applicants have engaged with the Environment Agency to agree a financial contribution of £200,000 that will be secured via the section 106 agreement to mitigate for the loss of habitat area within the diverted River Brent channel. This payment will be used towards improving biodiversity and ecology habitats off-site but within the London Borough of Barnet.

The reduction in the area of Nature Park 5 is addressed in the Environmental Statement: Further Information Report that supports the RMAs and which proposes mitigation due to the reduced habitat area by a like for like addition to the extent of Nature Park 4 which is located to the west of the development scheme.

In addition, a separate application has been submitted under s96A in respect of non-material amendments to the Section 73 Consent. The amendments comprise consequential changes to some definitions. The s96A application relates to non-material changes and can be determined under powers delegated to officers.

The application is for approval of detailed matters reserved under the 2014 outline planning permission. The proposals are generally consistent with the earlier planning approval and accord with Development Plan and national planning policy and guidance.

This submission provides details of the layout, scale, appearance, access and landscaping of the proposed enhanced open space. The proposals are in accordance with all the parameters and principles (as may be revised under application 15/05040/CON) relevant to specified open space requirements.

2. BACKGROUND TO THE BRENT CROSS CRICKLEWOOD REGENERATION

2.1 Outline Consent

The principle of development at Brent Cross Cricklewood was first established by way of a site-specific Development Framework produced in April 2004 as Supplementary Planning Guidance (SPG) in accordance with the London Plan. The SPG established a vision to *'to create a new gateway for London and a vibrant urban area for Barnet'*.

The comprehensive redevelopment of the wider Brent Cross Cricklewood regeneration area was subsequently granted planning permission in outline in 2010 under planning permission C/17559/08 (the 2010 permission). Subsequently, this permission was revised under a Section 73 Planning application (F/04687/13) which was approved on 23 July 2014 (2014 Section 73 Consent') described below:

Section 73 Planning application to develop land without complying with the conditions attached to Planning Permission Ref C/17559/08, granted on 28 October 2010 ('the 2010 Permission'), for development as described below: Comprehensive mixed use redevelopment of the Brent Cross Cricklewood Regeneration Area comprising residential uses (Use Class C2, C3 and student/special needs/sheltered housing), a full range of town centre uses including Use Classes A1 - A5, offices, industrial and other business uses within Use Classes B1 - B8, leisure uses, rail based freight facilities, waste handling facility and treatment technology, petrol filling station, hotel and conference facilities, community, health and education facilities, private hospital, open space and public realm, landscaping and recreation facilities, new rail and bus stations, vehicular and pedestrian bridges, underground and multi-storey parking, works to the River Brent and Clitterhouse Stream and associated infrastructure, demolition and alterations of existing building structures, CHP/CCHP, relocated electricity substation, free standing or building mounted wind turbines, alterations to existing railway including Cricklewood railway track and station and Brent Cross London Underground station, creation of new strategic accesses and internal road layout, at grade or underground conveyor from waste handling facility to CHP/CCHP, infrastructure and associated facilities together with any required temporary works or structures and associated utilities/services required by the Development (Outline Application).

Both the 2010 and 2014 permissions were subject to Environmental Impact Assessment.

2.2 Phasing of the BXC Regeneration Scheme

The 2014 S73 Consent is a multi-phase scheme for delivery over a period of 16 years. The permission proposes the phased delivery of acceptable comprehensive development for the whole site in accordance with the planning policy.

Phase 1 is proposed to be delivered in sub phases which are divided between north and south. This is to reflect the new delivery responsibilities including the Council's intention to procure a new development partner for the area south of the A406. The sub phases are as follows:

- Phase 1A (North) – this includes all the highways infrastructure to support the northern development including the key highways infrastructure to support the Phase 1 South, such as the improvements

to the southern junctions of the A5/A407 Cricklewood Lane and the A407 Cricklewood Lane/Claremont Road Junction improvements. In addition the River Brent re-routeing and Bridge works will be delivered as part of Phase 1A (North), along with the Clitterhouse Playing Fields Part 1 (excluding the Nature Park) and the Claremont Park Improvements. The Living Bridge is included in (and its details will be approved before the commencement of) Phase 1A (North). Under the Revised Section 106 Agreement, its delivery will be triggered by the commencement of Phase 1B (North) and its delivery will be programmed to commence and be completed no later than before the occupation of Phase 1B North plots.

- Phase 1A (South) – A number of highway improvements needed to support Phase 1 of the Southern Development will be provided including the Waste Handling Facility (Diverted Geron Way/A5 junction; Claremont Park Road (Part 1); School Lane Works.
- Phase 1B (North) – This includes all of the plot development on the north side with the exception of the residential development within the Brent Cross West Zone. The sub phase also includes the new bus station, reconfigured shopping centre, Brent Cross Main Square, High Street North and other northern pedestrian routes, as well as the Riverside Park, Sturgess Park Improvements and around 300 housing units. Commencement of this Sub-Phase will trigger the BXP's obligations to deliver the Living Bridge which will link into the buildings and public realm to be provided on the Plots forming part of this Sub-Phase.
- Phase 1B (South) – This includes the Market Square, the Clarefield Park Temporary Replacement Open Space the replacement food store, the Waste Handling facility, the CHP and the new and expanded Claremont School, in addition to more than 1000 residential units.
- Phase 1C – This will include the remaining plot development on the south side.

2.3 Phase 1A (North)

This application is one part of the Phase 1A (North) Reserved Matters applications.

Phase 1A (North) is largely an infrastructure phase. It includes necessary highways infrastructure to support the northern development as well as improvements to critical southern junctions including A5/A407 Cricklewood Lane and the A407 Cricklewood Lane/Claremont Road Junction improvements. The infrastructure required relevant to the River Brent re-routeing and Bridge works are also delivered as part of Phase 1A (North), along with the Living Bridge, Replacement Templehof Bridge, Clitterhouse

Playing Fields Part 1 (excluding the Nature Park) and the Claremont Park Improvements.

The reserved matters for Phase 1A (North) have been broken down into four separate reserved matters submissions due to the size, scale and complexity of this initial sub phase of the Brent Cross Cricklewood Regeneration scheme.

Four Reserved Matters Applications were submitted to London Borough of Barnet for Phase 1A (North) in January, 2015. As a result however of discussions with the Development Partners, it was agreed that the Infrastructure and Central Brent Riverside Park Reserved Matter Applications would be withdrawn prior to registration whilst their design was progressed further. These two RMAs were subsequently submitted together in June 2015.

The reserved matters that make up Phase 1A (North) are shown in the table below:

Table 1.0

No.	Summary Description	Planning Reference	Status
1.	The residential development of Plots 53 and 54.	15/00720/RMA	Approved at Committee on 18 th May, 2015.
2.	The Open Space Improvements of Clitterhouse Playing Fields (Part 1) and Claremont Park	15/00769/RMA	Approved at Committee on 25 th June, 2015.
3.	Open Space proposals for Central Brent Riverside Park	15/03315/RMA	Subject of this application
4.	Infrastructure Proposals including Roads and Junctions, Templehof and Living Bridges and diversion of the River Brent corridor.	15/03312/RMA	Submitted in June 2015 and reported to Members at this Committee.

2.4 Pre-Reserved Matters Conditions

The Section 73 permission for the Brent Cross Regeneration project (the '2014 Section 73 Consent') includes a number of Pre-Reserved Matters conditions intended to establish key principles of the forthcoming development. The majority of these require submission prior to applications for reserved matters being submitted to the Council. Reserved Matters applications are required to accord with commitments and strategies approved under these conditions where relevant.

Appendix 3 of this report shows those Section 73 Consent Pre Reserved Matters Conditions for Phase 1A (North).

3. DESCRIPTION OF THE SITE, SURROUNDINGS AND PROPOSALS

3.1 Site Description and Surroundings

The application site currently comprises car parking and associated urban form related to Brent Cross Shopping Centre. The wider site comprises a mix of different land uses broken up by major road and rail infrastructure. As part of the wider Brent Cross Cricklewood Regeneration proposals (BXC) the River Brent will be diverted from its current channelled/canalised course to create a more naturalised meandering river course. The diverted river runs east to west and will meander towards the realigned Prince Charles Drive and the A406 North Circular Road in order to maximise the developable space to accommodate the extended Brent Cross Shopping Centre before meandering back to its current course to the west and on to Welsh Harp reservoir. The proposed diversion and development works afford an opportunity to create a new riverside park along the course of the diverted river in order to create a significantly enhanced public open space. The riverside park will provide an attractive park environment as an integral element of the wider BXC works and will provide enhanced public amenity space in an area where there is currently little opportunity for public access. The riverside park will provide an improved setting to the Brent Cross Shopping Centre, make provision for recreational park space and enhance local habitat and ecology. The works will improve biodiversity in the area. The works to the River Brent are also designed to contribute to water quality within the River Brent corridor.

The river diversion is defined in three 'reaches'. The current application relates to the central reach and is bookended by an eastern reach and western reach. The eastern reach, Reach 1, extends west from A41 Hendon Way and comprises the land between the existing centre and the North Circular Road. The western reach, Reach 3, takes up the space between the North Circular and existing residential property on Brent Park Road before running under the M1 and into the Site of Special Scientific Interest (SSSI) Welsh Harp Reservoir.

The development includes the provision of a nature park, defined as Nature Park 5 which sits under elevated road infrastructure to the west of the central reach. More specifically, a new Templehof Avenue will connect into a new western roundabout to provide improved vehicular access to the Shopping Centre; Nature Park 5 sits beneath the elevated roundabout structure. An additional nature park, Nature Park 4, is located to the western end of the river diversion within Reach 3. A number of road and pedestrian bridges cross the riverside park and the detail of these are included in the separate RMA, 15/03312/RMA relating to Infrastructure and before Members at this Committee.

The Design Development Report that accompanies the planning application identifies the following general site conditions:

- Limited biodiversity
- Low ecological value
- Limited public access

- Limited amenity value
- Poor landscape character

It is intended that the proposed development will unlock the potential of the River Brent and reveal it as a major asset to the wider mixed use regeneration site, as well as becoming an enhanced asset for the public and the environment, by creating a park with:

- Improved water quality and reduced flood risk
- Significant biodiversity value and ecological habitats
- Positive amenity value as a riverside park and key cycle / pedestrian route for existing residents as well as visitors to Brent Cross Shopping Centre

3.2 Description of the Proposals

The context of the proposals contained within this RMA need first of all to be clarified.

With the granting of the 2014 S73 Consent in July 2014 the development proposals for Brent Cross Cricklewood (BXC) will be brought forward in a number of different phases over a number of years. Phase 1A(North) is the first phase of development to be progressed and it is an essentially infrastructure orientated phase that once implemented will allow subsequent phases to be brought forward.

As explained at section 3.3 and table 1.0 above, Phase 1A(N) comprises four separate RMAs covering infrastructure, enhanced open spaces provision, the delivery of residential development plots and the provision of the River Brent Riverside Park.

The Infrastructure RMA (reference 15/03312/RMA) provides details of the proposed roads infrastructure, the provision of new bridges including the river bridges and the Living Bridge that will connect the southern development zone into Brent Cross Centre and the diversion of the River Brent.

The application which is the subject of this report relates to the Central Brent Riverside Park RMA which provides details of the proposed landscaping and ecological enhancements for the Central section of the proposed Riverside Park, including public access and maintenance access provision.

The current RMA covered by this report is restricted to the central Reach (Reach 2) of the proposed River Brent diversion works as this area is directly related to the provision of the infrastructure works proposed in the Infrastructure RMA and will ensure that the works are provided in a coherent and joined-up manner. Whilst the Infrastructure RMA (15/03312/RMA also before Members at this Committee) seeks planning permission for the details of the full diversion works of the River Brent, the current open space

application seeks detailed planning permission for the associated Riverside Park only in so far as it relates to the Central Brent Riverside Park (Reach 2) of the diverted river. The details of the Eastern Brent Riverside Park (Reach 1) and Western Brent Riverside Park (Reach 3) are to be submitted for approval through the Phase 1B North RMA with the river diversion infrastructure works are therefore considered in the Infrastructure RMA.

It will be noted that the landscaping and access proposals contained in the current RMA cannot proceed in isolation from the infrastructure works and both applications must therefore be considered together.

In order to enable an understanding of the development proposals in their wider context and to understand how the open space proposals for Central Brent Riverside Park will sit within the context of similar development on either side of the central reach, details of the proposed landscaping, access and open space provision proposals for the full extent of the River Brent diversion area and details of the whole extent of the Riverside Park have been provided. The details which are outside of the central reach are however illustrative and have been submitted for information purposes only and, whilst some detail has been presented, this are regarded as illustrative material to enable assessment of the current proposals and to ensure that the Central Brent Riverside Park is not considered in total isolation. The illustrative material however is not for discussion under the current reserved matters application.

The reserved matters submission under consideration in this report therefore provides details of site layout, scale, appearance, access and landscaping in relation to the central reach of the proposed Riverside Park or more specifically, Central Brent Riverside Park, (Reach 2).

The main elements of the development proposal are:

- Landscaping and open space proposals for Central Brent Riverside Park (Reach 2);
- Bench seating on river terrace set at even intervals throughout;
- The provision of a naturalised meandering River Brent (in respect of landscaping and ecology enhancements)
- Provision of a naturalised riparian environment;
- Enhanced habitat for local flora and fauna;
- Environment Agency maintenance access;
- Inclusive access via steps and a ramp;
- The provision of a lift to connect the riverside park to the Living Bridge and Bus Station levels
- Footpath and cyclepath access, and
- Provision of Nature Park 5.

The application consequently is supported by the following documents:

- Application Covering Letter;
- Explanatory Report (June 2015);
- Environmental Statement – Further Information Report (revised and updated, dated May 2015);
- Central Brent Riverside Park Design Development Report;
- Application Detailed Plans;
- Sustainability and Energy Statement (revised May 2015)
- Statement of Community Involvement;
- Illustrative Reconciliation Plan; and
- Reserved Matters Transport Report

3.3 Relationship to Other Phase 1AN RMAs

As stated above the delivery of the open space and biodiversity proposals within this RMA are inextricably linked with the Infrastructure RMA which is also reported to members at this Committee.

The Infrastructure RMA provides the framework for the open space RMA, including setting down the route of the diverted River Brent and surrounding highway network. The proposed infrastructure comprises the upgraded highway framework that will improve access to and around Brent Cross Shopping Centre. The Brent Cross regeneration project will see the delivery of a new town centre at Brent Cross and the delivery of the necessary infrastructure in Phase 1 will consequently enable the different development elements to be brought forward.

The elements of critical infrastructure delivered in Phase 1AN include the diversion of the River Brent, the realignment of Prince Charles Drive to follow the course of the diverted river and the provision of new bridges including vehicular and pedestrian river bridges spanning the diverted River Brent and a 'Living Bridge' which will provide connectivity for pedestrians and cyclists through a series of public spaces that will link the southern development to the Shopping Centre. This broad sweep of public realm will frame the entrance leading towards the new extended shopping centre. In addition the improved road network will see a number of new road bridges crossing the diverted river corridor to provide access into the Shopping Centre and the new Bus Station. A number of pedestrian bridges are also proposed to cross the river corridor to provide connectivity.

The open space and biodiversity works contained within the current RMA is restricted to the extent of Central Brent Riverside Park, or Reach 2; the adjoining Reaches 1 and 3 will complete the open space and biodiversity provision within the new Riverside Park with the detail of these Reaches to be provided within the Phase 1B North RMA.

An inclusive access strategy has been approved for the whole of Phase 1AN including the current RMA. In the current application, access to the Riverside

Park accommodates a maintenance route for the Environment Agency as well as pedestrians and cyclists. Inclusive access is provided by way of ramped access and a proposed lift (the details of which are to come forward via a condition) in a proposed zone to the west of the Living Bridge.

Additional works proposed through the Infrastructure that will impact on the current RMA include a number of road bridges, primarily those related to the western and eastern roundabouts which provide entry to the Shopping Centre, including the Bus Station, at an elevated level. Additional road bridges provide access to different basement areas within the shopping centre. The Living Bridge, as noted above, provides a direct connection to the southern development area, including Clitterhouse Playing Fields and Claremont Park. All the bridges that cross the river corridor impact on the amenity and biodiversity that will be available within the riverside park. Where bridges cross the park and river there will be impacts caused by shadow that will affect the type of flora and fauna that can be introduced. The bridges will in addition restrict headroom to park visitors. The park layout, in relation to bridge headroom and the biodiversity strategy has therefore been critically designed by the applicants in discussion with relevant consultees to ensure that these matters are adequately addressed.

The Infrastructure RMA also includes the provision of an acoustic barrier between the A406 North Circular and Riverside Park. Full details of the acoustic barrier are not provided in this RMA however the location and scale of acoustic barrier that will be required has been assessed in the Environmental Statement Further Information Report that supports the current RMAs. Without the acoustic barrier the amenity of park users would be adversely impacted by traffic noise from the A406. Full design details of the barrier are reserved for the further assessment of the planning authority by way of a planning condition (Condition 29.10 attached to the 2014 S73 Consent).

The Infrastructure RMA includes a Temporary Bus Station strategy which will see a temporary bus station being provided to the west of the Shopping Centre. In time, the new Bus Station will be provided one level up from and to the north of the Riverside Park overlooking the eastern section of Reach 2. Road bridges will connect across the park and river corridor to provide access and egress to the new permanent Bus Station. The detailed design and location of the permanent bus station will be submitted in a future phase RMA.

4. MATERIAL CONSIDERATIONS

4.1 Key Relevant Planning Policy

In this case, the Development Plan comprises the London Plan (Consolidated with Further Alterations since 2011) (March 2015) at the strategic level and, at the local level, Barnet's Local Plan (Core Strategy (2012)) and the Saved UDP Policies GCRICK and C1-C11, which apply to the application site and

are supplemented by the Cricklewood, Brent Cross and West Hendon Regeneration Area Development Framework (2005).

The Council's Development Management Policies DPD (2012) states at paragraph 1.4.3 that it will not apply to planning applications for comprehensive development in the Brent Cross unless and until the Core Strategy is reviewed in accordance with Policy CS2 and Section 20:13 of the Core Strategy.

Detailed consideration of the application against key London Plan and London Borough of Barnet policies can be found in **Appendix 2**. Suffice to state here that the application is considered to be in accordance with Policy. The application is for matters reserved following the grant of the outline planning permission under the 2014 permission and as such the policy considerations have previously been considered and have been found to have been met.

National Planning Policy Framework

The 'National Planning Policy Framework' (NPPF) was published on 27 March 2012. This is a key part of the Governments reforms to make the planning system less complex and more accessible, and to promote sustainable growth.

In March 2014 the National Planning Practice Guidance was published (online) as a web based resource. This resource provides an additional level of detail and guidance to support the policies set out in the NPPF.

4.2 Assessment Against Outline Consent

The RMA must comply with the approved 2014 permission, or any subsequent variation to that permission. The 2014 planning permission includes revisions to the 2010 permission including revised parameter plans, a revised Development Specification and Framework (DSF), revised Design Guidelines (DG), revised Public Realm and Open Stage Strategy (PROSS) and a revised Design and Access Statement (DAS).

The relevant Parameter Plans approved in the DSF, in relation to Central Brent Riverside Park are 003: Public Realm and Urban Structure and 011: River Brent. The former is development-site wide and shows how the open space provision and the riverside park fits into the wider development, including the relationship of the proposed riverside park with the proposed infrastructure including the diverted River Brent and the relationship with proposed green corridors and cycle and footpath networks.

The approved 2014 Section 73 Consent includes a requirement for open space/habitat provision for the proposed Riverside Park and Nature Park 5 (contained within the wider Riverside Park) of 3.1 ha and 0.2 ha respectively. The current proposals include an overall reduction in the amount of open space to be provided within the River Brent Corridor.

The detailed design process for the road infrastructure in Phase 1A (North) has been ongoing with LBB highways and TfL. As a result of these discussions it is proposed to widen Prince Charles Drive through Reach 2 and it is proposed to run a signalised roadway through the western internal roundabout within Brent Cross East Development Zone which links to the replacement A406 Tempelhof Bridge.

The widening of Prince Charles Drive results in the area for central section of the Brent Riverside Park being reduced by 0.12 ha from 1.01ha to 0.89ha. The area of Nature Park NP5 (which is located within the western roundabout) will reduce from 0.2ha to 0.13ha. However, to compensate for the area lost in Nature Park NP5, it is proposed to increase the area of Nature Park NP4 from 0.2ha to 0.27ha. Overall the reduction in the open space provision within Riverside Park is from 3.1ha to 3.07ha when the increased area of NP4 is factored in.

As stated above these reductions are as a result of the detailed design of the road infrastructure which has resulted in the need to widen Prince Charles Drive to ensure it is of sufficient Highways' standard and this has consequently resulted in the reduction in the available width of the Riverside Park and space for biodiversity planting. It is important to note that the width of the walkway and river channel remain unchanged. The reduction in Nature Park 5 is as a result of design changes and the need to include a further lane across the western, Tempelhof Road, roundabout. This requires embankments to support the roadway and which have consequently reduced the size of the Nature Park. A third marginal reduction relates to changes to the eastern roundabout design. The eastern roundabout however is located in Reach 1 and impacts arising from detailed design of the roundabout, in relation to open space and biodiversity within Riverside Park, cannot be approved in this application. The detailed design of the eastern roundabout is however considered within the current Infrastructure RMA.

The Environment Agency ("EA") has stated that the reduction in Riverside Park can be addressed by the provision of compensatory provision, either by way of financial contribution or the provision of open space elsewhere in the area. The EA response relates to the reduction in habitat and biodiversity within Riverside Park and the applicants have engaged with the EA in order to determine the compensation sum to be agreed. This compensation payment will be used to improve biodiversity and ecology habitats outwith the application site but within the Borough and will need to be secured via S106 Agreement. The approach adopted is considered by officers to be a practical approach to addressing the reduction in open space. It is noted that the reduction is to habitat on the south side of the diverted River Brent where there is no public access. A compensatory sum of £200,000 has been agreed between the EA and the applicants.

The reduction in open space within Nature Park 5 is discussed in the Environmental Statement: Further Information Report that accompanies the current RMAs and which concludes that the loss can be mitigated by a like for

like expansion in Nature Park 4 (increasing from 0.2ha to 0.27ha) which is located outwith the current RMA in the west of the wider Brent Cross scheme, the details of which are to be submitted under Phase 1B North RMA. The Developers have therefore confirmed that the reduction in the area of Nature Park 5 will be compensated by a corresponding increase in Nature Park 4. This is also captured in the accompanying application under condition 2.4 and 2.5 which will update Parameter Plan 003 and associated Table 4. .

A contribution of £200,000 has also been agreed towards improvements in biodiversity and ecology habitats off-site within the borough. This will need to be secured via a deed of variation to the section 106 agreement attached to the S73 Consent. In combination, these measures are considered sufficient to mitigate and compensate for the reductions in area within the Riverside Park and Nature Park 5. The Environment Agency have agreed to this package of measures and do not object to the application.

The proposed development is, subject to the changes proposed under the application under Conditions 2.4 and 2.5 (15/05040/CON) – see section 5.4 below), generally consistent with the requirements of the Section 73 2014 planning permission.

4.3 Public Consultations and Views Expressed

Public Consultation

4,173 local residents were consulted by letter. The application was advertised in the local press on 11th June 2015 and 17 site notices were put up on site on 11th June 2015. The consultation letters allowed a 5 week period to respond. 5 letters of objection were received in response to this consultation. Statutory consultees and other interest groups were also consulted on the application.

A summary of the comments received and officer comments in response can be found under **Appendix 4** 'Consultation Responses' of this report.

The consultation process carried out for this application is considered to be appropriate for a development of this nature. The extent of consultation exceeded the requirements of national planning legislation and the Council's own adopted policy.

The S73 Consent includes a requirement under Condition 1.23 for submission of a Public Consultation Strategy This was submitted to the Council and considered under planning reference 14/07891/CON. It was approved on 31 March 2015. Details of the developer's own consultation process are set out in the Statement of Community Involvement submitted with the Phase 1A (North) Reserved Matters Submission and accorded with the requirements of the approved strategy.

Consultation Responses from Statutory Consultees and other interest

groups

Environment Agency:

The Environment Agency have assessed the Central Brent Riverside Park proposals in conjunction with the parallel RMA for Infrastructure and have provided comments that respond jointly to both applications given the interrelationship of the river diversion with the riverside park landscaping. The EA have reviewed both applications from the perspectives of the impacts on the ecological environment, the Water Framework Directive (WFD), flood risk and the risks of surface water pollution. More detail on their comments is provided in Appendix 4.

The EA have confirmed that they do not object to the approval of the Central Brent Riverside Park RMA subject to securing a S106 financial contribution towards procurement of Ecological Environment and Water Framework Directive improvements to a stretch of the Mutton Brook within Barnet; and subject to the imposition of a number of conditions relating to the construction of the river channel, installation of furniture within the riverside park, and construction method statement to demonstrate how pollution to surface water will be prevented. Appropriate conditions and informatives are included in Appendix 1.

Transport for London:

TfL does not object to the approval of the Central Brent Riverside Park RMA subject to the comments and advice provided, summarised in Appendix 4.

London Fire Brigade:

No objection Raised

Highways Agency:

No objection Raised

Brent:

No objection Raised

Thames Water:

No objection Raised

Consultative Access Forum (CAF):

The Consultative Access Forum has assessed the Central Brent Riverside Park proposals in conjunction with the parallel RMA for Infrastructure and have provided comments that respond jointly to both applications given the interrelationship of the river diversion with the riverside park landscaping. The Consultative Access Forum does not object to the approval of the Central Brent Riverside Park RMA and have provided comments regarding inclusive access to the Riverside Park including in relation to the pedestrian/cycle ramp and lift access to the lower level riverside walkway. Their comments are summarised in Appendix 4.

London Borough of Barnet Internal Consultation responses:

Environmental Health:

The EH officers have reviewed the Brent Riverside Park RMA information in conjunction with the details submitted for the Infrastructure RMA and have no objections to the approval of the Central Brent Riverside Park RMA.

Transport and Regeneration:

Transport and Regeneration officers have reviewed the Brent Riverside Park RMA information in conjunction with the details submitted for the Infrastructure RMA. The interfaces between the riverside walkway/cycleway with the proposed road network have been designed in consultation with transport officers. There are no transport objections to the approval of the Central Brent Riverside Park RMA.

4.4 Application under Conditions 2.4 and 2.5 for the variation to the provision of open spaces (ref: 15/05040/CON)

Conditions 2.4 and 2.5 of the 2014 permission allow for amendments to be made to the DSF, DAS and DG where changes brought about, through for example a Reserved Matters Application, necessitate such amendment.

An application has been submitted under condition 2.4 and 2.5 to vary a number of provisions relating to the principal Infrastructure RMA and also to the Central Brent Riverside Park RMA. It is noted in the application that the requirements of Parameter Plan 003 relating to Public Realm and Urban Structure defines in Table 4 the area of Nature Park 5 and the Riverside Park. The marginal reduction in the available area of each is addressed in detail in the current application under 15/05040/CON whereby the area of Nature Park 5 will be reduced from 0.2ha to 0.13ha and where the area of the river corridor is reduced from 3.1ha to 3.07ha. The developers argue that the reduction in Riverside Park arise as a result of the need to widen Prince Charles Drive, to ensure it is of sufficient Highways' standard and as a result of minor changes to the design of the eastern roundabout.

The proposed change in the area of Nature Park 5 is addressed in this separate application by way of consequential changes to the Design and Access Statement changing the size parameters of Nature Parks from 0.2ha – 0.5ha to 0.1ha – 0.5ha. In addition, the spatial extent of the Riverside Park was approved in the Section 73 Consent at 3.1ha. The current application proposes a park at 3.07ha.

It is the applicants' intention that changing the parameters and definitions within the approved planning permission will consequently render the current changes as acceptable as the development is brought back into accordance with the 2014 S73 Consent.

The s73 planning permission is designed in such a manner so as to allow such changes in order to reflect the phased approach to delivery of the wider

development and the likelihood of delivery being impacted by external economic events.

The application, reference 15/05040/CON under Condition 2.4 and 2.5 is also before Members at this Committee for approval. Approval of the application will ensure that the RMA application is consistent with the development's supporting documents. Officers recommend that the Condition 2.4 and 2.5 application reference 15/05040/CON is approved.

It will be noted that the Environment Agency have sought compensation for the loss of open space within the river corridor; this is discussed in more detail in section 5.2 above and will be secured via a deed of variation to the section 106 agreement attached to the 2014 S73 Consent.

5. PLANNING ASSESSMENT

5.1 Assessment Against Parameters of the 2014 Section 73 Consent

The 2014 S73 Consent for Brent Cross Cricklewood is a 'hybrid' permission, in that planning permission has been granted in outline for the majority of the proposed development, whilst detailed permission has been granted in relation to the key gateway access junctions.

Parameters and principles are provided in the approved application documents, in particular within the Revised Development Specification and Framework and the Parameter Plans appended to it, to control the nature and timing of reserved matters applications to accord with the assessments undertaken within the EIA of the likely significant environmental impacts of the Scheme.

The 2014 permission is tied to those key parameters and principles in order to ensure that that proposed development is carried out, used and occupied in accordance with the assumptions which underpin the EIA Process. The current RMAs have been considered under the requirements of the EIA Regulations and it has been agreed that the assessment of the proposals under the Regs can be dealt with by way of an Environmental Statement Further Information Report. The Further Information Report accompanies the Infrastructure and Central Brent Riverside Park RMAs.

The revised Development Specification and Framework (the 'DSF (2013)') sets out the updated physical and other parameters and principles to guide and govern the subsequent design and approval of details in accordance with conditions attached to the 2014 permission. The DSF (2013) identifies aspects of the proposed scheme that fall within the parameters and principles approved under the 2014 permission, and those that do not (the Reserved Matters) which are therefore subject to application to obtain Reserved Matters approval.

The development has been assessed as being generally compliant with the Section 73 planning permission. An assessment against the relevant parameter plans within the Revised Development Specification Framework is provided in **Appendix 5**.

As with other large-scale redevelopment schemes, the need for flexibility was anticipated in framing the 2010 Permission in a way which was specifically designed so as to allow the BXC Development to evolve and respond to market forces and opportunities, as well as to enable improvements to be made to the design and delivery of the development in accordance with relevant development plan policies and other guidance. Conditions 2.4 and 2.5 of the Section 73 Consent therefore allow applications to be submitted to vary the content and development parameters set down in the DSF, DAS and DG documents (see section 5.4).

Once the phase or sub phase condition 2.4/2.5 application is approved, the development proposed in any corresponding RMA will be expected to be in full compliance with the revised s73 planning permission. The current RMA application consequently must be assessed with the changes made under 2.4 and 2.5 in mind.

The RMA must also accord with the planning conditions attached to the Section 73 Consent. The Section 73 Consent contains specific conditions requiring submission of further details pre-commencement or compliance with the terms of the condition.

With this in mind, it will be noted that conditions 44.2 - 44.4 of the Section 73 Consent relate specifically to the diversion of the River Brent and associated works. Condition 44.2 is a pre-commencement condition and states that no development is to take place within Phase 1 or any sub phase or phase unless and until details of the River Brent alteration and diversion works relevant to phase 1 have been submitted and approved; the details are specified in the condition and include for example details of flood storage and a scheme to provide a return overland flood flow route from the flood plain on the North Circular Road to the River Brent.

Condition 44.3 relates to the storage of materials within the River Brent corridor. Condition 44.4 is also a pre-commencement condition and states that no development will begin within Phase 1 (or Sub Phase of Phase 1) or any other Phase unless and until a geomorphological assessment and scheme demonstrating, for that element of the River Brent Alteration and Diversion Works relevant to such Phase, that the realignment of the River Brent is sustainable, shall have been submitted to, and approved in writing by, the LPA.

In addition, Condition 44.5 – 44.9 relate to surface water drainage and 44.5 requires that any Sustainable Urban Drainage System to be submitted for approval in accordance with Condition 1.27 in relation to each Phase or Sub-Phase shall be integral to the site and ensures a commitment to a minimum 25% reduction in surface runoff of current 1 in 100 year flow plus 30% for

climate change through incorporation of SUDS features, such as Green and Brown Roofs, detention basins, gravelled areas, swales, permeable paving and pipe storage. It must be demonstrated that SuDS have been maximised across the site, with justification provided if targets set in the London Plan cannot be met.

Condition 44.10 relates to the Water Framework Directive and states that no development shall begin within any Phase or Sub Phase until a detailed Water Framework Directive Assessment examining the impacts to the watercourses on site and associated mitigation measures has been submitted to, and approved in writing by, the local planning authority. The scheme shall be implemented as approved.

Matters relating to surface water management and flood risk are addressed below in section 6.8.

It will be noted that the development has been subject to previous Flood Risk Assessment during the 2010 permission and the Section 73 Consent. The separate Infrastructure RMA (15/03312/RMA) includes an addendum to the Section 73 Consent Flood Risk Assessment.

Where these matters have not been fully addressed in the current application then clearly work cannot commence until the planning authority is satisfied that the full requirements of the planning conditions have been met. The Developer's attention is drawn to this requirement. Omission of aspects from the current application that are required under pre-commencement conditions does not in itself prevent determination of the current application.

It is therefore considered that the proposed open space and ecological habitat improvements for Central Brent Riverside Park conform to the Parameter Plans and s73 2014 permission documents, as modified by application under conditions 2.4 and 2.5 (reference 15/05040/CON see Section 5.4 above). In addition, the proposed development will continue to need to comply with all relevant conditions attached to the s73 planning permission.

5.2 Principle of Use

The principle of use has already been established through the granting of the original 2010 outline planning permission and the subsequent Section 73 outline planning permission (F/04687/13) (the 2014 permission or Section 73 Consent). The 2014 permission masterplan was supported by a Revised Development Specification and Framework (RDSF), Revised Design Guidelines (RDG), Revised Public Realm Open Space Strategy (RPROSS) and a Revised Design and Access Statement (RDAS) along with a number of other supporting background documents.

The masterplan includes Parameter Plans 003: Public Realm and Urban Structure and 011: River Brent which are relevant to the development proposals and which identify the significant enhancement to the current River

Brent corridor by way of the river diversion.

Parameter Plan 003 defines the general location of the Riverside Park and establishes the location of Nature Park 5 to the west of the central reach. The provision of a connecting access from the proposed Living Bridge to the river is also shown. In addition, Parameter Plan 011 defines the extent of the three different reaches proposed, the general width of the riverside park, different user group access points and the vertical connection between the Riverside Park and Living Bridge/Bus Station. The location of walkways and crossing points are identified.

The proposed development has been assessed against current development plan policies and has been found to be in accordance with policy.

Worth particular note here is Policy CS 7 of Barnet's Core Strategy (adopted September 2012) which aspires to create a greener Barnet. The policy aims to meet increased demand for open space through:

- Securing improvements to access, children's play and sports facilities.
- Maintaining and improving the greening of the environment through protection of green space, trees, hedgerows and watercourses and enabling green corridors to link green spaces.
- Ensuring development protects existing site ecology and makes the fullest contributions to enhancing biodiversity, both through on-site measures and by contribution to local biodiversity improvements.

In addition, 'saved UDP policy' Policy C4 – Sustainable Design, states that the Council will seek to ensure that the redevelopment of the Regeneration Area pursues the highest standards of environmental design. Development should for example:

- Meet high performance standards for environmentally sustainable design and construction;
- Create an integrated network of open spaces and pedestrian and cycle routes to meet leisure, access, urban design and ecological needs both within the Regeneration Area and through enhanced connections to the surrounding area; and
- Ensure the restoration and enhancement of the River Brent and its corridor in order to provide both amenity and nature conservation to the area; development which would be directly or indirectly detrimental to the nature conservation value of the Brent Reservoir Site of Special Scientific Interest will not be permitted.

The planning history and support in principle for the development proposals is long established. Nevertheless, the current proposals must in the first instance be assessed against the 2014 planning permission documents, including any relevant planning conditions attached to that permission that may impact on or control the proposals.

5.3 Timetable for delivery of different Riverside Park elements

The current RMAs under consideration by Members relate to the Infrastructure proposed under Phase 1A(N) and the open space relative to the central reach of the diverted River Brent. The Infrastructure reserved matters application includes the diversion works for the River Brent, the roads infrastructure and the provision of bridges across the diverted River Brent, including the proposed Living Bridge linking a series of green spaces to the extended Brent Cross Centre. The relationship between the two RMAs is discussed in section 4.3 above.

Whilst the Infrastructure RMA proposes to deliver the full infrastructure requirements pertaining to Phase 1A(N), the Central Brent Riverside Park RMA relates solely to the design details of Reach 2 of the diverted river corridor. The diverted River Brent corridor is divided into three separate reaches with the central reach area being the subject of this application. The information submitted in respect of the adjoining reaches to the west and east provide indicative details only and cannot be approved under this application. They do however demonstrate how the proposed works in Reach 2 will tie into the adjoining river diversion proposals.

The two current RMAs must be determined at the same committee because the open space provisions for the Riverside Park are inextricably linked to the provision of the base infrastructure. Without the infrastructure being confirmed, the open space design cannot be confirmed. In effect, Members are asked to approve the infrastructure in its entirety but approve the open space detail for the central reach of the proposed Riverside Park to the exclusion of the eastern and western reaches.

Members should also note that a number of planning conditions attached to the Section 73 Consent will continue in force and require the submission of further details relating to for example flood prevention and drainage design. These need to be submitted to and approved by the planning authority prior to any work commencing on site and will be considered in subsequent applications.

5.4 Overall Design Principles

In relation to the RMA under consideration at this Committee, there are no buildings proposed. The works comprise the provision of landscaping to accompany the new river corridor, including planting details of the marginal riparian environment, the planting of amenity spaces, the design of retaining walls where required and the design and route of the combined pedestrian, cycle and Environment Agency maintenance route that runs through the riverside park. The Infrastructure RMA (15/03312/RMA) incorporates details of the River Brent diversion alignment, however all other matters relating to Reach 2 or Central Brent Riverside Park are contained in this current application.

Drawings 1065-03-003J, 004J, 005J and 006J provide the General Arrangement drawings for the proposed Central Brent Riverside Park.

Whilst the road and pedestrian bridges, including the proposed Living Bridge, are not included in the current planning application (see 15/03312/RMA for relevant details) these structures will have a considerable impact on the amenity of Riverside Park and the manner in which it can be landscaped. More particularly, the area under the more substantial vehicular bridges is more difficult to landscape and this impacts on the design approach that has been adopted.

Full details and discussion on all the proposed bridges can be found in the separate reserved matters application report for Infrastructure application reference 15/03312/RMA. It is useful to briefly describe the key structures here however in order to understand their influence on design and layout of the current RMA.

It is proposed to replace the existing Tempelhof Avenue Bridge with a new bridge that will improve transport links between the north and south sides of the development. The bridge spans over the:

- A406 North Circular Road and associated slip roads;
- Re-aligned Prince Charles Drive;
- Tilling Road; and
- Re-aligned River Brent.

The design of Templehof Avenue Bridge leads into the western roundabout which has consequently resulted in a reduction in the available open space within Nature Park 5 as discussed elsewhere in this report.

The Living Bridge is the main pedestrian connection route from the southern development area into the shopping centre and the design of the Living Bridge will allow views from that structure over the Riverside Park.

As noted above, the realigned Prince Charles Drive, running parallel to the A406 North Circular, reflects the diversion of the River Brent. The road runs between the A406 and the new river corridor with a number of vehicular bridges crossing the realigned River Brent. There are two bridges associated with the Western Roundabout accessed via Templehof Bridge. There are also two bridges associated with the Eastern Roundabout. Between the two roundabouts there is a series of 4 access bridges (Bridges 3 – 6) that provide vehicular access from Prince Charles Drive to development plots to the north of the diverted river. Bridge 6 to the east gives access direct into the new proposed Bus Station location.

The bridges are presented as a 'family' of bridge structures, comprising curved, simple, elegant structures including steel or aluminium railings.

The initial design of Prince Charles Drive provided for two lanes of traffic with no pedestrian pavement. Through discussion with the applicants the design

has been amended to include sufficient space for pedestrian in the event of a vehicle breakdown. This resulted in an approximate 2.5m increase in the width of the proposed Prince Charles Drive. This also accommodates improved forward sightlines for traffic emerging from the bridge crossings across the River Brent. The result of the increased width in Prince Charles Drive is a reduction in the available width of the proposed Riverside Park within the Reach 2 area as outlined in section 4.4 above. The applicants have engaged with the Environment Agency to ensure that the design and layout of the park and river channel now proposed is acceptable.

The proposed Riverside Park remains in accordance with the 2014 S73 Consent and associated Parameter Plans as adjusted by the corresponding application under conditions 2.4 and 2.5.

A consequence of the reduction however, is to reduce the available planting area available south of the river, between the river and the proposed Prince Charles Drive; this area is not accessible to the public.

There is no pedestrian or cycle access to the south bank of the river within Reach 2, a typical cross section of the river shows that Prince Charles Drive will sit at an elevated level south of the river. An acoustic barrier between the A406 and Prince Charles Drive has been identified as a potential requirement to ensure that the amenity of park users is not significantly adversely impacted by excessive noise.

The available space between the highway infrastructure and the river bed is to be landscaped with a permanent turf reinforcement planted geo-textile mat. No details of the acoustic barrier have been provided with this RMA and this detail is subject to a further planning condition of the s73 Consent (condition 29.10) that must be agreed prior to beginning development on Brent Riverside Park.

The vertical sloping walls of Prince Charles Drive fronting the river incorporate a mix of different methods of 'greening' the vertical walls; these include the use of weldmesh panels with climbing plant species fixed to the retaining wall and the use of 'living willow' panels. The mix of finishes ensures a varied appearance to the vertical walling.

The river corridor comprises marginal planting areas and gabion baskets to define the actual river channel. An area of river gravel, of varying width, separates the marginal planting area from coir rolls which then mark the interface between the marginal planting areas and the river flow.

The north bank of the diverted river is wider and accommodates the footpath/cycle path and EA maintenance track as well as areas of amenity planting. Bench seating areas are provided at locations throughout the riverside park. The amenity grass areas incorporate trees, these will all be located above the 1 in 100 year flood level to ensure they are less likely to suffer from water inundation.

The proposed access route is to be formed in brushed concrete. The 2014 S73 Consent requires that the maintenance route is provided with a 6m wide access track. The proposed route within Reach 2, to connect into the reaches on either side, is a minimum of 6m in width. However in order to create a route of enhanced interest areas of appropriate perennial planting will be allowed to encroach into the path, effectively reducing the width to 4m in places. The effect of the planting is to form a more sinuous meandering and softer green route to reflect the adjoining river channel. The construction of this green edge, employing a 'grasscrete' type construction method, will still enable the EA to use the full 6m width for vehicular traffic. The effective reduction in width has been raised with the Environment Agency however they have not expressed any concerns about the proposal. Officers therefore consider the proposals acceptable.

As noted above, in order to address potential constraints to vegetation growth under the proposed bridges the surface finish tends to a greater use of river gravel and harder surfacing materials compared to less shaded planted areas.

Access to the Park is discussed in more detail in section 6.6 however it can be noted here that the access arrangements within Reach 2 comprise four stepped access points, incorporating cycle ramp channels, one at the east roundabout from the river walkway to the bus station level, one in the vicinity of the Living Bridge where this crosses the diverted river to access the bus station, one towards the western roundabout that would access the high level walkway proposed in a future phase of development and one in the vicinity of the western roundabout that will also access the future high level walkway.

In addition a ramped access is proposed from Tempelhof Avenue to provide access from the river path to the high level walkway. In discussion with the applicants it has been agreed that the developers will provide a lift access to the immediate west of the Living Bridge. Full details of the lift structure and its location will be provided by way of planning conditions. Access will also be available from the east via the A406 underpass.

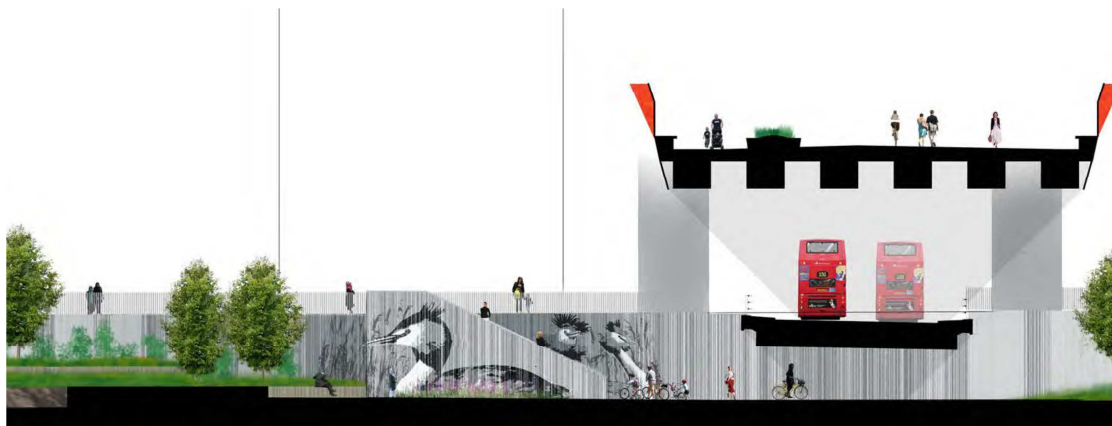
Drawings 1065-03-203F, 204F, 205F, and 206F provide detailed information of the proposed planting regime, including species, numbers, location and size of planting.

Included in the Central Brent Riverside Park RMA is the provision of Nature Park 5 (NP5). NP5 is located to the west of Reach 2 and is located beneath the western roundabout. The road infrastructure sits at a higher level than the nature park and the area within the roundabout is open. The diverted river is allowed to meander into the nature park area by way of a narrow channel. The soft landscaping areas and riverside park areas will be planted in a similar manner to the rest of the riverside park. Given the design of the area, a more substantial wetland area will be created. The area of Nature Park 5 as noted above, has been reduced to reflect the detailed design of the highway infrastructure. There is no public access to Nature Park 5.

Typical cross sections through the riverside park accompany the planning application. These show how the south bank will be formed between the river channel and Prince Charles Drive and define the construction of the river channel, designed below a 1 in 5 year + climate change flood level. To the north of the river channel is the combined access/EA maintenance route and then the north bank. A large extent of the central reach of riverside park will front onto (and sit lower than) the proposed bus station within Brent Cross Centre. The interface between the proposed bus station and the riverside park will be dealt with in a future RMA.

As noted above, a number of bridges are proposed to cross Riverside Park and details of these are provided in the associated Infrastructure RMA 15/03312/RMA. The proposed Living Bridge detail is also provided in 15/03312/RMA and shows that the Living Bridge sits at a higher level than the river crossing bridges. The Living Bridge takes access from Market Square in the south and will link directly into the extended Shopping Centre on the north side. The proposed Bus Station sits at a lower level compared to the level that the Living Bridge accesses the Shopping Centre.

The indicative section below provides some clarity on the different levels. The footpath/cycle path runs at a level above the river channel. The section shows a proposed access road into the bus station with the Living Bridge access into the Shopping Centre above the access road level. Whereas the riverside park forms a green corridor with rising planted ground on the north bank, the construction geometry of the access road bridges and the shadow that these create require a hard landscape solution and retaining walls which will be subject to an artwork strategy.



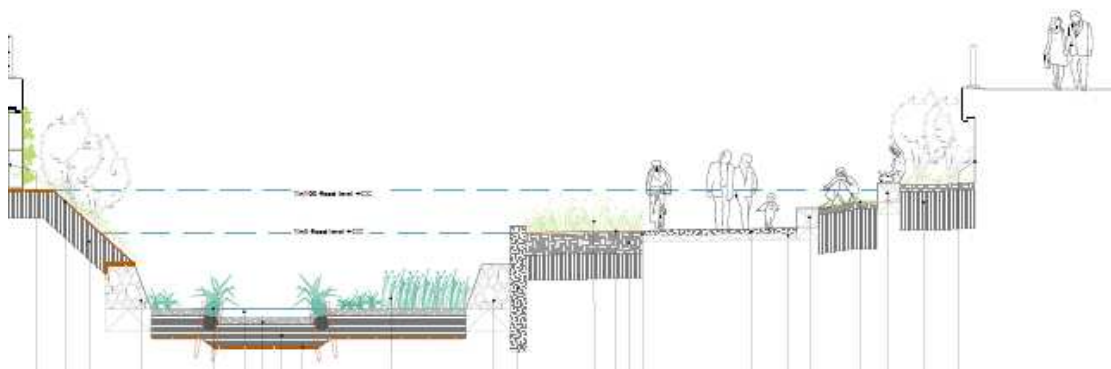
Illustrative section through Living Bridge

Similarly, the eastern section of the riverside park and its interface with access/egress roads into/from the bus station requires a more formal hard landscaped approach to the northern face as shown in the image below; again the indicative art work strategy is shown. The stepped access from the park to the bus station level is also shown in this view.



Illustrative section at eastern end of Riverside Park

With regard to the western section of the proposed central reach the cross section below shows the relationship between the river channel, the low level access road and north bank; the high level access route shown on the right of the extract will be provided in a subsequent development phase.



Illustrative section through river channel and park

An acoustic barrier (the details of which is to come forward under Condition 29.10 of the 2014 S73 Consent) is proposed to be erected between the A406 and Riverside Park. The provision of the acoustic barrier is discussed in more detail in the Infrastructure RMA. The barrier is essential to ensure that the amenity of park users is significantly adversely affected by noise pollution arising from the A406.

The design and layout of the proposed Central Brent Riverside Park is considered acceptable. The park is designed to provide an area of open space for visitors to the shopping centre as well as providing a linear green corridor for residents in the area. The opportunity to divert the river Brent facilitates the expansion of Brent Cross Shopping Centre however it also presents an opportunity to enhance biodiversity and ecological habitat in the area.

The proposals achieve their aim by virtue of the fact that they fall within the parameters of the 2014 S73 Consent which they must comply with. Whilst the parameters have been met this has been achieved in some cases through minor variation to the parameters. Officers have negotiated with the applicants to ensure that any reductions in open space are satisfactorily compensated by corresponding elsewhere within the development or via contributions to deliver ecological improvements elsewhere. In the circumstances the

development proposed is considered acceptable.

5.5 Ecology, Landscaping and Bio-diversity

The 'sustainable development' imperative of NPPF 2012 includes enhancing the natural environment and improving biodiversity (para 7). London Plan 2011 policy 7.19 states that development proposals, where possible, should make a positive contribution to the protection, enhancement, creation and management of biodiversity. One of the principal aims of this application is to significantly enhance local habitats, landscaping, ecology and biodiversity.

One of the key aims of the river diversion is to improve local bio-diversity within the riverside park environment. The creation of a marginal wetland environment associated with the river will help facilitate this aim.

The development proposals relative to reach 2 include a number of measures designed to specifically improve bio-diversity; these can be summarised:

- Bird tunnels – to provide habitat for kingfishers and sand martins;
- Log piles/insect hotels;
- Bird boxes;
- Bat boxes;
- Vertical pre-grown willow panels – to provide nesting opportunities for breeding birds;
- Invasive species – programme of removal;

These features are provided throughout Riverside Park. Details of the provision of bat and bird boxes for example are also included in the Infrastructure RMA as these will be incorporated into the proposed bridge structures.

In terms of landscaping, the development proposes a landscaping regime that is appropriate to the riverside park environment. Within the river corridor this will include the provision of marginal and aquatic planting, including coir rolls and different aquatic planting mixes. Tree and shrub planting will be introduced within the wider corridor outwith the river channel. Amenity grass areas will be planted in a mix of general meadow and wildflower planting in addition to general amenity grass planting areas.

The southern walls of the river park are steep given the difference in level between the park and the adjoining Prince Charles Drive. These will be 'greened' with a combination of willow stakes, vertical pre-grown willow panels and climbers.

As a result of the increased width in Prince Charles Drive, required to ensure the highway is constructed to meet Highways' requirements, there is a reduction in width along the length of Riverside Park within Reach 2. This in turn has led to a reduction in the available open space and area of biodiversity. In addition, changes in the design to the eastern roundabout to

improve visibility on the roundabout have resulted in a negligible reduction in the area of open space at that location; this area is however outwith Reach 2 and is located in Reach 1. Reduction in the extent of Nature Park 5, beneath Templehof Bridge roundabout to the west of the site will also result in a marginal loss of open space and biodiversity.

The design changes are necessary to ensure that the roads infrastructure is designed to meet required road safety standards following discussion with TfL and LBB Highways and to facilitate improved access to the Shopping Centre. The loss of open space in Nature Park 5 is addressed by a variation to the size range for Nature Parks as detailed in the Section 73 Consent Design and Access Statement, changing the small nature park range of 0.2 – 0.5ha to 0.1 – 0.5ha. The reduction is compensated however by a like for like increase in Nature Park 4. This ensures that the development accords with the Section 73 Consent. The loss in areas of biodiversity within the river channel has been subject to discussion with EA and it has been agreed that a sum will be paid by the Development Partners to the EA by way of compensation.

The loss of open space is considered marginal in the context of the River Brent diversion works and is therefore acceptable. The loss of habitat areas has been accepted by the Environment Agency and is also consequently considered acceptable.

5.6 Pedestrian, Cycle and Maintenance Access

The revised DSF incorporates Parameter Plan 011 with specific reference to River Brent. The Parameter Plan shows the general arrangements relating to the river diversion route, extent of riverside park and access points.

With regard to Reach 2, the approved DSF states that the combined pedestrian/cycle and Environment Agency maintenance route to be located on the northern side of the diverted river is to be a minimum of 6m wide. There is no requirement to provide access along the southern side of the riverside park within Reach 2.

The detailed plans accompanying the application (1065-03-003J, 004J, 005J and 006J) confirm that the proposed layout is in accordance with the requirements. The 6m wide access route is shown to incorporate planting in order to create a more sinuous route. Whilst overall the 6m width is retained the route is on occasion reduced to 4m wide of an open surfaced route. The approach is however considered by officers to be acceptable and the Environment Agency has confirmed that they have no objection. The route provided in this phase of development is a low level route. In future phases a high level route for pedestrians will be added.

The DSF also states that on the northern side of the riverside park there will be 4 locations where steps from the upper level drop down to meet the lower level pedestrian and cycle path. The proposed plans show the 4 locations, the details of how they link with the upper walkway will be submitted with the

Phase 1B North RMA. The step access points include cycle channels to facilitate use by different user groups.

With regards to inclusive access the developers have offered two options to provide all-inclusive access. The first is the provision of a ramped access within Reach 3 to the west, the details of which will come forward with the Phase 1B North RMA. .

The other graded ramped access is located within Reach 2 and is located at Plot 101. It is considered that this ramp provides an acceptable access for different user groups to the Riverside Park.

Given the different levels, inclusive access to the Living Bridge from Riverside Park is problematic. Whilst a number of stair access points will be available, incorporating cycle channels to facilitate cycle users, a more inclusive solution is required. Whilst the proposals are generally consistent with the Section 73 Consent it is considered by officers that inclusive access is essential to ensure that all potential users will benefit from access to the new riverside environment. Through discussion with the Development Partners it has therefore been agreed that an access lift will be provided to the west of the Living Bridge. The lift will ensure access is fully inclusive between the upper walkway, which is approximately at the level of the Bus Station, and the Riverside walkway. Full details of the lift have still to be provided and it is considered that this can be addressed by way of an appropriately worded planning condition.

A Phase 1A(N) Reserved Matters Transport Report (RMTP) accompanies the Infrastructure RMA and confirms that the route through the Riverside Park will be a combined footpath and cycle path route.

5.7 Archaeology

The 2014 permission includes Condition 43 requiring the approval and implementation of a programme of archaeological investigation in the following terms:

Prior to beginning the Development an over-arching Scheme of Archaeological Investigation will be submitted to the LPA setting out the process for assessing and mitigating the impact of development on archaeological interest, including appropriate post-excavation analysis, archiving and publication.

No part of the Development shall begin within any Phase or Sub Phase unless and until proposals have been submitted to and approved by the LPA for a site-specific Scheme of Archaeological Investigation to preserve or record any archaeological evidence within the Phase or Sub Phase.

Each Phase or Sub-Phase of the Development shall take place in accordance with the approved over-arching Scheme of Archaeological Investigation and the site-specific Scheme of Archaeological Investigation approved for that Phase or Sub-Phase.

In order to protect the potential heritage interests on the site it is necessary to impose a planning condition requiring a scope of investigation to be completed with the agreement of GLAAS. The condition will ensure adequate investigation is undertaken, recorded and reported with such mitigation as may be deemed necessary undertaken prior to work commencing on the proposed development.

5.8 Surface Water Management

As noted above, there are a number of conditions attached to the 2014 S73 Consent relating to surface water management and flood risk.

Essentially the development needs to demonstrate that Sustainable Urban Drainage Systems have been considered across the development site, in terms of each phase or sub-phase. As Phase 1AN is taken forward there is a clear commitment expressed by the applicants to ensure that SuDS is accommodated across the development. With regard to this current planning application however the developers have made it clear that it is not possible to accommodate SuDS provision within the river park corridor area.

This omission has been discussed with the applicants who have highlighted that the maintenance route which runs along the north bank of the diverted river is designed at a level that means the paved route will be permitted to flood during events beyond a 1 in 5 year scenario in order to maximise the capacity of the third stage river channel and thereby minimise flood risk to the adjoining development.

Given the frequency that the paved route will flood and the infrequent nature of vehicular traffic using the route it is considered impractical to incorporate Sustainable Drainage Systems within the drainage network to attenuate and improve the quality of runoff. For example it will not be practical to provide SuDS to attenuate runoff from rainfall events with a return period of 1 in 100 years + 30% climate change in an area that is inundated by fluvial floodwater during a 1 in 5 year event.

Nevertheless a new drainage network will be provided to intercept surface water from the paved route and to direct it to the river channel. This drainage network will be designed to enable floodwater to be effectively directed off the path into the main channel to enable the paved route to be quickly reopened following a major flood event. Non-return valves will also be provided on drainage outfalls to prevent floodwater from inundating the paved route when water levels are elevated within the river channel. Details of these features will

be submitted for the approval of the planning authority by way of planning conditions.

Whilst the riverside park is an integral element of the Brent Cross Regeneration scheme project it is accepted in this case that the applicants have endeavoured to ensure a robust surface water management strategy is in place for the wider development. The omission of SuDS from the river corridor in this instance is acceptable. It is also noted that the EA has not objected to these proposals.

The drainage strategy has been subject to extensive modelling and sensitivity testing by the applicants and the EA has commissioned an independent review of the proposed strategy. Support colleagues in the planning authority have also reviewed the drainage strategy and it is considered that the strategy is robust.

It should also be noted that conditions attached to the s73 planning permission will continue to exert control over the development to ensure that SuDS requirements and the requirements of the Water Framework Directive Assessment are adhered to as the development is delivered.

With regard to the latter WFD assessment, the EA consultation response to the S73 application in 2013, stated that the WFD assessment submitted at the time (Appendix D of BXC16 FRA October 2013) provided a good high level appraisal of the potential risks of deterioration to WFD quality elements. However the EA were not satisfied that the proposals would not prevent improvements required to promote recovery of the water body as required by the WFD.

A condition was placed on the S73 permission requiring a detailed WFD assessment to be submitted prior to commencement of works, there is no explicit requirement for discharge of that condition alongside this reserved matters application. Officers however consider that the assessment would usually go hand in hand with demonstrating that the river diversion proposals are acceptable under this reserved matters application. It follows that given a detailed WFD assessment is not available at this time, the applicant hasn't demonstrated that the proposals will comply with the requirements of WFD.

The EA however recommend that the only way to offset the impacts of the development and make it acceptable from an ecological and WFD perspective would be for the developer to provide a financial contribution towards achieving ecological benefit and the aims of the WFD elsewhere within the same waterbody, secured via a Section 106 (S106) agreement.

A WFD action has been identified to naturalise a stretch of the bank of the Mutton Brook and the EA believe that the naturalisation of an appropriate stretch of the Mutton Brook could be a suitable mitigation for the loss of value in Reach 2 of Brent Cross as a result of the latest proposals. This would include, but not be limited to the removal of the existing river bank treatment and replacement with a treatment that mimics a naturally occurring river. The

EA and developers have agreed a s106 contribution of £200,000 to undertake this work.

5.9 Lighting Strategy

Details of a lighting strategy have been provided within the documents that accompany the planning application. The information is illustrative in detail only and the developers have suggested that a planning condition could be imposed requiring full design details of the proposed Riverside Park lighting to be submitted for the further approval of the planning authority prior to commencing the work.

5.10 Estate Management Framework for Phase 1A (North)

Condition 7.1 of the Section 73 Consent requires an Estate Management Framework to be submitted for each phase of the development. The Estate Management Framework submitted in respect of Phase 1A North anticipates a structure for Estate Management Companies to be established for each phase of this development and sets out some general principles.

The longer term ownership of the northern element of the project will remain with the Brent Cross Partners whilst the southern part of the development will be largely owned and managed by the LB Barnet and another development partner – likely to be Argent Related.

The Brent Riverside Park – the central section of which is the subject of this RMA - will be managed and maintained by the Brent Cross Development Partners in perpetuity whilst the river channel itself will remain the responsibility of the Environment Agency.

An acoustic barrier is required to ensure that the Brent Riverside Park is protected from the noise of adjacent roads such as the A406 and can meet suitable standards expected for a publically accessible open space. It is anticipated that this noise barrier may be within the public highway and it is expected that the Highways Authority will either give a license to the Brent Cross Cricklewood development partners to allow them to maintain the structure or the structure will be adopted as part of the public highway by the relevant highway authority and a commuted sum will be paid by the Developer to cover maintenance costs.

5.11 Summary

The application is a Reserved Matters Application, a detailed application following the grant of planning permission in outline under the Section 73 Consent in July 2014. As a detailed planning application it would be anticipated that the proposals are generally complete and that the application is wholly in accordance with the outline planning permission and current

Development Plan policy. Some final details have however still to be confirmed, principally relating to matters such as a lighting strategy or art strategy.

Despite the application being for reserved matters and some final details still requiring final approval, officers are satisfied that the application can be determined as submitted and that further conditions attached to the planning permission will ensure that the final detail of those elements mentioned will be subject to the further consideration and written approval of the planning authority.

The planning application is in accordance with the existing planning permission and the principle of development has therefore been established. An assessment against Development Plan policy supports the application. Members are requested to note that this application must be considered and determined at the same time as the Infrastructure RMA and the application under Conditions 2.4 and 2.5 of the Section 73 Consent, referenced 15/03312/RMA and 15/05040/CON respectively and both before Members at this Committee.

6. ENVIRONMENTAL IMPACT ASSESSMENT

The EIA procedure in the UK is directed by the Town & Country Planning (Environmental Impact Assessment) Regulations 2011 (the 'Regulations'), EU Directive 85/337/EEC (as amended), as well as the National Planning Practice Guidance (2014).

Regulation 8 of the Regulations requires local planning authorities to consider whether or not the environmental information already before them (i.e. the ES submitted with the 2013 s73 application F/04687/13 and any additional environmental information) is adequate to assess the environmental effects of the development:

To demonstrate the continued acceptability of the ES associated with application F/04687/13 in the context of the detailed reserved matters applications for Phase 1A (North) an Environmental Statement Further Information Report (the 'ES FIR') has been submitted.

Within the ES FIR it is necessary to consider the impact of all relevant aspects of Phase 1A (North). The report therefore considers all of the Reserved Matters submissions to date and their individual and cumulative effects.

The ES FIR was first submitted and based upon the proposals within the original Reserved Matters submissions made to the Council in January. Since that time, further changes to detail in relation to the Infrastructure and Riverside Park RMAs have resulted in a revised Further Information Report being submitted with the current RMAs in order to further assess these more recent changes to the development proposals.

The supporting statement concludes that there are no new or different significant effects so as to warrant changes to the relevant parts of the ES FIR.

The ES FIR has assessed the potential cumulative impact arising from a number of committed development schemes in the wider area and has consequently identified where mitigation will be required to address any significant impacts arising from the proposed Phase 1AN development. In general, where mitigation has been identified as required the impacts have been designed out. The most critical area is that relating to noise impact from the A406 affecting the amenity of users of the Riverside Park. The incorporation of an acoustic barrier has been tested through different options within the ES FIR and a preferred solution identified that will address this matter.

The existing Environmental Statement associated with the 2014 permission supplemented by the ES FIR and the other additional environmental information previously submitted satisfactorily assess and address the impacts of the development for the purposes of determining the Central Brent Riverside Park and Infrastructure Reserved Matters Applications.

7. EQUALITY AND DIVERSITY ISSUES

Section 149 of the Equality Act 2010, which came into force on 5th April 2011, imposes important duties on public authorities in the exercise of their functions, including a duty to have regard to the need to:

- “(a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;*
- (b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;*
- (c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.”*

For the purposes of this obligation the term “protected characteristic” includes:

- age;
- disability;
- gender reassignment;
- pregnancy and maternity;
- race;
- religion or belief;
- sex; and
- sexual orientation.

Officers have in considering this application and preparing this report had regard to the requirements of this section and have concluded that a decision to grant planning permission for this proposed development will comply with

the Council's statutory duty under this important legislation.

The site will generally be accessible by various modes of transport, including by foot, bicycle, public transport and private car, thus providing a range of transport choices for all users of the site. Access to the Riverside Park is restricted to no motor based traffic, with the exclusion of Environment Agency maintenance vehicles.

This RMA has been prepared in consultation with the Consultative Access Forum (CAF) which has been set up specifically to advise on inclusive access requirements of the Brent Cross Cricklewood regeneration. Design aspects that incorporate inclusive design have been reviewed by the CAF to insure that the development proposal for the Central Brent Riverside Park is designed, delivered and managed in accordance with the Inclusive Access Strategy and the Way finding Strategy.

To provide inclusive, step free access to the Brent Riverside Park three facilities have been incorporated comprising two graded ramps and the provision of lifts from Living Bridge level to the bus station level and from the bus station level down to the lower Riverside Park walkway. The first graded ramp option is the provision of a ramped access within reach 3 to the west and is essentially the EA maintenance access route but is designed to also be suitable for use by pedestrians. The other graded ramped access is located within reach 2 and is located between the point where Tempelhof Bridge crosses the river and crosses over Nature Park 5.

With regards to the lift from the bus station/Living Bridge level to the Riverside Park, the submitted plan alongside this RMA application is indicative. The exact location of the lift within the identified zone of the Riverside Park, along with the specification and design, will be controlled via condition and will be submitted to and approved by the Local Planning Authority in consultation with the Consultative Access Forum. The submitted scheme is considered to provide for inclusive access for all users.

8. CONCLUSION

The application proposes the provision of hard and soft landscaping, ecology and bio-diversity enhancements and pedestrian and cycle path access to a proposed riverside park to be developed along the route of the diverted River Brent at Brent Cross Shopping Centre. The proposals relate solely to the provision of works within the central reach of three reaches proposed to accommodate the works to the Park. The works tie in with the diverted river, the new Prince Charles Drive and the Living Bridge that connects the Shopping Centre to the southern development area within the wider Brent Cross Cricklewood regeneration area and which crosses the riverside park. A number of bridges will also cross the park. Details of the roads and bridges infrastructure are submitted in a separate RMA planning application (reference 15/03312/RMA) which is also before Members at this Committee.

The works under this application cannot proceed without the river diversion infrastructure being approved and constructed in advance of the works hereby proposed.

There are a small number of matters that still require addressing in terms of some fine details and consequently a number of conditions are recommended to accompany any planning permission. The application has been subject to statutory consultation and a small number of third party representations have been made. No significant issues have been raised that have not been addressed or which would move officers to recommend refusal.

The application is in accordance with Development Plan policies and accords with the conditions and parameters approved in the Section 73 planning permission for the wider Brent Cross Cricklewood regeneration scheme.

Overall, officers find the proposals acceptable and recommend that the application is approved subject to conditions as outlined in Appendix 1.

LIST OF APPENDICES

- APPENDIX 1 – CONDITIONS
- APPENDIX 2 – POLICY COMPLIANCE
- APPENDIX 3 – PRE RESERVED MATTERS CONDITIONS
- APPENDIX 4 – OBJECTIONS AND OFFICER RESPONSES
- APPENDIX 5 – CONFORMITY WITH REVISED DEVELOPMENT SPECIFICATION AND FRAMEWORK
- APPENDIX 6 – SITE LOCATION PLAN

APPENDIX 1

Conditions

Conditions

1	Approved plans	
	The development hereby permitted shall be carried out in accordance with the following approved plans unless minor variations are agreed in writing after the date of this reserved matters consent with the Local Planning Authority:	
	Reach 2 Brent Riverside Park GA	1065-03-003 J
	Reach 2 Brent Riverside Park GA	1065-03-004 J
	Reach 2 Brent Riverside Park GA	1065-03-005 J
	Reach 2 Brent Riverside Park GA	1065-03-006 J
	Reach 2 Planting Plan	1065-03-203 F
	Reach 2 Planting Plan	1065-03-204 F
	Reach 2 Planting Plan	1065-03-205 F
	Reach 2 Planting Plan	1065-03-206 F
	Bench Detail	1065-03-401 C
	Bench Detail 2	1065-03-402 C
	Tree Pit In Soft Detail	1065-03-403 B
	Paving Detail Type 1	1065-03-404 C
	Step Details Type 1	1065-03-405 B
	Bench Detail 3	1065-03-406 A
	Paving Detail 2- Reinforced Grass	1065-03-406 -
	Detail Plan 1 - Bus station threshold	1065-03-430 D
	Detail Plan 2 - Living Bridge Threshold	1065-03-431 D
	Detail Plan 3 - Nature Park 5	1065-03-432 D
	Reach 2 Section - Bus Station	1065-03-433 E
	Reach 2 Section - Seating	1065-03-434 E

	Reach 2 Section - Living Bridge	1065-03-435	E
	Reach 2 Section - Seating Terrace	1065-03-436	E
	Reach 3 Section - Nature Park 5	1065-03-437	E
	Reach 2 Section - Seating Terrace B	1065-03-438	D
	Proposed Lift Zone	SK-1708	2
	<p>Reason: For the avoidance of doubt and in the interests of proper planning and so as to ensure that the development is carried out fully in accordance with the application as assessed in accordance with policies CS1, CS4, CS5, of the Barnet Local Plan and policy 1.1 of the London Plan.</p>		
2	<p>Prior to commencement of the River Brent Alteration and Diversion Works details of the lift between the Lower Ground Level and Lower Level Riverside Walkway, the location of which is indicated on plan 'SK-1708 Rev2' shall be submitted to and approved in writing by the Local Planning Authority. The submitted details shall include any associated hardstanding and access ramps within riverside park.</p> <p>Reason: In the interest of achieving a high level of accessibility, to open up the use of this Key Pedestrian and Cycle Route to a broader group of users, to ensure the delivery of a lift which improves the visual amenity of the Brent Riverside park and to allow consideration of its impacts during flood events.</p>		
3	<p>Prior to commencement of the River Brent Alteration and Diversion Works details shall be submitted to and approved in writing by the Local Planning Authority of the Brent Riverside Corridor Flood Event Management Strategy.</p> <p>Brent Riverside Corridor Flood Event Management Strategy will ensure an agreed approach to the closing of public access points to the Brent River corridor during times of flood and the swift re-opening of this important pedestrian and cycle route following the easing of flood conditions and shall include the following:</p> <ul style="list-style-type: none"> • Triggers for warning of danger of flooding of the Brent River Corridor based upon River levels and Predictive flooding information from the Environment Agency and the Met weather service. • Signage strategy at entrance points to the Brent River Corridor providing information on the status of the route and suggesting alternate routes for Pedestrians and cyclists including accessibility information. • Details of the manner of closure that will be employed, • Strategy for management and subsequent maintenance of the lift 		

	<p>shown in location on Plan 'SK-1708 Revision 2'. This shall include specifications necessitated by the incursion of the lift into a flooding zone and shall provide details of the management regime both during periods of regular use and during and following flood events.</p> <ul style="list-style-type: none"> • Details of clean up and maintenance following a flood event and targets for the timely reopening of the Brent river corridor to the public and for EA Access. • Details of soft and hard landscaping evaluation and replacement process. <p>Reason: In the interest of the safety of pedestrian and cyclist users of the river corridor, to ensure that this key route is swiftly brought back into use and its appearance maintained following a flood event.</p>
4	<p>Prior to commencement of the River Brent Alteration and Diversion Works, details shall be submitted for approval in writing by the local planning authority of the management strategy for the Brent Riverside Corridor. Following approval of the Brent Riverside Corridor Management Strategy the Strategy shall be implemented in perpetuity.</p> <p>Details submitted shall include:</p> <ul style="list-style-type: none"> - Details of the timing and the route to be taken by Regular visits by uniformed representatives of the Brent Cross Shopping Centre Security/staff between the hours of 9:00 am to 8:00 pm daily. - Details of management relating to Bin, Graffiti, other forms of vandalism <p>Reason: To provide activity along this route and to deter antisocial behaviour in the interest of maintain a high quality appearance of the route and encouraging its use.</p>
5	<p>Full details of the proposed CCTV coverage outlined in the Estate Management Framework, in so far as it relates to Central Brent Riverside Park, shall be submitted to and approved by the local planning authority. The approved CCTV strategy shall be implemented prior to the riverside park opening.</p> <p>Reason: In the interest of security within the Central Brent Riverside Park and to comply with Policy 7.3 of the London Plan and Policy CS12 of Barnet Local Plan.</p>
6	<p>The approved Landscape and Environmental Management Plan (LEMP) shall be fully implemented by the developers and their successors and adhered to with regard to hard and soft landscaping, save for any minor changes agreed in writing with the LPA.</p> <p>Reason: In the interest of the ongoing appearance of the Central Brent Riverside Park.</p>
7	<p>Notwithstanding the submitted plans, prior to the commencement of the River Brent Alteration and Diversion Works a detailed public art strategy relating to the public art features on the walls of the Central Brent</p>

	<p>Riverside Park shall be submitted to the local planning authority for its approval and shall thereafter be implemented in the manner approved.</p> <p>Reason: In the interest of the appearance of the Central Brent Riverside Park.</p>
8	<p>Prior to commencement of the development full details of the proposed lighting strategy, including:</p> <ul style="list-style-type: none"> - Locations - Materials - Lux levels - Operating hours <p>shall be submitted to and approved in writing by the LPA. The lighting shall be managed in order to avoid light spill which will reduce the ecological value of the watercourse habitats. The development shall thereafter be implemented in accordance with these approved details</p> <p>Reason: In the interest of security and ecology within the Central Brent Riverside Park.</p>
9	<p>Prior to the commencement of work, design details of the proposed steps to access the riverside walkway shall be submitted for the written approval of the LPA and shall for the avoidance of doubt conform to the requirements of BXC Inclusive Access Strategy and include suitable handrails</p> <p>Reason: In the interest of the accessibility of the Central Brent River Corridor</p>
10	<p>Details of any temporary signage that may be required in relation to the provision of access to the Central Brent Riverside Park herein approved shall be submitted to the LPA for their approval.</p> <p>Reason: In the interest of achieving clear direction throughout the construction process.</p>
11	<p>Prior to the installation of bins, benches and bird boxes, full details of the design of these items will be submitted to and approved by the local planning authority.</p> <p>Reason: To minimise blockages and pollution of the river during a flood event.</p>
12	<p>Prior to the commencement of work, details of the proposed surface water management strategy and drainage network within Central Brent Riverside Park shall be submitted to the LPA for their written approval.</p> <p>Reason: To ensure appropriate drainage infrastructure to comply with policy CS13 of the Local Plan Core Strategy (Adopted September 2012) The Sustainable Design and Construction SPD (adopted April 2013) and Policies 5.13 and 5.14 of the London Plan FALP (March 2015).</p>

Informatives:

1	<p>“In accordance with Reg 3 (4) and Reg 8 (2) of the Town and Country Planning (Environmental Impact Assessment) Regulations 2011, it is considered that:</p> <p>this submission of reserved matters reveals , with regard to the subject matter of the application, that there are no additional or different likely significant environmental effects than is considered in the environmental information already before the Council (the Environmental Statement (ES) (BXC02) submitted with the Section 73 application (F/04687/13) and any further and/or other information previously submitted;</p> <p>and</p> <p>the environmental information already before the Council (the ES submitted with the Section 73 application, and any further and/or other information previously submitted) remains adequate to assess the environmental effects of the development.”</p>
----------	--

This page is intentionally left blank

APPENDIX 2

Analysis of Scheme Compliance with Regional and Local Planning Policy

Table 2.1: Analysis of the proposals compliance with London Plan (March 2015) Policies

Policy	Content Summary	Extent of compliance and comment
Policy 1.1 (Delivering the strategic vision and objectives for London)	Strategic vision and objectives for London including managing growth and change in order to realise sustainable development and ensuring all Londoners are able to enjoy a good and improving quality of life. Improving environments which are easy, safe and convenient for everyone to access.	Compliant: As a London Plan Opportunity Area, the approved BXC scheme seeks to make the most of brownfield land to meet wider growth requirements in terms of housing, retail and commercial activities in a location accessible by a range of transport modes. The provision of the Central Brent Riverside Park will assist in achieving the wider sustainable aims of the Plan and provide a new open space.
Policy 2.6 (Outer London: vision and strategy);	Work to realise the full potential of outer London and enhance the quality of life for present and future residents. Understand the significant difference in the nature and quality of neighbourhoods; improvement initiatives should address these sensitively and draw upon strategic support where necessary.	Compliant: The proposed development represents one of the most important opportunity areas in outer London. The BXC Opportunity Area represents a significantly underutilised area of accessible brownfield land in need of regeneration. More specifically, in relation to the current application, the Riverside Park will be an enhanced amenity space for visitors to the extended Brent Cross Centre. It will be an asset for the public and existing neighbourhoods.
Policy 2.13 (Opportunity Areas and Intensification Areas)	Support the strategic policy directions for the opportunity areas, and where relevant, in adopted opportunity area planning frameworks Support wider regeneration, including in particular improvements to environmental quality, and integrate development proposals to the surrounding areas especially for regeneration.	Compliant: With regard to the current application, the provision of enhanced Central Brent Riverside Park will set the context for amenity and open space provision for future phases of development. The Proposed enhancements are associated with the diverted River Brent and will be provided in three distinct phases. The Central Brent Riverside Park is Reach 2. The early delivery is to be completed as part of the Phase 1A(N) given the close linkage between the diversion of the River Brent and the provision of the enhanced infrastructure.
Policy 2.14 (Areas for regeneration)	Boroughs should identify spatial areas for regeneration and spatial policies to bring together regeneration.	Compliant: The BXC site is identified as a London Plan Opportunity Area (See Policy 1.1 above) in need of comprehensive regeneration and capable of accommodating significant housing, jobs and community infrastructure. The Central Brent Riverside Park will be developed as a Linear Park/ Green Corridor and contributes to the wider regeneration aims. The proposal solely relates to the provision works to the Riverside Park; however works tie in with the diverted River, the new Prince Charles Driver and the Living Bridge that connects the Shopping Centre to the Southern development within the wider Brent Cross Cricklewood Regeneration Area.

Policy	Content Summary	Extent of compliance and comment
Policy 2.15 (Town Centres)	Development proposal should promote access by public transport walking and cycling. Promote safety and security and contribute towards an enhanced public realm and links to green infrastructure.	Compliant: The proposed development would be generally accessible by various modes of transport. Access to the Riverside Park is restricted to no motor based traffic, with the exclusion of the Environment Agency maintenance vehicles.
Policy 2.18 (Green infrastructure: the multi-functional network of green and open spaces)	Development proposals should protect, promote, expand and manage the extent and quality of, and access to, London's network of green infrastructure.	Compliant: With regard to the current application, once completed the Riverside park will connect into the proposed sequence of open spaces lining different green spaces throughout Brent Cross, primarily by way of access to/from the Living Bridge which connects south via market square to Clitterhouse Playing Fields and Claremont Park.
Policy 3.1 (Ensuring equal life chances for all)	Development proposals should protect and enhance facilities and services that meet the needs of particular groups and communities. Proposals involving loss of these facilities without adequate justification or provision for replacement should be resisted.	Compliant: It is considered that the impact of the development on existing social infrastructure provision has been appropriately considered and that the proposed provision will be adequate to meet forecasted need in accordance with this policy.
Policy 3.2 (Improving health and addressing health inequalities)	New developments should be designed, constructed and managed in ways that improve health, promote healthy lifestyles and wellbeing of communities.	Compliant: The design of the development has been influenced by the desire to create a healthy environment. The proposal is found to be compliant with the objectives of this policy. Provision of a Riverside Park would provide an enhanced amenity space for visitors to the extended Brent Cross Centre and is in accordance with policy requirements.
Policy 5.10 (Urban greening); Policy 5.11 (Green roofs and development site environs)	Development proposals should integrate green infrastructure from the beginning of the design process to contribute to urban greening, including the public realm. Elements that can contribute to this including tree planting and soft landscaping Proposals should be designed to include site planting to deliver as wide a range of the objectives associated with such planting as possible.	Compliant: The wider development includes an increase of approximately 9 hectares of open space/public realm which will significantly contribute to the urban greening of BXC. This includes the provision of a range of spaces such as Nature Parks, City Gardens, Neighbourhood and Community Parks. It also included the proposed Central Brent Riverside Park; to be developed as a Linear Park/ Green Corridor and provide hard and soft landscaping, ecology and bio-diversity enhancements.
Policy 5.12 (Flood risk management); Policy 5.13 (Sustainable drainage)	Proposals must comply with the flood risk assessment and management requirements of set out in the NPPF and supporting Technical Guidance. Proposals should utilise sustainable urban drainage systems (SUDS) unless there are practical reasons for not doing so and should aim to achieve Greenfield	Compliant: The proposals have been based on a comprehensive flood risk assessment that has been previously accepted by the Environment Agency. It is therefore considered that the application complies with this policy. Furthermore, Conditions 44.2 - 44.4 of the s73 permission relate specifically to the diversion of the River Brent and associated works. This proposal

Policy	Content Summary	Extent of compliance and comment
	runoff rates and ensure that surface water runoff is managed as close to its source as possible. Drainage should be designed and implemented in ways that deliver other objectives of the London Plan.	has been based on initial technical work around the content of the above planning conditions and therefore complies with the policy.
Policy 6.1 (Strategic approach);	The Mayor will work with all relevant partners to encourage a high quality public realm where appropriate, a corridor-based approach should be taken to ensure the needs of street users and improvements to the public realm are co-ordinated.	Compliant: The proposed development will see the provision of a network of connecting different green spaces and will primarily allow access to/from the Living Bridge which connects the southern development via the market square to Clitterhouse Playing Fields and Claremont Park; and access to the Brent Cross shopping Centre.
Policy 6.9 (Cycling);	Proposals should identify and implement a network of cycle routes. Contribute positively to an integrated cycling network for London by providing infrastructure that is safe, comfortable, attractive, coherent, direct and adaptable and in line with the guidance set out in the London Cycle Design Standards (or subsequent revisions).	Compliant: The Area Wide Walking and Cycling Study and the Pedestrian and Cycle Strategy will ensure that a network of cycle and pedestrian routes is provided on a phase by phase basis as part of the scheme, including appropriate links between the site and adjacent communities. The developers will be required to fund appropriate improvements and mitigation measures in the light of these studies, as well as the relevant transport reports and the monitoring strategy. To access the Riverside Park walkway the proposal includes cycle channels at 4 locations to facilitate cycle users.
Policy 6.10 (Walking)	Development proposals should ensure high quality pedestrian environments and emphasise the quality of the pedestrian and street space by referring to Transport for London's Pedestrian Design Guidance. Promote the 'Legible London' programme to improve pedestrian way finding. Encourage the use of shared space principles, such as simplified streetscape, de-cluttering and access for all.	Compliant: The proposed development includes a combined pedestrian, cycle and Environment Agency maintenance route on the northern side of the diverted river and is to be a minimum of 6m wide. There is no requirement to provide access within Reach 2 to the southern section of the Riverside Park. Throughout the Riverside Park, there will be 4 locations where steps from the upper level drop down to meet the lower level pedestrian and cycle path.
Policy 7.1 (Lifetime Neighbourhood)	Development should enable people to live healthy, active lives; should maximize the opportunity for community diversity, inclusion and cohesion; and should contribute to people's sense of place, safety and security. Places of work and leisure, streets, neighbourhoods, parks and open spaces should be designed to meet the needs of the community at all stages of people's lives, and should meet the principles of lifetime neighbourhoods.	Compliant: A key objective of the BXC scheme is to create a sustainable and balanced community. The new and existing community will benefit from the enhanced Riverside Park.

Policy	Content Summary	Extent of compliance and comment
Policy 7.2 (An Inclusive Environment)	Development proposals should explain how following engagement with relevant user groups and the principles of inclusive design has been integrated into the proposed development; and how inclusion will be maintained and managed.	<p>Compliant: With regard to inclusive access a lift will be added from the Bus station and Living Bridge level to provide step free access to the Riverside Park; the exact location of the lift within the proposed identified zone and the specification and design of the lift, will be submitted and approved by the Council subject to consultation and agreement with the Consultative Access Forum.</p> <p>In addition, ramped access is provided to the Riverside Park by way of a graded route connecting a proposed upper walkway to the riverside park walkway.</p>
Policy 7.3 (Designing out crime)	Development proposals should reduce the opportunities for criminal behaviour and contribute to a sense of security without being overbearing or intimidating.	<p>Compliant:</p> <p>The proposed development will be subject to a proposed lighting strategy which will assist in designing out crime. Whilst lighting strategy details have been provided in the Design Development Report that accompanied the application the final detail has still to be agreed and is subject to planning conditions requiring the submission of relevant details.</p> <p>In addition, the draft Estate Management Framework includes a section on Safety in the Public Realm and notes that the Shopping Centre CCTV strategy extends to the Central Brent Riverside Park.</p>
<p>Policy 7.4 (Local character);</p> <p>Policy 7.5 (Public realm);</p>	<p>Buildings, streets and open spaces should provide a high quality design that has regard to the pattern and grain of the existing spaces in orientation, scale, proportion and mass. Contributes to a positive relationship between the urban structure and natural landscape features.</p> <p>Development should make the public realm comprehensible at a human scale, using gateways, focal points and landmarks as appropriate to help people find their way. Landscape treatment, street furniture and infrastructure should be of the highest quality, have a clear purpose, maintain uncluttered spaces and should contribute to the easy movement of people through the space.</p>	<p>Compliant: The urban design principles that underpin the BXC masterplan as set out in the Revised Design and Access Statement, Design Guidelines, DSF and Parameter Plans fulfil the key criteria of these policies to provide high quality buildings, streets and spaces. The development incorporates the principles of "Secured by Design"</p> <p>Compliant: The Riverside Park has been designed to ensure that it is accessible and user friendly.</p>
Policy 7.13 (Safety Security and Resilience)	Development proposals should contribute to the minimization of potential physical risks, including those arising as	<p>Compliant:</p> <p>The proposed development will be subject to a proposed lighting strategy which will assist in</p>

Policy	Content Summary	Extent of compliance and comment
to Emergency)	a result of flood and related hazards.	<p>designing out crime.</p> <p>In addition, the draft Estate Management Framework includes a section on Safety in the Public Realm and notes that the Shopping Centre CCTV strategy extends to the Central Brent Riverside Park.</p> <p>As regards risk associated with flooding, the management of Riverside Park sits with the developers and a requirement should be inserted in the Estate Management Framework to ensure that a strategy is developed to address public access to the access route at times of flood or imminent flood. By controlling access at times of risk this issue can be addressed.</p>
Policy 7.18 (Protecting open space and addressing deficiency)	The loss of protected open spaces must be resisted unless equivalent or better quality provision is made within the local catchment area. Replacement of one type of open space with another is unacceptable unless an up to date needs assessment shows that this would be appropriate	Compliant: The proposals will see the provision of an enhanced naturalised riverside park setting to replace the existing canalised River Brent where this sits within the Brent Cross area. The new park will provide an area of public open space that currently does not exist and will in addition provide improved ecology and biodiversity.
Policy 7.19 (Biodiversity and access to nature)	<p>Proposals should:</p> <ul style="list-style-type: none"> - Wherever possible make a positive contribution to the protection, enhancement, creation and management of biodiversity. - Prioritise assisting in meeting targets in biodiversity action plans and/or improve access to nature in areas deficient in accessible wildlife sites. - Be resisted where they have significant adverse impacts on the population or conservation status of a protected species, or a priority species or habitat identified in a biodiversity action plan. 	Compliant: One of the key principles of this proposal is to improve local bio-diversity within the Riverside Park environment. The creation of a marginal wetland environment associated with the river will help facilitate this aim. It is considered that the development will result in a net gain in biodiversity in terms of habitat quality and connectivity.
Policy 7.21 (Trees and woodlands)	Existing trees of value should be retained and any loss as a result of development should be replaced. Wherever appropriate the planting of additional trees should be included in developments.	Compliant: Tree and shrub planting will be introduced within the wider corridor, throughout the river channel. Amenity grass areas will be planted in a mix of general meadow and wildflower planting in addition to general amenity grass planting areas; these will all be located above the 1 in 100 year flood level to ensure they are less likely to suffer from water inundation. A detailed landscaping plan is included with the proposal.

Policy	Content Summary	Extent of compliance and comment
<p>Policy 7.27 (Blue Ribbon Network: supporting infrastructure and recreational use).</p>	<p>Development proposals should protect and improve existing access points to (including from land into water such as slipways and steps) or alongside the Blue Ribbon Network (including paths).</p>	<p>Compliant: With regard to the current application, the combined pedestrian/ cycle access and Environment Agency maintenance route situated to the north of the diverted river and runs through the Riverside park, is to be a minimum of 6m wide. The 6m wide access route is shown to incorporate planting in order to create a more sinuous route. To improve access and connect the proposal to the wider BXC development, there will be 4 locations where steps and a lift from the upper level drop down to meet the lower level pedestrian/cycle and EA Maintenance path. It is considered that the proposal meets the requirements of this policy.</p>
<p>Policy 7.28 (Restoration of The Blue Ribbon Network)</p>	<p>Development proposals should restore and enhance the Blue Ribbon Network by taking opportunities to open culverts and naturalise river channels, increasing habitat value and protect the open character of the Blue Ribbon Network.</p>	<p>Compliant: It is considered that the proposal enhances local habitat, ecology and biodiversity and makes a positive contribution to the area. The proposal is in accordance with this policy.</p>
<p>Policy 7.30 (London's canals and other river's and water spaces)</p>	<p>Development proposals along London's canal network and other rivers and water bodies (such as reservoirs, lakes and ponds) should respect their local character and contribute to their accessibility and active water related uses, in particular transport uses, where these are possible.</p>	<p>Compliant: It is considered that the Central Brent Riverside Park will contribute to the overall improvement of the habitat of rivers, brooks and streams in accordance with this policy. The Environment Agency has been consulted on the application and has engaged with the developers to inform the overall design and layout of the Park..</p>

Table 2.2: Analysis of the proposals compliance with Barnet's Local Plan Policies
(September 2012)

Policy	Content Summary	Extent of Compliance and Comment
Core Strategy		
CS NPPF (National Planning Policy Framework – presumption in favour of sustainable development)	Take a positive approach to proposals which reflect the presumption in favour of sustainable development and approve applications that accord with the Local Plan, unless material considerations indicate otherwise. Where there are no policies relevant to the proposal or the relevant policies are out of date permission should be granted, unless material considerations indicate otherwise.	Compliant: the proposal is considered to constitute sustainable development in accordance with the NPPF and which complies with Local Plan taken as a whole. It is therefore recommended for approval.
CS1 (Barnet's place shaping strategy – the three strands approach)	<p>As part of its 'Three Strands Approach' the council will:</p> <ul style="list-style-type: none"> - Concentrate and consolidate growth in well located areas that provide opportunities for development, creating a high quality environment that will have positive impacts. - Focus major growth in the most suitable locations and ensure that this delivers sustainable development, while continuing to conserve and enhance the distinctiveness of Barnet as a place to live, work and visit. - Ensure that development funds infrastructure through Section 106 Agreements and other funding mechanisms. - Protect and enhance Barnet's high quality suburbs. 	<p>Compliant: the proposal is considered to show the influence of this policy and demonstrates compliance with its key objectives.</p> <p>As an Opportunity Area in the Mayor's London Plan, the BXC scheme has been developed with the consideration that the site has significant capacity for new housing, commercial and other development linked to existing or potential improvements to public transport accessibility.</p> <p>The proposed development relates to matters reserved following the grant of planning permission in 2014. The proposed enhancements to Open Space associated with the diverted River Brent will be provided in three distinct phases. The current phase within this RMA relates to the Central Brent reach of the river, 'Reach 2'. The proposal will provide significant enhancements to the provision of open space and recreational facilities to serve the existing and future residents of the area.</p>
CS2 – Brent Cross - Cricklewood	<p>The Council will seek comprehensive redevelopment of Brent Cross – Cricklewood in accordance with the London Plan, the saved UDP policies (Chapter 12) and the adopted Development Framework. The Policy makes provision for the following:</p> <ul style="list-style-type: none"> • It is considered likely that comprehensive regeneration will be achieved in accordance with the planning permission. If this is not achieved, the Council will 	<p>The 2014 consent of the S73 application continues to ensure the comprehensive redevelopment of Brent Cross Cricklewood in accordance with this policy and the Saved UDP policies. The current Reserved Matters Application for the proposed Open Space developments at Clitterhouse Playing Fields and Claremont Park is consistent with the 2014 permission.</p> <p>The Central Brent Riverside Park area, or Reach 2, is to be completed as part of the Phase 1A(N) works to ensure that the setting of the Living Bridge and associated infrastructure is delivered at the outset.</p>

Policy	Content Summary	Extent of Compliance and Comment
	<p>consider whether in the circumstances the Local Plan needs to be reviewed.</p> <ul style="list-style-type: none"> • Specific monitoring indicators for Brent Cross – Cricklewood are set out in Appendix B of the Core Strategy On the basis of these indicators it is expected that comprehensive re-development will commence in relation to Phase 1 at some time between 2015 and 2017. • If these milestones are not achieved (or are not likely to be capable of being delivered) we will consider the possible need for a review of the Core Strategy Policy on Brent Cross – Cricklewood • The key milestone for the regeneration of Brent Cross – Cricklewood is likely to be the Phase 1 Compulsory Purchase Order (CPO). If by the end of 2014 any CPO that is required to deliver Phase 1 and commence the development has not been made and submitted for confirmation we will instigate a review of the policy framework for Brent Cross – Cricklewood. 	<p>The provision of significantly improved open spaces will provide facilities for existing residents in the area as well as future residents in subsequent years as new housing is brought forward.</p>
<p>CS5 (Protecting and enhancing Barnet's character to create high quality places)</p>	<p>The council will ensure that development in Barnet respects local context and distinctive local character, creating places and buildings with high quality design.</p> <p>Developments should:</p> <ul style="list-style-type: none"> - Address the principles, aims and objectives set out in the relevant national guidance. - Be safe attractive and fully accessible. - Provide vibrant, attractive and accessible public spaces. - Respect and enhance the distinctive natural landscapes of Barnet. - Protect and enhance the gardens of residential properties. - Protect important local views. - Protect and enhance the boroughs 	<p>Compliant: It is considered that the Section 73 approved application takes into account the requirements of different groups and sectors of the community, the Central Brent Riverside Park would be a quality open space for existing residents as well as future residents in the area.</p> <p>The proposed enhancements to Open Space associated with the diverted River Brent will be provided in three distinct phases. The current phase within this RMA relates to the Central Brent reach of the river, Reach 2 and the proposal is defined as a Linear park/ green corridor.</p> <p>A lighting strategy is proposed for Riverside Park however full design details associated with this strategy will be subject to a planning condition requiring the submission of these details prior to work commencing.</p> <p>The river channel associated with the diverted River Brent will provide flood defences for a 1:5 year event scenario. The channel, including the maintenance route will be allowed to flood naturally outwith this event scenario. It is intended that the</p>

Policy	Content Summary	Extent of Compliance and Comment
	<p>high quality suburbs and historic areas and heritage.</p> <ul style="list-style-type: none"> - Maximise the opportunity for community diversity, inclusion and cohesion. - Contribute to people's sense of place, safety and security. 	<p>Management Framework Agreement will ensure that access is restricted during flood events.</p> <p>The proposed development is consistent with approved Parameter Plan 011 (River Brent) and Parameter Plan 003 (Public Realm).</p>
CS7 (Enhancing and protecting Barnet's open spaces)	<p>Protection and enhancement of Metropolitan Open Space</p> <p>Create a greener Barnet by:</p> <ul style="list-style-type: none"> - Meeting increased demand for access to open space and opportunities for physical activity. - Improving access to open space in areas of public open space deficiency. - Securing improvements to open spaces including provision for children's play sports facilities and better access arrangements, where opportunities arise. - Maintaining and improving greening by protecting incidental spaces, trees, hedgerows and watercourses. - Protecting existing site ecology and ensuring development makes the fullest contributions enhancing biodiversity. - Enhancing local food production - Improve the green networks 	<p>Compliant. The delivery the Central Brent Riverside Park, or Reach 2, is to be completed as part of the Phase 1A(N) works. Provision of landscaping under this phase is appropriate given the close linkage between the diversion of the River Brent and the provision of new infrastructure and, the desire to provide a completed scheme within this location.</p> <p>The proposal will provide a significant enhancement and provide a valuable open space for both local residents and for future residents.</p> <p>Once completed the Riverside park will connect into the proposed sequence of open spaces lining different green spaces throughout Brent Cross, primarily by way of access to/from the Living Bridge which connects south via market square to Clitterhouse Playing Fields and Claremont Park.</p> <p>An extensive planting strategy and landscaping proposals, together with enhanced habitat and ecology areas further ensures that the development complies with this policy.</p>
CS9 (Providing safe, efficient and effective travel)	<p>Developments should provide and allow for safe effective and efficient travel and include measures to make more efficient use of the local road network.</p> <p>Major proposals should incorporate Transport Assessments, Travel Plans, Delivery and Servicing Plans and mitigation measures and ensure that adequate capacity and high quality safe transport facilities are delivered in line with demand.</p> <p>The council will support more environmentally friendly transport networks, including the use of low emission vehicles (including electric cars), encouraging mixed use development and seeking to make cycling and walking more attractive for</p>	<p>It is considered that the development complies with this policy; the cycle and pedestrian path through the Central Brent Riverside Park will connect into the wider and improved cycle and pedestrian network.</p>

Policy	Content Summary	Extent of Compliance and Comment
	leisure, health and short trips.	
CS10 (Enabling inclusive and integrated community facilities and uses)	The council will ensure that community facilities are provided for Barnet's communities and expect development that increases the demand for community facilities and services to make appropriate contributions towards new and accessible facilities.	Compliant: the proposed Central Brent Riverside Park is designed to cater for the wider community. In order to ensure that all relevant user groups' needs are met and in particular to address concerns raised regarding inclusive access, a lift will be added from the Bus station and Living Bridge level to provide step free access to the Riverside Park. The exact location of the lift within the proposed identified zone and the specification and design of the lift will be submitted to and approved by the Council subject to consultation and agreement with the Consultative Access Forum.
CS11 (Improving health and wellbeing in Barnet)	Will improve health and wellbeing in Barnet through a range of measures including supporting healthier neighbourhoods, ensuring increased access to green spaces and improving opportunities for higher levels of physical activity.	Compliant: although the Riverside Park serves to enhance the setting of the redevelopment of Brent Cross shopping Centre it will do so in a manner which provides an enhanced river corridor with improved ecology, biodiversity and visual amenity. The park environment will benefit visitors to the Shopping Centre as well as local residents. The park also provides a linear green corridor incorporating pedestrian and cycle access to enable residents to move west to east without having to access the shopping centre and remote from vehicular traffic.
CS12 (Making Barnet a safer place)	<p>The Council will:</p> <ul style="list-style-type: none"> - Encourage appropriate security and community safety measures in developments and the transport network. - Require developers to demonstrate that they have incorporated community safety and security design principles in new development. - Promote safer streets and public areas, including open spaces. 	<p>Compliant: a lighting strategy is proposed by the development partners. Although indicative details have been submitted with the current application full design details are still required and will be subject to the requirements of a planning condition. The lighting strategy will ensure the environment is secure.</p> <p>In addition, the Estate Management Framework Agreement advises that a team of SIA (Security Industry Association) accredited CCTV operators (closed circuit television) will operate an extensive system within Brent Cross shopping centre 24 hours of every day supporting the Duty Assistant team in providing a safe and secure environment.</p> <p>These systems will extend to the Central Brent Riverside Park and Living Bridge (surface).</p>
CS13 (Ensuring the efficient use of natural resources)	<p>The council will:</p> <ul style="list-style-type: none"> - Seek to minimise Barnet's contribution to climate change and ensure that the borough develops in a way which respects environmental limits and improves quality of life. - Promote the highest environmental 	Compliant: The proposed infrastructure development associated with the Riverside Park development will include a Sustainable Urban Drainage Strategy which will use detention basins within landscaped areas and oversized pipes below new roads to achieve a 25% reduction of the current 1:100 year return flow plus 30% for climate change in accordance with planning condition 44.5 of the

Policy	Content Summary	Extent of Compliance and Comment
	<p>standards for development to mitigate and adapt to the effects of climate change.</p> <ul style="list-style-type: none"> - Expect development to be energy efficient and seek to minimise any wasted heat or power. - Expect developments to comply with London Plan policy 5.2. - Maximise opportunities for implementing new district wide networks supplied by decentralised energy. - Make Barnet a water efficient borough, minimise the potential for fluvial and surface flooding and ensure developments do not harm the water environment, water quality and drainage systems. - Seek to improve air and noise quality. 	<p>s73 permission.</p> <p>Given the nature of the river channel the developers have advised that there is no scope within the site to accommodate surface water management specifically within this area. Separation and filtration mechanisms will be adopted to prevent pollution of the river channel from the maintenance route.</p> <p>Condition 44.5 of the S73 permission requires that SuDS provision within each phase or sub-phase will be integral to the site with a requirement that it be demonstrated that SuDS is maximised across the site. As the drainage arrangements for Phase 1AN seek to maximise SuDS across the Phase 1AN area officers are satisfied that the condition is met.</p>
<p>CS15 (Delivering the Core Strategy)</p>	<p>The council will work with partners to deliver the vision, objectives and policies of the Core Strategy, including working with developers and using planning obligations (and other funding mechanism where appropriate) to support the delivery of infrastructure, facilities and services to meet needs generated by development and mitigate the impact of development.</p>	<p>Compliant: the Section 73 permission was subject to a S106 Agreement. The current RMA sits under the S73 permission and the requirements of the s106 are still applicable. Where required, the s106 will be varied in order to secure deliver of specific elements within the current application.</p>
<p>Adopted UDP (May 2006) Saved Policies (May 2009) as referred to in Core Strategy Policy CS2 – Chapter 12 of the UDP: Cricklewood, Brent Cross and West Hendon Regeneration Area</p>		
<p>GCrick Cricklewood, Brent Cross and West Hendon Regeneration Area</p>	<ul style="list-style-type: none"> - The Council seeks integrated regeneration in the Cricklewood, Brent Cross and West Hendon Regeneration Area. - All development to the highest environmental and design standards - Aim to develop a new town centre over the plan period. 	<p>Compliant: the development is consistent with this strategic aim, falling as it does within the terms of the S73 permission.</p>
<p>C1 Comprehensive Development</p>	<ul style="list-style-type: none"> - The Council seeks the comprehensive development of the regeneration area in accordance with the area framework and delivery strategy. - Development proposals will need to meet policies of the UDP and their more detailed elaboration in the 	<p>Compliant: The scheme accords with this policy. The proposals are Phase 1A(N) proposals within the scope of the S73 permission, the first phase of development within the wider Brent Cross Cricklewood regeneration project.</p>

Policy	Content Summary	Extent of Compliance and Comment
	development framework.	
C2 Urban Design & Quality	-The Council will seek to achieve the standards of urban design for the Regeneration Area to result in a development of landmark quality.	Compliant: The LPA considers that the development will deliver a high quality environment in accordance with accepted principles of good urban design and this policy.
C3 Urban Design – Amenity	-The development should protect and improve the existing amenities of existing residents.	Compliant: The development will deliver an enhanced open space and provide improved access for residents whilst protecting residents' existing amenity.
C4 Sustainable Design	<p>The Council will seek to ensure that the Regeneration Area pursues the highest standards of environmental design, including:</p> <ul style="list-style-type: none"> • meet high performance standards for environmentally sustainable design and construction • an integrated network of public open spaces • Ensure the restoration and enhancement of the River Brent, without detrimental impact to the Brent Reservoir SSSI. • There must be a buffer zone, appropriate protection of legally protected species, and opportunities taken to enhance the biodiversity of the area. 	<p>Compliant: It is considered that the development will improve the open space, cycle and pedestrian networks in accordance with the planning conditions and obligations.</p> <p>The development will result in a net gain in biodiversity in terms of habitat quality and connectivity. Substantial improvements are proposed to the Central Brent Riverside Park and it is therefore considered that all criteria of this policy are met.</p> <p>A detailed landscaping scheme, habitat study and biodiversity strategy ensure that where required protected species will be protected and across the site the development will deliver enhanced habitat and ecology.</p> <p>Statutory consultees have not raised any objections to the proposal and the application is considered to demonstrate the influence of this policy.</p>
C7 Transport Improvements	<p>Transport improvements - the following should be provided through planning conditions and/or Section 106 agreements:</p> <p>i. Connections and improvements to the strategic road network.</p> <p>ii. Sufficient transport links to and through the development, to include at least one vehicular link across the North circular Road and one vehicular link crossing the railway to the Edgware Road.</p> <p>iii. A new integrated railway station and new integrated bus station at Cricklewood, linked by a rapid transport system to Brent Cross Bus Station and Hendon Central and/or Brent Cross Underground Stations on the Northern Line.</p> <p>iv. A new bus station at Brent Cross, to</p>	<p>To comply with this policy an integrated pedestrian and cycle path has been developed to facilitate connectivity through the development to connect into existing networks. The proposals will provide enhanced connectivity.</p> <p>Inclusive access is achieved by way of ramped access to the Riverside Park. The developers have in addition agreed to the provision of a lift to provide connection between the Living Bridge access point to the Riverside Park and Bus Station levels.</p>

Policy	Content Summary	Extent of Compliance and Comment
	<p>north of the North Circular Road, with associated improvements to the local bus infrastructure.</p> <p>v. An upgrade of the rail freight facilities.</p> <p>vi. Provision of an enhanced, rail-linked waste transfer station serving North London.</p> <p>vii. Priority measures for access to disabled persons, pedestrians, buses and cyclists throughout the Regeneration Area.</p>	
<p>UDP Specific Proposals (2006)</p> <p>Site</p>	<p>Parts of the BXC development site are subject to site-specific proposals as shown in the Proposals Map (2006) (as saved). The aspirations for these sites are set out below:</p> <ul style="list-style-type: none"> • Brent Cross New Town Centre (Site 31) – new town centre, comprising a mix of appropriate uses, improved public transport and pedestrian access, landscaping and diversion of the River Brent; • Cricklewood Eastern Lands (Site 37) – mixed use including residential, office, leisure, local and neighbourhood shops, education, community uses and open space; • New Railway Station Cricklewood (Site 38) – railway station and public transport interchange; • New Waste Transfer Station (Site 39) – waste handling facility. 	<p>Compliant: the proposal is consistent with the Proposals Map.</p>
<p>Cricklewood, Brent Cross, West Hendon Regeneration Area Development Framework SPD 2005.</p>		
<p>The Cricklewood, Brent Cross and West Hendon Regeneration Area Development Framework was adopted by the council and the Mayor of London as Supplementary Planning Guidance in 2005. This Development Framework was produced in collaboration with the Mayor and the Greater London Authority, landowners and developers in order to guide and inform the design and delivery of the development with the aim of achieving high quality comprehensive redevelopment of the area around a new sustainable mixed use town centre spanning the North</p>		<p>Compliance in relation to the proposed Open Space provision: The parameters and principles of the BXC scheme are considered to be in accordance with the principles set out in the guidance contained in the adopted Development Framework (2005) generally and taken as a whole.</p> <p>The general proposals for the Central Brent Riverside Park are consistent with the vision and aims of the Development Framework. A hierarchy</p>

Policy	Content Summary	Extent of Compliance and Comment
Circular Road.	The London Plan and the UDP saved policies combined with the Development Framework establishes a series of strategic principles for the comprehensive redevelopment of the area to create a new town centre, the overall vision for which is set out in UDP Policy GCrick.	<p>of open spaces will be provided across the Brent Cross Cricklewood regeneration project and the enhancements Riverside park will ensure compliance with those aims and will connect into the proposed network.</p> <p>The proposal shows how the provision of the Central Brent Riverside Park fits into the wider development, including the relationship of the proposed riverside park with the proposed infrastructure including the diverted River Brent and the relationship with proposed green corridors and cycle and footpath networks.</p>

Key relevant local and strategic supplementary planning documents

Local Supplementary Planning Documents and Guidance:

Sustainable Design and Construction (2013)
 Planning Obligations (Section 106) (April 2013)

Strategic Supplementary Planning Documents and Guidance:

Accessible London: Achieving an Inclusive Environment (April 2004)
 Sustainable Design and Construction (May 2006)
 London's Foundations SPG (March 2012)

APPENDIX 3

Pre Reserved Matters Conditions Under consideration with Relevance to the Reserved matters application 15/03315/RMA

Pre RMA Planning Reference	Description	Registration Date Status	
15/00660/CON	Illustrative Reconciliation Plan to clear condition 1.17 for Phase 1a (North) of S73 Planning Application Ref: F/04687/13 approved 23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area	02.02.2015	Under Consideration
14/08105/CON	Area Wide Walking and Cycling Study to address condition 1.20 of S73 Planning Application Ref: F/04687/13 approved 23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area.	19.12.2014	Under Consideration
14/08112/CON	Framework Servicing and Delivery Strategy to address condition 1.21 of S73 Planning Application Ref: F/04687/13 approved 23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area.	17.12.2014	Under Consideration
14/08111/CON	Servicing and Delivery Strategy for Sub-Phase 1A North to address condition 1.22 of S73 Planning Application Ref: F/04687/13 approved 23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area.	17.12.2014	Under Consideration
14/08110/CON	Pedestrian and Cycle Strategy for Phase 1A North to address condition 2.8 of S73 Planning Application Ref: F/04687/13 approved 23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area	17.12.2014	Under Consideration

15/00667/CON	Estate Management Framework to address condition 7.1 for Phase 1a (North) of S73 Planning Application Ref: F/04687/13 approved 23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area	02.02.2015	Under Consideration
14/08109/CON	Car Parking Management Strategy to address condition 11.1 of S73 Planning Application Ref: F/04687/13 approved 23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area	17.12.2014	Under Consideration
14/08108/CON	Phase Car Parking Standards and the Phase Car Parking Strategy for Sub Phase 1A North to address condition 11.2 of S73 Planning Application Ref: F/04687/13 approved 23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area	17.12.2014	Under Consideration
14/07897/CON	Existing Landscape Mitigation Measures in relation to Phase 1a North to address condition 27.1 of S73 Planning Application Ref: F/04687/13 approved 23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area	09.12.2014	Under Consideration
14/07896/CON	Tree Protection Method Statement in relation to Phase 1a North to address condition 27.2 of S73 Planning Application Ref: F/04687/13 approved 23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area	09.12.2014	Under Consideration
15/00668/CON	Acoustic Design Report to address condition 29.1 for Phase 1a (North) of S73 Planning Application Ref: F/04687/13 approved	02.02.2015	Under Consideration

	23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area		
15/00812/CON	Proposed Phase Transport Report for Phase 1 to address condition 37.2 of S73 Planning Application reference F/04687/13 approved 23/07/2014 for the Comprehensive Mixed Use redevelopment of the Brent Cross Cricklewood Area.	10.02.2015	Under Consideration

This page is intentionally left blank

APPENDIX 4

CONSULTATION RESPONSES

Residential Consultation Responses for Consultation Period Ending 10/07/2015

<u>Resident 1 Response:</u>	Officer Comments
<p>Objection to the planning and rejuvenation of the Whitefield Estate based on the S73 Contamination Assessment Report.</p> <p>(BXC 17 – Supplementary Section 73 Phase 1 Geo-Environmental Assessment & Geotechnical Development Report; Appendix F- Contamination Assessment Report and study.)</p>	<p>It is not clear from the letter of objection submitted what aspect of the Contamination Assessment is of concern in relation to the Whitefield Estate.</p> <p>A Contamination Assessment was submitted as part of the S73 Planning Permission Ref No: F/04687/13. Appendix F- Contamination Assessment Report forms part of the Supplementary Phase 1 Geo- Environmental Assessment, which was carried out to assess current ground investigations and summarises the contamination results obtained.</p> <p>Planning Conditions 31.1 – 31.7 of the Section 73 permission relate specifically to Remediation of Contamination, and are in place to protect human health, prevent any potential contamination and ensure necessary remediation at the site has been undertaken.</p> <p>Condition 31.1 is a Pre reserved matters condition which was submitted and approved for Phase 1A North, prior to submission of any reserved matters application. An initial framework and breakdown of areas of land for remediation along with schedules of earthworks and soil treatment activities relevant to each of these Remediation Zones or Sub-Zones have been provided for Phase 1A north.</p> <p>The Brent Terrace Triangles RMA in relation to Plots 53 and 54 has been determined and it is intended that the accommodation provided through this application will provide accommodation for Whitefield Residents displaced within Phase 1a (North). Planning Condition 1.10 (Residential Relocation Strategy) is currently under consideration. The aim of this Condition is to ensure the satisfactory relocation of Whitefield residents.</p>

Resident 2 Response:

The development will encourage vehicles to enter Oxgate Gardens in Dollis Hill and will increase the existing flow of traffic even further. Oxgate Gardens is a residential road with a primary school located nearby; It is already used as a cut through to avoid existing traffic lights and the proximity of the BXC development would increase noise and disturbances.

Parking is extremely tight and on daily basis the majority of spaces are used by the existing factory workers, leaving residents of Oxgate Gardens to park on surrounding residential streets. Residents of Oxgate Gardens are going to approach Brent Council to request parking permits or restrictions.

When the car parks are full or when people are unwilling or unable to pay for parking visitors will park on surrounding residential streets close to the development; this would have a further detrimental impact on parking conditions.

The installation of the traffic lights on Oxgate Gardens has caused a loss of privacy, and the proposed development will have a further impact.

Lorries during construction will use Oxgate Gardens to access the waste dump

Two Reserved Matters Applications are before this Planning Committee for consideration; and this particular RMA submission (RMA 15/02215/RMA) specially relates to the Central Brent Riverside Park. Whilst the objection letter refers to both RMA references, this issue is not dealt under this particular RMA and comments can be found under Appendix 6 in the committee report for the Reserved Matters Application 15/03312/RMA relating to Infrastructure works.

Please see answer above.

<p><u>Resident 3 Response:</u></p> <p>With reference to a newspaper article published in the Evening Standard on the 4th June 2015 titled '1300 people have died this year due to polluted air in the capital'; concerns have been raised whether the Brent Cross infrastructure development would create more pollution?</p> <p>Staples Corner and the surrounding areas currently suffer from large volumes of traffic. The proposed development would increase the traffic flows.</p> <p><u>Resident 4 Response:</u></p> <p>The key in the plans for the Central Brent Riverside Park describes the lower-level path as 'incorporating planting to create 4m wide sinuous path'. The plan seems to show a path that continuously varies in width. What is the maximum and minimum for the path excluding and including the planting?</p> <p>Cycling and walking should be segregated to reduce any potential conflict between user groups.</p>	<p>Please see answer above.</p> <p>The Section 73 permission requires that a 6 metre wide Environmental Maintenance Route is provided in the Central Brent Riverside Park. The path proposed in this Reserved Matter Application is generally a minimum of 6m in width; however in order to create a route of enhanced interest, areas of planting will be allowed to encroach into the path effectively reducing the width to 4m in places. This effective reduction in width has been discussed with the Environment Agency who has not expressed any concerns about the proposal. Officers consider the proposal acceptable.</p> <p>Since the path serves the purpose of the Environmental Agency, pedestrians and cyclists there is not sufficient room to provide a segregated route and so a shared pedestrian and cycle route is proposed.</p>
--	---

Resident 5 Response:

Objection to the governance of the planning service. The closing date for comments mentioned in your letter sent in July was later than the date your system stopped accepting comments. That makes you in breach to consult under the Town and Country Planning Act 2000, as amended. How many letters were sent out? And why does your Planning database system does not indicate when consultation is going to end, this is in breach to the Human Rights Act 1998, Section 6, Paragraph 1.

Please see the main report for details of the number of residents consulted. Consultation time periods comply with the statutory requirements and the consultation expiry dates are also available on the Councils website. Comments have also been accepted after the consultation expiry date.

Statutory Consultees and other interest groups Responses for Consultation Period ending 10/07/2015

<p><u>Transport for London Response:</u> Letter dated: 1st September 2015 TfL welcome measures to improve access between the new bus station and the Brent River Park agreed by the Council and the applicant</p>	<p>Noted</p>
<p><u>Consultative Access Forum (CAF) Response:</u> Letter dated: 28th August 2015: <u>Tempelhof Bridge ramp leading the Riverside Park:</u> Inserting a landing at the midpoint of each slope to provide space will improve the usability of the ramp. However, it is not possible from the drawings provided to assess the gradients and the handrails which are critical aspects of ramps. <u>Lift to the Riverside Park:</u> A lift from the Living Bridge/ Bus station level to access the Riverside Park would enhance access from the Lower Riverside Walkway, and beneficially for those who require step free access.</p>	<p>Officers comments: The gradient for the ramp from Tempelhof Bridge ramp leading to the Riverside Park is 1 in 21 with a short 'dog leg' landing. Details and specifications regarding handrails and other details aspects will be considered at detailed design stage.</p>
<p><u>Thames Water Response:</u> Letter dated: 6th July 2015 No objections raised and does not affect Thames Water</p>	<p>Noted</p>

<p><u>London Borough of Harrow Response:</u></p> <p>Letter dated: 28th July 2015 No objection raised</p>	<p>Noted</p>
<p><u>Environmental Agency Response:</u></p> <p>Letter dated: 25th August 2015</p>	<p>Comments regarding flood modelling and concerns raised by the Environment Agency have been captured; officer comments can be found under Appendix 6 in the committee report for the Reserved Matters Application 15/03312/RMA relating to Infrastructure works.</p>
<p>The Environmental Agency reviewed both Reserved Matter Applications from the perspectives of potential impacts on the ecological environment, the Water Framework Directive (WFD), flood risks and the risks of surface water pollution. It was considered that the current proposal results in a reduction in ecological buffer zone of 1217m² compared to the proposals approved under the Section 73 application.</p> <p><u>Flood Risk:</u></p> <p>The EA considered that the flood modeled will not cause any unacceptable increases in flood risk. However, noted that the modeling demonstrates high sensitivity to channel roughness ,and if future design changes are made this would impact the modeled channel roughness; to prevent any potential flood risk they have proposed the following condition:</p> <p>Condition 1</p> <p>The river channel will be constructed in such a way that the roughness values used in the current flood model are representative of the physical channel. Any changes to channel attributes that would make the current modeled roughness values unrepresentative will not be permitted without the prior written permission of the local planning authority.</p> <p>Reason</p> <p>To prevent increased flood risk</p>	

<p>At the detailed design stage, the EA have requested that the development should demonstrate that assets/ street furniture are fixed appropriately to the ground or walls; to prevent entering into the river during a flood event. To minimise this they have requested the following condition:</p> <p>Condition 2 Prior to the installation of bins, benches and bird boxes, full details of the design of these items will be submitted to and approved by the local planning authority.</p> <p>Reason To minimise blockages and pollution of the river during a flood event.</p> <p><u>Surface Water Pollution:</u> The Environmental Agency is satisfied with the evidence provided to allow natural continuity between the surface water in the new river channel and groundwater. However, had concerns that if the contaminated groundwater is not managed correctly this could enter the River system and effect the surface water; therefore proposed the following condition to request a detailed method statement to assess environmental safeguards.</p> <p>Condition 3 The development hereby permitted shall not be commenced until such time as a detailed construction method statement has been submitted to, and approved in writing by, the local planning authority. The construction method statement will demonstrate how pollution to surface water will be prevented.</p> <p>Reason To prevent pollution of surface water</p>	<p>Please see answer above.</p>
--	---------------------------------

This page is intentionally left blank

Appendix 5
Conformity with Revised Development Specification and Framework

Revised Design Specification and Framework (RDSF) and Parameter Plans (PP)	Requirement relevant to Infrastructure Reserved Matters	Compliance
Parameter Plan 002: Transport Infrastructure	<p>PP 002 identifies various infrastructure elements that are proposed as part of the site's comprehensive development including new highway junctions, primary routes and transport interchanges.</p> <p>More specifically the PP defines the parameters for road bridge clearance heights and the length of bridges and the general location of proposed road and pedestrian bridges including access points from the proposed Shopping Centre to the riverside walkway.</p>	<p>The proposed development is generally in accordance with the parameter plan. A through pedestrian route is shown running west to east through Riverside Park.</p> <p>Details of the roads and pedestrian bridges is covered by a separate RMA and therefore not included in the RMA for Central Brent Riverside Park (CBRP) however the provision of all these bridges does impact on the layout and design of the CBRP. There is in any event no conflict with the PP.</p>
Parameter Plan 003 Public Realm and Urban Structure	<p>PP 003 identifies a network of existing and proposed open spaces.</p> <p>The general location of Nature Parks 4 and 5 are shown on the PP and their respective area of NP4 is defined as 0.2ha</p> <p>The new River Brent Corridor is defined as RBC and its area defined as 3.1ha.</p> <p>The development zones for the proposed road and pedestrian crossings are included in the PP.</p> <p>An 'optional' acoustic barrier location is shown to protect the amenity of riverside park users.</p>	<p>The indicative locations of the Nature Parks are shown in the submitted plans however only the details for NP 5 is included in the CBRP RMA. No specific area for NP 5 is defined. The location of NP 5 is identified and the RMA submission complies with the general locational requirement.</p> <p>With regard to the new Riverside Park, changes introduced through Condition 2.4/2.5 reduce the park area to 3.0ha thereby ensuring that the development currently proposed is compliant with the PP.</p>

		<p>The road and pedestrian bridge locations are compliant with the revised PP although no detail is provided within this RMA; detail is however provided in 15/03312/RMA relating to the provision of Infrastructure.</p> <p>The ES FIR confirms an acoustic barrier is required as mitigation to the proposed development in relation to the amenity of users of the Central Reach of the Brent Riverside Park. Although the acoustic barrier impacts positively on the CBRP it is located outwith the scope of this RMA. The detailed design and specification of the acoustic barrier will be subject to separate approval under condition 29.10 of the Section 73 Consent.</p>
<p>Parameter Plan 011: River Brent</p>	<p>PP 011 relates specifically to the diversion of the River Brent and associated works and defines the proposed river corridor and the indicative river bed.</p> <p>The river corridor is dealt with in three separate distinct reaches, Reach 1 and 2 relating to the retail development and Reach 3 providing a more natural park environment.</p> <p>The PP includes an indicative river realignment plan.</p> <p>Environment Agency Maintenance Route and combined pedestrian/cycle path route to be a minimum of 6m</p> <p>Continuous planting required, including planting on both</p>	<p>The proposed river corridor is consistent with the PP.</p> <p>The development proposals identify the three reaches however only the provision of Reach 2 is provided in detail under this current application.</p> <p>The river realignment plan has been updated through changes made under Conditions 2.4 and 2.5 to accommodate design changes to the road network and is compliant.</p> <p>Although the river corridor has reduced in width as a result of widening of the realigned Prince Charles Driver (to meet Highways requirements) the river corridor is still compliant with the PP.</p>

	<p>river banks and under bridges.</p> <p>A series of stairs and ramps are proposed to connect the riverside walkway level with upper levels.</p> <p>Potential flood risk on the A406 at the eastern end of the development is discussed in the PP with a potential solution being the provision of a dedicated gradient pathway from the A406 to the River Brent to remove flood water from the road in times of flood.</p>	<p>The anticipated landscaping requirements within the PP are adhered to within the relevant RMA submission.</p> <p>The EA combine maintenance route/pedestrian and cycle route has been designed as a minimum 6m wide route to accord with the PP requirement. The design of the route has introduced in some areas a planted area between the route and the river channel, this in effect reduced the brushed concrete surface to a minimum of 4m with a maximum of 2m planted. The design of the route and the planted paved area has been discussed with EA who confirm that they are satisfied with the proposal; the planted area will be constructed as a 'grasscrete' type surface to allow EA vehicles to drive across the surface.</p> <p>With regard to planting, both sides of the river are planted however areas underneath bridges have reduced planting given the overshadow element, to some extent this is ameliorated by the perennial planting introduced along the access route and the proposals are considered acceptable.</p> <p>Access stairs and ramps are proposed as required. The parameter plan for the Riverside Park included a vertical access between the ground and upper ground levels, connecting the bus station and Living Bridge. Full details have not been provided however officers</p>
--	---	--

		<p>have negotiated with the applicants to ensure that the lift continues to the riverside walkway level. In any event the inclusion of the lift complies with the PP. The design detail of the lift is reserved by a recommended planning condition.</p> <p>The potential A406 flooding issue is proposed to be addressed via the provision of additional road gullies to allow water to drain to River Brent, this is in accordance with the PP. Full details to be provided under planning conditions.</p>
--	--	--

Appendix 6
Red Line BXC Phase 1AN



This page is intentionally left blank

LOCATION: Brent Cross Regeneration Area

REFERENCE: 15/05040/CON

Received: 01 June, 2015 **AGENDA ITEM 10**

Accepted: 01 June, 2015

WARD: Childs Hill, Golders
Green, West Hendon

Expiry: 27 July, 2015

APPLICANT: Brent Cross Development Partners

PROPOSAL: Submission under conditions 2.4 and 2.5 for revision of the Development Specification and Framework and Design and Access Statement attached to Section 73 permission reference F/04687/13 for the Comprehensive mixed-use redevelopment of the BXC Regeneration Area. Changes relate to the provision of Phase 1A (North) Infrastructure and Central Brent Riverside Park (Reach 2) Open Space, Landscaping, Access and Bio-diversity proposals together with minor consequential changes to the section 73 permission.

RECOMMENDATION

This application is recommended for **APPROVAL**.

1. APPLICATION SUMMARY

An application has been made under Conditions 2.4 and 2.5 to vary the Revised Development Specification and Framework (RDSF), including the Parameter Plans and Revised Design and Access Statement (RDAS) as approved under Section 73 application F/04687/13 ("2014 S73 Consent). The minor changes to the aforementioned documents are consequential modifications brought about through development design changes to the Phase 1A (North) RMAs, specifically, but not exclusively in relation to the provision of site wide Infrastructure (reference 15/03315/RMA) and the provision of open space, landscaping, access arrangements, biodiversity and design of the Central Brent Riverside Park (reference 15/03312/RMA).

In addition to the changes to the current RMAs before Members at this Committee the application under Conditions 2.4 and 2.5 seeks to make minor variations to the 2014 S73 Consent which relate to the suite of Phase 1A (North) RMAs.

Justification for the proposed modifications has been established under assessment of the two Reserved Matters Applications against the Development Plan and in respect of their likely environmental impacts under the EIA Regulations. The modifications have also been assessed in relation to the 2014 S73 Consent and its attendant supporting documents and are found to be satisfactory.

2. DESCRIPTION OF THE SITE, SURROUNDINGS AND PROPOSAL

2.1 Site Description and Surroundings

The 151 hectare application site is defined to the west by the Edgware Road (A5) and the Midland Mainline railway line and to the east by the A41 and is bisected east to west by the A406 North Circular Road. It is adjacent to Junction 1 of the M1 (Staples Corner) and includes the existing Brent Cross Shopping Centre and Bus Station to the north of the A406. The River Brent runs east to west through the northern development area through a concrete lined channel.

To the south of the North Circular Road the area contains the Brent South Shopping Park, existing Tesco store and Toys 'R' Us store, the Whitefield estate (approximately 220 units), Whitefield Secondary School, Mapledown Special School and Claremont Primary School, Hendon Leisure Centre, Brent Cross London Underground Station to the east; Clarefield and Claremont Parks and Clitterhouse Playing Fields (Metropolitan Open Land), the Hendon Waste Transfer Station, Claremont Way Industrial Estate and Cricklewood Railway Station to the far south.

The application site can be described as under-utilised urban development

land and the regeneration proposals will see significant investment and development throughout the development site with improved infrastructure and enhanced open spaces and biodiversity. The Infrastructure RMA that this application primarily relates to is site wide whereas the Riverside Park RMA relates solely to Central Reach 2 of the diverted River Brent corridor. Previous Phase 1A (North) RMAs in relation to Clitterhouse Playing Fields and Development Plots 53 and 54 were approved by Members in May and June 2015.

2.2 Proposals

The 2014 S73 Consent is supported by a suite of documents which together provide the parameters, principles and controlling framework to facilitate delivery of the Brent Cross Cricklewood Regeneration Scheme. The 2014 S73 Consent is also subject to a S106 Agreement which, along with relevant Planning Conditions, provides a further layer of control for the implementation of the development.

Condition 1.16 of the 2014 S73 Consent requires all RMAs to be in accordance with the parameters and principles contained in the RDSF, the RDAS and Revised Design Guidelines (RDG). It was however recognised in the permission that as detailed designs are developed in the form of RMAs it may be necessary to make minor revisions to these documents, subject to obtaining approval from the local planning authority

Conditions 2.4 and 2.5 of the 2014 S73 Consent are therefore structured to specifically enable changes to the RDSF, RDAS and RDG subject to confirmation that the proposed amendments will not give rise to significant adverse environmental effects.

The current RMA submissions have been subject to a lengthy process of consultation and engagement with the local planning authority and statutory consultees to ensure that the evolving design is in accordance with the overarching aims and vision of the Development Plan, the 2014 S 73 Consent and its associated supporting documents, and that it is to all intents and purposes acceptable in its form. The development must also demonstrate that the evolving design has been assessed in terms of its environmental impact and that any changes have been assessed against the original Environmental Impact Assessment to ensure that changes are captured and assessed accordingly.

2.3 Revisions under Condition 2.4

RDSF Parameter Plan 002: Transport Infrastructure and Parameter Plan 003: Public Realm and Urban Structure

The 2014 S73 Consent Parameter Plan 002 states that Primary Routes are to provide a maximum building line to building line width of 28m and a minimum

of 13m, of which pedestrian footways will be a minimum of 3m wide. The supporting text to Parameter Plan 003 states that main connections will have a minimum width of 10m between buildings.

This approach assumed that in all instances buildings would immediately frame Primary Routes. However, as a result of the detailed design process, it is clear that in certain locations building will not always frame these routes. For example, the location of the Living Bridge southern approach buildings do not frame both sides of Claremont Avenue and, as a result, the width of road exceeds the maximum building line to building line threshold.

In order to address this issue it is considered that it would be more appropriate to measure the width of the proposed route from back of pavement to back of pavement as the main purpose of the control is to ensure the route (including road, pedestrian and cycle facilities) is of sufficient width, rather than control the location of buildings, which is managed by other parameters.

The RDSF also states that along Primary Routes pedestrian footways will be a minimum of 3m wide. Claremont Avenue for example provides a pedestrian footway of between 4m and 2.5m which in places is less than the minimum width. However, the proposed width of 2.5m is considered to be acceptable given that it can accommodate pedestrian flows in excess of those likely to be observed along the route. In addition, Tempelhof Avenue typically has a 4.5m segregated footway/cycleway and a 2.5m pedestrian footway. The width of the pedestrian footway is below the minimum parameter, although the proposed width of 2.5m is considered to be an acceptable width given that it would be able to accommodate pedestrian flows.

In all cases it is considered that the extent to which the various routes deviate from the approved parameters can be justified. Consequently, it is proposed to amend the wording of paragraph 5 of the supporting text to Parameter Plan 002 as follows (deleted text struck through and revised text in bold underlined):

*“Main roads will have a maximum building line to building line **back of pavement to back of pavement** width of 28m **(aside from Tempelhof Avenue which is to have a maximum width of 34m and Claremont Avenue which is to have a maximum width of 31m)** and a minimum of 13m, of which pedestrian footways will be a minimum of **2.5m** ~~(2-3m)~~ **(apart from Tempelhof Link Road which is to have a minimum pedestrian width of 2m)**”*

Note that Tempelhof Link Road has an existing approved footway width of 2m in the Section 73 Consent.

Bridge Structure B1

Proposed bridge structure B1 provides a link over the A406 North Circular to connect Market Quarter and Brent Cross East and West Zones and is the

western most bridge that crosses the riverside park and western leg of the western roundabout. Paragraph 4.6 of the approved RDSF identifies the principal characteristics for the detailed design of the bridge to include 'intermediate piers, none of which will be located between individual lanes of the A406.' Four piers were proposed in the 2014 S73 Consent drawings.

Following the design review of the bridge in discussion with LBB it is now proposed to provide 3 piers with a pier being removed at Tilling Road. The southern abutment has consequently been brought further north reducing the span between Pier 3 and the southern abutment. As a result it is proposed to amend paragraph 4.6 of the RDSF to read as follows:

'...3 or 4 intermediate piers, as shown in Approval in Principles plans and none of which will be located between individual lanes of the A406, as shown on the Approval in Principle plans.'

River Brent Bridges, Access Ramps and Lift Access

The S73 Consent provides consent for up to 4 vehicular bridges across the central section of the River Brent, and pedestrian and cyclist only bridges across the western section in addition to the vehicular bridge structures which form part of the eastern and western internal roundabout. The general locations of these bridges are shown on Parameter Plan 011. The Living Bridge is also included as a pedestrian/cycle route.

Paragraph 4.23 of the RDSF identifies that the pedestrian/cyclist bridges will have length, width and height thresholds of 15 - 30m, 4 - 8m and 600mm – 2,000mm (above the 100 yr plus climate change water level) respectively, and that the vehicular bridges will have length, width and height thresholds of 20 - 48m, 8 - 26m and 600mm – 1200mm (above the 100 yr plus climate change water level) respectively.

As a result of design development it has been identified that it will be necessary to build the vehicular and pedestrian/cyclist bridges to a height which marginally exceeds the limits defined in the 2014 S73 Consent. The reduction in the width of the river corridor (to accommodate LBB's requirement for an increase in the size of Prince Charles Drive to address Highway safety concerns) has also reduced the length of a number of the bridges slightly beneath the defined minimum length threshold. Although the widths of the bridges are compliant over the centre of the river, so as to ensure that the bell mouths of the bridges are also within the thresholds it is proposed to make minor amends to the length, width and height parameters of the pedestrian/cycle bridges and the vehicular bridges respectively, as follows;

- 15 - 30m, 4 - 8m and 600mm – **2,600mm** (above the 100 yr plus climate change water level); and

- **16** - 48m, 8 - **39m** and 600mm – **3,600mm** (above the 100 yr plus climate change water level).

Parameter Plan 011 also identifies the proposed approximate crossing point levels for bridges 2 and 8 (the eastern most bridge on the western and eastern roundabouts). Through the design review process it has been established that the bridge crossing levels do not fully comply with the approximate figures identified on the plan. To address this, the following minor amendments to change the levels are proposed:

- Bridge 2 – change from 42.5m to **43.6m**;
- Bridge 8 – change from 43.0m to **44.10m**.

Parameter Plan 011, paragraph 4.25, identifies principles which apply to the access ramps to the River Brent bed including:

- width of 4m clearance i.e. allowing for lockable infill panel gate at top of ramp with 4 metres internal clearance; and
- Minimum height clearance 4m.

Through the design development process it has been agreed with the Environment Agency (EA) that owing to the constraints of the river channel, the width of the access ramp clearance can be reduced to **3.5m**. In addition, as agreed with the EA, the height clearance has been amended to provide for a clearance of between **3.1m and 4.4m**.

Officers have also engaged with the applicants to ensure that inclusive access opportunities to the riverside walkway can be maximised. The provision of the ramped access alone was deemed to be insufficient provision given its location some distance from the Living Bridge and Bus Station indicative access points to the walkway. The applicants have therefore agreed that a lift will be provided in the River Brent Central zone (Reach 2) to allow access between the lower level riverside walkway and the lower ground level (bus station level). The annotation on Parameter Plan 011, specifically River Re-alignment Plan 1, has therefore been modified to reflect the lift provision and now reads:

*‘vertical connection between **lower level riverside walkway**, lower ground level and upper ground level.’*

In order to address the above points consequential amendments are made to paragraphs 4.23 and 4.25 of the RDSF.

Brent Riverside Park and Nature Parks

The detailed design process for the road infrastructure has resulted in a roadway being driven through the western internal roundabout to the replacement A406 Templehof Bridge. This roadway is to be delivered as an

embankment structure and cuts through an area defined in the 2014 S73 Consent as Nature Park NP5 as defined on Parameter Plan 003. In order for the proposed section of road to be accommodated it is necessary to reduce the area of NP5 from 0.2ha to 0.13ha. This is addressed in the Central Brent Riverside Park RMA report (15/03315/RMA) also before the Members at this committee.

In order to compensate for the loss of 0.07ha from Nature Park NP5 it is proposed to increase the area of Nature Park NP4, which is within the western section of the River Brent Corridor, from 0.2ha to 0.27ha. In addition to this minor amendment, as a result of the widening of Prince Charles Drive, the river corridor within the central section (Reach 2) has been slightly reduced in size by 0.1ha. This reduction is compensated via a financial contribution of £200,000 which has been agreed with the Environment Agency and is discussed in more detail in the Central Brent Riverside Park RMA report (reference 15/03315/RMA) also before the Members at this committee.

The total area of the Brent Riverside Park has therefore reduced from 3.1ha to 3.07ha when the increased area of NP4 is factored in.

In light of the above changes it is proposed that Table 5, within page 43 of the RDSF, as well as Table 4 in Appendix 2 will be amended to reflect these changes. In addition, Nature Park NP5 was previously omitted from Table 4 and this omission has been rectified through this submission.

The updated tables are included at Appendix 2 of this committee report.

2.4 Revisions under Condition 2.5

As described above, the size of Nature Park NP5 is to be reduced and Nature Park NP4 increased to ensure there is no net loss as a result of the design changes to the highway layout. Consequential changes are therefore required to be made to Section A2.6 of the RDAS, which is concerned with Landscape and Public Realm. Paragraph A2.6.2 identifies the Open Space Hierarchy and currently refers to Nature Parks ranging in size from 0.2-0.5ha. This is to be updated to read as 0.1-0.5ha to reflect the aforementioned proposals.

2.5 Minor Consequential Changes

As a result of the ongoing development design process, the following minor updates and revisions are proposed:

- Minor updates to the quantum of existing open space at Clitterhouse Playing Fields and the Whitefield Estate following the discharge of Condition 2.3 (Ref No: 14/07888/CON) are required;
- Update reference to Highway Plan revision number P/D111870/H/100/1007 from Rev C to Rev F regarding the A406

Ingress/Egress Junction (as related to the current Section 96a application Ref No: 15/01038/NMA);

- Updates to the revision numbers of Parameter Plans 006, 011, 012, 014 and 016 (as related to the current Section 96a application Ref No: 15/01038/NMA); and
- Minor updates to the replacement units within the Whitefield Residential Estate to reflect the Reserved Matters Approval Ref No: 15/00720/RMA and the current application under Condition 1.10 (Ref No: 15/01038/NMA) regarding the residential relocation Strategy.

3. MATERIAL CONSIDERATIONS

Consideration of applications under Conditions 2.4 and 2.5 for variations to the RDSF or the RDAS need to also consider the likelihood of such changes having a material impact on the findings of the October 2013 Section 73 ES submitted with Section 73 application F/04687/13.

The applicants have considered the changes proposed in the current application against the terms and content of the approved Environmental Statement as well as the subsequent Environmental Statement: Further Information Report (ES FIR) that was submitted to assess design detail within all the Phase 1A (North) Reserved Matters Applications. Additional revisions have been made to the ES FIR to reflect matters raised by officers and statutory consultees in relation to the RMA proposals. The applicants have concluded that the RMAs do not have any new or additional significant adverse impact beyond those previously assessed in the original 2014 S73 permission Environmental Statement.

The changes proposed in the current application under Conditions 2.4 and 2.5 have similarly been assessed against the previous Environmental Statement and found not to incur any additional impacts.

There is one exception to the foregoing statement and that relates to the loss of biodiversity habitat in Nature Park 5 and the overall reduction in the area of Riverside Park. With regard to the impact of the design changes to NP 5 the applicants have determined that the marginal loss of biodiversity land can be mitigated through a like for like increase in the area of Nature Park 4 which is located within Brent Riverside Park to the west. This is addressed in the Central Brent Riverside Park RMA and this report.

In addition, the reduction in available open space and marginal wetland biodiversity associated with the diverted River Brent has led to the applicants agreeing with the Environment Agency compensatory payment of £200,000 which will be utilised by the EA to improve biodiversity out with the BXC application site but still within the Borough of Barnet.

Officers accept that the modifications discussed within this Report will not result in any significant environmental effects not previously identified in the 2014 S73 Consent nor would the changes materially affect the outcome of the assessments or mitigation measures presented in the Section 73 ES. The modifications have also been considered within the scope of the Phase 1A (North) RMA ES Further Information Report.

In accordance with Reg 3 (4) and Reg 8 (2) of the Town and Country Planning (Environmental Impact Assessment) Regulations 2011, it is considered that under this submission there are no additional or different likely significant environmental effects than those that have been considered in the environmental information already before the Council (the Environmental Statement (ES) (BXC02) submitted with the Section 73 application (F/04687/13) and any further and/or other information previously and subsequently submitted.

4. CONCLUSION

It is not considered that the proposed variations under conditions 2.4 and 2.5 will materially affect the findings of the October 2013 Section 73 Environmental Statement and that they have been properly addressed under the revised ES Further Information Report.

The changes proposed result from extended pre and post submission engagement between the Council, statutory consultees and the developer in order to secure the best design solutions to the proposed infrastructure and design of the Central Brent Riverside Park. The changes ensure that access is more inclusive in relation to the Riverside Park.

The proposed changes are therefore recommended for approval in line with the recommendation for approval of the Reserved Matters Applications relating to Infrastructure, 15/03315/RMA, and Central Brent Riverside Park, 15/03312/RMA.

This page is intentionally left blank

APPENDIX 1

Wording of Conditions 2.4 and 2.5

CONDITION 2.4 AND CONDITION 2.5 OF F/04687/13

- 2.4 The DSF shall be revised by the Developer (subject to obtaining approval in accordance with this Condition) from time to time in order to incorporate approved revisions into the Reconciliation Mechanism reflecting any changes brought about through:
- 2.4.1.1 Reserved Matters Approvals, Other Matters Approvals or best practice guidance, or any other matters; and/or
 - 2.4.1.2 any Further Section 73 Permission and/or Alternative Energy Permission and/or any Additional Planning Permission; and/or
 - 2.4.1.3 any consequential changes as a result of any approved variation of the Phases in accordance with condition 4.2.
- 2.4.2 Any application for a proposed revision pursuant to condition 2.4 will be determined in accordance with the requirements of the EIA Directive.
- 2.4.3 The development of each Plot or other part of the Development approved thereafter shall be designed and carried out in accordance with such approved revised Development Specification and Framework.

Reason: To ensure the ongoing implementation of comprehensive, high quality urban design within the overall scheme in accordance with the parameters and principles which are approved in this permission.

- 2.5 The Design and Access Statement and Design Guidelines shall be revised by the Developer (subject to obtaining approval in accordance with this Condition) from time to time to incorporate approved revisions into the Reconciliation Mechanism reflecting any changes brought about through:
- 2.5.1.1 Reserved Matters Approvals, Other Matters Approvals or best practice guidance, or any other matters; and/or
 - 2.5.1.2 any Further Section 73 Permission and/or Alternative Energy Permission and/or any Additional Planning Permission and/or
 - 2.5.1.3 any consequential changes as a result of any approved variation of the Phases in accordance with condition 4.2.
- 2.5.2 Any application for a proposed revision pursuant to condition 2.5 will be determined in accordance with the requirements of the EIA Directive as appropriate.
- 2.5.3 The development of each Plot or other part of the Development shall thereafter be designed and carried out in accordance with such approved revised Design and Access Statement.

Reason: To ensure the ongoing implementation of comprehensive, high quality urban design within the overall scheme in accordance with the parameters and principles which are approved in this permission.

APPENDIX 2

Updated Table 4 and Table 5 to the RDSF

Table 4: Open Space Provision

Category	Reference	Minimum Size/Width	Development Zone	Characteristics
Community Park	CP1	18.2 ha	Clitterhouse Playing Fields	Clitterhouse Playing Fields
Neighbourhood Parks	NH1	1.2 ha	Eastern Lands	Eastern Park
	NH2	1.95 ha	Market Quarter	Claremont Park
	NH3	2.1 ha	Brent Terrace	Brent Terrace Park
	NH4	0.7 ha	Brent Cross East	Sturgess Park
City Gardens	CG1	0.8 ha	Station Quarter	Office District Park
	CG2	0.48 ha	Brent Terrace	Millennium Green
	CG3	0.15 ha		Living Bridge
Nature Parks	NP1	Within CP1	Clitterhouse Playing Fields	Clitterhouse Stream Nature Park
	NP2	0.42 ha	Railway Lands	Railway Lands Nature Park
	NP3	0.2 ha	Station Quarter	Northern Nature Park
	NP4	Within RBC (0.27ha) 8-26 ha	Brent Cross West East	River Brent Nature Park
	NP5	0.12 ha	Brent Cross East	River Brent Nature Park
Main Squares	M1	0.87 ha	Station Quarter	Station Square
	M2	0.884 ha	Market Quarter	Market Square
	M3	0.354 ha	Brent Cross East	Brent Cross Square
Squares	S1	0.19 ha	Station Quarter	Community Square
	S2	0.16 ha	Brent Terrace	Gas Governor Square
	S3	0.16 ha	Cricklewood Lane	Cricklewood Station Square
	S4	0.15 ha	Adjacent to Eastern Lands	Brent Cross LUL Square
	S5	0.26 ha	Eastern Lands	School Square
	S6	0.51 ha	Station Quarter	Tower Square
	S7	0.13 ha	Eastern Lands	Whitefield Square
	S8	0.05	Brent Cross East	Fenwick Place
	S9	0.03	Brent Cross East	Templehof Circus
	S10	0.05	Brent Cross East	Layfield Place
Green Corridors	GC1	1.43 ha	Eastern Lands	Eastern Lands Corridor
	GC2	8 – 30 m	Station Quarter	North Circular Corridor
	GC3	Within CP1	Clitterhouse Playing Fields	Clitterhouse East Corridor
	GC4	Within CP1	Clitterhouse Playing Fields	Clitterhouse North Corridor
	GC5	Within CP1	Clitterhouse Playing Fields	Clitterhouse South Corridor
	GC6	3m (width)	Brent Terrace	School Corridor
	GC7	3m (width)	Brent Terrace	Brent Terrace Corridor
	GC8	Within NH3	Brent Terrace	Brent Terrace Park
R.Brent Corridor	RBC	3.07-3.48 2.7 ha Linear	Brent Cross East & West	Brent Riverside Park

Table 5: A schedule of existing and proposed open spaces

Location (Ref on Parameter Plan 003)	Existing Area (ha)	Proposed Area (ha)	Net Gain or Loss
Large Open Spaces			
Clitterhouse Playing Fields (incl. Clitterhouse Stream Nature Park NP1) (CP1)	17.83 17.83	18.20	+ 0.67 0.67
Medium Open Spaces			
Clarefield Park	2.0	-	- 2.0
Eastern Park (NH1)	-	1.20	+ 1.20
Claremont Park/Claremont Way Open Space (NH2)	2.30	1.95	- 0.35
Brent Terrace Park (NH3)	-	2.1	+ 2.1
Sturgess Park (NH4)	0.70	0.70	=
Eastern Lands Green Corridor (GC1)	-	1.43	+ 1.43
Brent Riverside Park (incl. River Brent Nature Park NP4) (RBC)	-	3.07 3.102.70	+3.07 3.102.70
Small Open Spaces			
Whitefield Sports Facilities	1.10	Within CP1	- 1.10
Whitefield Estate Amenity Space	1.30 1.49	-	- 1.30 1.49
Station Square (M1)	-	0.87	+ 0.87
Market Square (M2)	-	0.68 0.84	+ 0.68 0.84
Brent Cross Main Square (M3)	-	0.35 0.34	+ 0.35 0.34
Community Square (S1)	-	0.19	+ 0.19
Gas Governor Square (S2)	-	0.16	+ 0.16
Cricklewood Station Square (S3)	-	0.16	+ 0.16
Brent Cross LUL Station Square (S4)	-	0.15	+ 0.15
School Square (S5)	-	0.26	+ 0.26
Tower Square (S6)	-	0.51	+ 0.51
Whitefield Square (S7)	-	0.13	+ 0.13
Fenwick Place (S8)	-	0.05	+ 0.05
Templehof Circus (S9)	-	0.03	+ 0.03
Layfield Place (S10)	-	0.05	+ 0.05
Railway Lands Nature Park (NP2)	-	0.42	+ 0.42
Northern Nature Park (NP3)	-	0.20	+ 0.20
River Brent Nature Park (NP4)	-	Within RBC 0.37ha 0.36	- 0.36
River Brent Nature Park (NP5)	-	0.13	- 0.13
Office District Park (CG1)	-	0.60	+ 0.60
Millennium Green (CG2)	0.43	0.48	+ 0.05
Living Bridge (CG3)	-	0.15	+ 0.15
	05.48 05.49	34.22 34.22 33.76	

Key
Black
Text –

Approved DSF (2010)

Red Text – Amendments to DSF approved through Section 73 (2013)

Green Highlighting – Proposed amendments sought through Condition 2.4

Location Paul May Residential Home 17 Dukes Avenue London N3 2DE

AGENDA ITEM 11

Reference: 15/03759/S73

Received: 17th June 2015

Accepted: 7th July 2015

Ward: West Finchley

Expiry 1st September 2015

Applicant: Miss radlamah canakiah

Proposal: Variation of condition 2 pursuant to planning permission reference C09068 dated 25/06/1986 to read 'The premises shall be used only as supported temporary housing and shall not be used for any other purpose within Use Class C2 of the Town and Country Planning (Use Classes) Order 1987, or for any other purpose whatsoever, without the prior permission of the Local Planning Authority'

Recommendation: Approve subject to conditions

- 1 The development hereby permitted shall be carried out in accordance with the following approved plans: Additional Information (SENSITIVE), 1 of 2, 2 of 2, Planning Statement, Site Location Plan.

Reason: For the avoidance of doubt and in the interests of proper planning and so as to ensure that the development is carried out fully in accordance with the plans as assessed in accordance with Policies CS NPPF and CS1 of the Local Plan Core Strategy DPD (adopted September 2012) and Policy DM01 of the Local Plan Development Management Policies DPD (adopted September 2012).

- 2 The premises shall be used only as supported temporary housing and shall not be used for any other purpose within Use Class C2 of the Town and Country Planning (Use Classes) Order 1987, or for any other purpose whatsoever, without the prior permission of the Local Planning Authority'. After 10/09/2016, the premises shall be used only as an Old Peoples Home and shall not be used for any other purpose within Class C2 of the General Permitted Development Order 2015.

Reason: To enable the Local Planning Authority to maintain control of the use in the interests of neighbouring amenity and the character of the area.

- 3 Before the development hereby permitted is first occupied turning space and parking spaces shall be provided and marked out within the site in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority and that area shall not thereafter be used for any purpose other than the parking and turning of vehicles.

Reason: To ensure that parking and associated works are provided in accordance with the Council's standards in the interests of pedestrian and highway safety and the free flow of traffic in accordance with Policy CS9 of the Local Plan Core Strategy (adopted September 2012), Policy DM17 of the Local Plan Development Management Policies DPD (adopted September 2012) and 6.1, 6.2 and 6.3 of the London Plan 2011.

- 4 The rear staircase shall be used as a fire escape in the event of an emergency only and for no other purpose.

Reason: To safeguard neighbouring amenity.

- 5 When the premises cease to be occupied by Barnet Homes or at the end (of 1 year) whichever shall occur first, the use hereby permitted shall cease [and all materials and equipment brought on to the premises in connection with the use shall be removed].

Reason: To safeguard neighbouring amenity.

- 6 The premises shall not be used for the rehabilitation of persons with past or present dependence on alcohol or drugs, as a hospital, residential school, college or training centre.

Reason: To safeguard the amenities of neighbouring occupiers and enable the Local Planning Authority to maintain control over the use of the premises.

Informative(s):

- 1 In accordance with paragraphs 186 and 187 of the NPPF, the Local Planning Authority (LPA) takes a positive and proactive approach to development proposals, focused on solutions. The LPA has produced planning policies and written guidance to assist applicants when submitting applications. These are all available on the Council's website. A pre-application advice service is also offered. The LPA has negotiated with the applicant/agent where necessary during the application process to ensure that the proposed development is in accordance with the Development Plan.

Officer's Assessment

1. Site Description

The site property is a two storey semi-detached building on the east side of Dukes Avenue.

The surrounding area is predominantly residential in character. The site property is in use as a care home. Numbers 7, 27 and 33 are in use as flats. The remainder of properties are in use as single family dwellinghouses.

2. Site History

C09068 - Change of use to residential care home and erection of a single storey rear extension - Approved – 25/06/1986

C09068A - Single storey front and rear extensions. Roof extension incorporating front gable and rear dormer window to Residential Care Home. – Approved – 01/11/1995

3. Proposal

The proposals are for the variation of condition 2 pursuant to planning permission reference C09068 dated 25/06/1986 to read 'The premises shall be used only as supported temporary housing and shall not be used for any other purpose within Use Class C2 of the Town and Country Planning (Use Classes) Order 1987, or for any other purpose whatsoever, without the prior permission of the Local Planning Authority'.

The condition currently reads:

'The premises shall be used only as an Old People's Home and shall not be used for any other purpose within use class 14 of the Town and Country Planning Use Classes Order 1972 or for any other purpose whatsoever without the prior approval of the Local Planning Authority.'

The Use Classes Order has since been amended and the use as an Old Peoples Home now falls within Use Class C2.

4. Public Consultation

Consultation letters were sent to 77 neighbouring properties.

38 responses have been received, comprising 38 letters of objection.

The objections received can be summarised as follows:

- Applicant has not been transparent about what is proposed
- It is not clear from the application exactly what the change in use will entail or what its consequences will be; a change from an old age home to temporary housing is not clear.
- Application does not say who will be occupying the premises. It could be anyone applying for assistance with temporary housing.
- HMOs are known to have negative consequences for the surrounding neighbourhood.
- Dukes Avenue is a quiet residential street

- the use of number 17 as accommodation for a revolving group of new 6 short-term residents would have a substantial and detrimental effect on the residential amenity of its neighbours
 - It seems inappropriate to site this type of business premises in a quiet residential street
 - With a planned reduction of staff at the property, the application is even more flawed: having fewer staff to look after potentially dangerous lodgers seems like a recipe for disaster.
 - I object to the change of use on two grounds. Firstly, that permission should never have been granted for business use, as a nursing home, in a residential road.
 - Excluding those undergoing 'drug rehabilitation' for instance does not exclude drug users, just as 'ex-offenders' does not rule out current offenders.
- The range of people with difficult conditions or behaviour not excluded from the application is limitless and the application is therefore wide open to abuse.
- The large fire escape at the rear of No.17 directly overlooks the gardens of Nos. 19 and 15, and will inevitably be used as a smoking area in the new development - this will result in intrusion and an unacceptable loss of privacy.
 - Traffic and Parking
 - Current owner has no experience of running such a facility
 - The application says that there is an oversupply of residential care homes in Barnet but the Barnet Housing Strategy predicts a 41% increase in the elderly population which would reverse any perceived oversupply very quickly.

5. Planning Considerations

5.1 Policy Context

National Planning Policy Framework and National Planning Practice Guidance

The determination of planning applications is made mindful of Central Government advice and the Local Plan for the area. It is recognised that Local Planning Authorities must determine applications in accordance with the statutory Development Plan, unless material considerations indicate otherwise, and that the planning system does not exist to protect the private interests of one person against another.

The National Planning Policy Framework (NPPF) was published on 27 March 2012. This is a key part of the Governments reforms to make the planning system less complex and more accessible, and to promote sustainable growth.

The NPPF states that 'good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people'. The NPPF retains a presumption in favour of sustainable development. This applies unless any adverse impacts of a development would 'significantly and demonstrably' outweigh the benefits.

The Mayor's London Plan 2015

The London Development Plan is the overall strategic plan for London, and it sets out a fully integrated economic, environmental, transport and social framework for the development of the capital to 2050. It forms part of the development plan for Greater London and is recognised in the NPPF as part of the development plan.

The London Plan provides a unified framework for strategies that are designed to ensure that all Londoners benefit from sustainable improvements to their quality of life.

Barnet's Local Plan (2012)

Barnet's Local Plan is made up of a suite of documents including the Core Strategy and Development Management Policies Development Plan Documents. Both were adopted in September 2012.

- Relevant Core Strategy Policies: CS NPPF, CS1, CS4, CS5, CS9.
- Relevant Development Management Policies: DM01, DM02, DM04, DM09, DM17

The Core Strategy in Policy CS4: Providing quality homes and housing choice in Barnet, seeks to provide supported housing to maximise the independence of vulnerable residents including young people, people with disabilities, older people, homeless people and other vulnerable adults.

The Council's approach to development as set out in Policy DM01 is to minimise the impact on the local environment and to ensure that occupiers of new developments as well as neighbouring occupiers enjoy a high standard of amenity. Policy DM01 states that all development should represent high quality design and should be designed to allow for adequate daylight, sunlight, privacy and outlook for adjoining occupiers. Policy DM02 states that where appropriate, development will be expected to demonstrate compliance to minimum amenity standards and make a positive contribution to the Borough. The development standards set out in Policy DM02 are regarded as key for Barnet to deliver the highest standards of urban design.

DM04 states that proposals to locate development that is likely to generate unacceptable noise levels close to noise sensitive uses will not normally be permitted. Proposals to locate noise sensitive development in areas with existing high levels of noise will not normally be permitted. Mitigation of noise impacts through design, layout, and insulation will be expected where appropriate.

DM09 states that Housing proposals for older people should:

- help to meet an identified need;
- demonstrate that they will not have a harmful impact on the character and amenities of the surrounding area; and
- be within walking distance of local shops and easily accessible by public transport; and
- provide adequate communal facilities including accommodation for essential staff on site.

DM17 states that the council will expect development to provide parking in accordance with the London Plan standards, except in the case of residential development, where the maximum standards will be:

- 2 to 1.5 spaces per unit for detached and semi detached houses and flats (4 or more bedrooms);
- 1.5 to 1 spaces per unit for terraced houses and flats (2 to 3 bedrooms); and
- 1 to less than 1 space per unit for development consisting mainly of flats (1 bedroom).

Supplementary Planning Documents

Sustainable Design and Construction SPD (adopted April 2013)

- Provides detailed guidance that supplements policies in the adopted Local Plan, and sets out how sustainable development will be delivered in Barnet.

5.2 Main issues for consideration

The main issues for consideration in this case are:

- Whether the principle of the use is acceptable.
- Whether harm would be caused to the character of the general locality;
- Whether harm would be caused to the living conditions of neighbouring residents.
- Whether the proposals would harm highway and pedestrian safety.

5.3 Assessment of proposals

The report to committee is accompanied by an exempt report as the information contained within is considered to be protected under the Data Protection Act 1998.

- Whether the principle of the use is acceptable with regard to the character of the area.

The existing property is in use as an Old Person's Home. It is recognised that this is already a non-conforming use within the area which consists of residential dwellings and some flats. The proposals seek to amend this condition to allow other uses within C2 use class.

The proposals would result in the loss of designated old people's accommodation. There is no objection to this, as there has been a general shift in demand across the borough away from traditional old peoples home to extra care homes and independent living. The preamble to policy CS4 and CS11 of the Adopted Core Strategy support this approach, stating that: 'Our analysis shows there is an oversupply of traditional rented sheltered housing'.

Other possible uses within C2 use class include:

Residential care homes
hospitals
nursing homes
boarding schools
residential colleges
training centres.

It is considered that it would be necessary to add a further condition restricting certain types of use which may be harmful to the character of the area.

Under the proposed use, the property will not be occupied by ex-offenders, young people, rough sleepers or those undergoing drug rehabilitation. Furthermore, secure residential institutions are covered by a separate use class C2A and the proposals would not permit this.

Although it is recognised that the occupiers of the premises will be more transient in their nature than the existing residents it is not considered that this would cause material harm to the amenities of neighbouring residents.

It is considered that, given that previous use of the property, the proposed amendment to the condition would not be likely to cause materially greater harm the character of the area the existing use.

- Whether harm would be caused to the living conditions of neighbouring residents.

The existing building has six bedrooms, a kitchen, lounge, meeting room and a living room. The proposed internal changes are limited to the installation of a new door and a shower. The proposals would not involve any extension to the property.

It is recognised that the existing use to an extent is non-conformant within the area. However it is not considered that the proposals would result in a materially greater intensity of use, and as a result are unlikely to cause harmful noise and disturbance to neighbouring residents.

- Whether the proposals would harm highway and pedestrian safety.

The property benefits from an existing driveway to the front with capacity to park two vehicles. The surrounding streets are within a Controlled Parking Zone (CPZ).

5.4 Response to Public Consultation

- Applicant has not been transparent about what is proposed *It is considered that there is adequate information to assess the application. Conditions are suggested to ensure that the proposals do not harm the character of the area.*

- It is not clear from the application exactly what the change in use will entail or what its consequences will be; a change from an old age home to temporary housing is not clear. *Conditions are suggested to ensure that the use of the property does not harm the character of the area or neighbouring amenity.*

- Application does not say who will be occupying the premises. It could be anyone applying for assistance with temporary housing. *Conditions are suggested to ensure that the proposed use would fit acceptably within the character of the area*

- HMOs are known to have negative consequences for the surrounding neighbourhood. *The proposals are not for a HMO which falls in a different use class. The proposals are for C2 use class as a residential institution.*

- Dukes Avenue is a quiet residential street *Noted and this is considered in the main report*
- the use of number 17 as accommodation for a revolving group of new 6 short-term residents would have a substantial and detrimental effect on the residential amenity of its neighbours *It is not considered that the proposed use would have a materially greater impact on neighbouring amenity than the existing use*

- It seems inappropriate to site this type of business premises in a quiet residential street *It is recognised that the proposals are to an extent out of character but no more so than the existing use*

-With a planned reduction of staff at the property, the application is even more flawed: having fewer staff to look after potentially dangerous lodgers seems like a recipe for disaster. *It is considered that the proposed staff would be adequate for the operation of the premises*

- Permission should never have been granted for business use, as a nursing home, in a residential road. *As a matter of fact, the existing use is lawful and cannot be rescinded.*

- Excluding those undergoing 'drug rehabilitation' for instance does not exclude drug users, just as 'ex-offenders' does not rule out current offenders. *It is not possible to ever stop such people from living within a given property, but the applicant has undertaken to ensure that the site is not expressly used for these purposes*

The range of people with difficult conditions or behaviour not excluded from the application is limitless and the application is therefore wide open to abuse. *It is considered that conditions can adequately control the use of the property*

- The large fire escape at the rear of No.17 directly overlooks the gardens of Nos. 19 and 15, and will inevitably be used as a smoking area in the new development - this will result

in intrusion and an unacceptable loss of privacy. *The fire escape is a feature of the existing property. A condition is suggested to ensure that this is only used for access purposes.*

- Traffic and Parking Addressed in the main report.

- Current owner has no experience of running such a facility The applicant is Barnet Homes. In any event, this is not a reason to withhold permission

- The application says that there is an oversupply of residential care homes in Barnet but the Barnet Housing Strategy predicts a 41% increase in the elderly population which would reverse any perceived oversupply very quickly. Addressed in main report – there is currently an oversupply of this form of housing.

6. Equality and Diversity Issues

Section 149 of the Equality Act 2010, which came into force on 5th April 2011, imposes important duties on public authorities in the exercise of their functions, including a duty to have regard to the need to:

“(a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;

(b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;

(c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.”

For the purposes of this obligation the term “protected characteristic” includes:

- age;

- disability;

- gender reassignment;

- pregnancy and maternity;

- race;

- religion or belief;

- sex;

- sexual orientation.

Officers have in considering this application and preparing this report had regard to the requirements of this section and have concluded that a decision to grant planning permission for this proposed development will comply with the Council’s statutory duty under this important legislation.

The proposals would provide supported temporary accommodation.

The proposals would result in the loss of a residential care home for the elderly with 6 rooms. It is considered that, there is an oversupply of this form of accommodation and that there is adequate provision in Barnet to meet demand. It is therefore considered that the loss of the accommodation would not be contrary to the Equalities Act 2010.

The proposed temporary supported accommodation would meet identified demand within the Borough and would have a positive impact in this regard.

It is not considered that the use of the premises as temporary supported accommodation would have a negative impact on neighbouring residents.

7. Conclusion

The proposal is considered to accord with the requirements of the Development Plan and is therefore recommended for approval.

This page is intentionally left blank

LOCATION: Phase 6a, Millbrook Park (former Inglis Barracks), Mill Hill East, London, NW7 1PX **AGENDA ITEM 12**
REFERENCE: 15/03305/RMA **Received:** 29 June 2015
WARD(S): Mill Hill **Accepted:** 29 June 2015
Expiry: 24 July 2015

Final Revisions: 1 September 2015

APPLICANT: Barry M Cosmetics Ltd

PROPOSAL: Reserved matters application seeking approval of appearance, landscaping, layout and scale for Phase 6a of the Mill Hill East development pursuant to Outline planning permission reference H/04017/09 dated 22/9/11, involving the erection of a three storey B1 light industrial building providing 2,935m² of gross internal floor area together with associated access, car parking and landscaping together with details to discharge the requirements of: Condition 5 (Reserved matter details), 5b (Advanced infrastructure works), 26 (Access points), 27 (Details of estate roads), 30 (Existing adopted highway), 35 (Petrol/oil interceptors), 83 (Grey water/rainwater recycling) and 85 (Green/brown roofs).

RECOMMENDATION: Approve Subject to Conditions

- 1 This development must be begun within three years from the date of this permission.

Reason:

To comply with Section 51 of the Planning and Compulsory Purchase Act, 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

331095-16C, 331095-17C, Site Location Plan, 331095-20A, 331095-21A, 331095-24A, 331095-23A, 331095-27A and 331095-22A.

Design and Access Statement;

Planning Statement

Soft Landscape Specification and Landscape Scheme;

Arboricultural Impact Assessment, Survey, Protection Plans and Details;

Sustainability/Energy Statement;

Phase 1 Environmental Assessment;

Highways Design Capacity Statement;
Drainage and Utilities Design Capacity Statement;
Construction Management Plan;
Extended Phase 1 Habitat Survey; and
Archaeological Written Scheme of Investigation.

Reason:

For the avoidance of doubt and in the interests of proper planning and so as to ensure that the development is carried out fully in accordance with the plans as assessed in accordance with policies DM01 of the Adopted Barnet Development Management Policies DPD (2012) and CS NPPF and CS1 of the Adopted Barnet Core Strategy DPD (2012).

3. Before the development hereby permitted is occupied the car parking spaces shown on Drawing No. 331095-20A shall be provided and shall not be used for any purpose other than parking of vehicles in connection with the approved development.

Reason: To ensure that adequate and satisfactory provision is made for the parking of vehicles in the interests of pedestrian and highway safety and the free flow of traffic in accordance with London Borough of Barnet's Local Plan Policy CS9 of Core Strategy (Adopted) September 2012 and Policy DM17 of Development Management Policies (Adopted) September 2012.

4. Before the development hereby permitted is occupied the cycle parking spaces shown on Drawing No. 331095-20A shall be provided and shall not be used for any purpose other than parking of vehicles in connection with the approved development. For the avoidance of doubt, this should include provision for a minimum of 12 staff and 2 visitors.

Reason: To ensure that adequate and satisfactory provision is made for the parking of vehicles in the interests of pedestrian and highway safety and the free flow of traffic in accordance with London Borough of Barnet's Local Plan Policy CS9 of Core Strategy (Adopted) September 2012 and Policy DM17 of Development Management Policies (Adopted) September 2012.

5. Notwithstanding the details shown on plans otherwise hereby approved and prior to development commencing, details, including samples of the materials to be used for the external surfaces of the buildings and hard surfaced areas shall be submitted to and approved in writing by the Local Planning Authority. The development shall be implemented in accordance with such details as approved.

Reason:

To safeguard the visual amenities of the locality in accordance with Policies CS5 of the Core Strategy (2012) and DM01 of the Development

Management Policies (2012).

6. All hard and soft landscaping shall be carried out in accordance with the approved landscaping scheme and shall be completed within the first planting and seeding seasons following the completion of each phase of the development or the occupation of the buildings, whichever is the earlier period.

The new planting and landscape operations should comply with the requirements specified in BS 3936 (1992) 'Nursery Stock, Part 1, Specification for Trees and Shrubs' and in BS 4428 (1989) 'Code of Practice for General Landscape Operations (Excluding Hard Surfaces)'. Thereafter, the areas of hard and soft landscaping shall be permanently retained.

Any tree, shrub or area of turfing or seeding shown on the approved landscaping scheme which, within a period of 5 years from the completion of the development, dies, is removed or in the opinion of the Local Planning Authority becomes seriously damaged or diseased, shall be replaced in the same place in the next planting season with another such tree, shrub or area of turfing or seeding of similar size and species unless the Local Planning Authority first gives written consent to, any variation.

Reason:

To ensure that the landscaped areas are laid out and retained in accordance with the approved plans in order to preserve and enhance the visual amenities of the locality in compliance with Policies CS7 of the Core Strategy (2012) and DM16 of the Development Management Policies (2012).

7. The development shall be carried out in strict accordance with the provisions and tree protection methods outlined in the submitted Arboricultural Method Statement by D F Clark Bionomique Ltd dated November 2014.

Reason: To safeguard the health of existing trees which represent an important amenity feature in accordance with policies CS5 and CS7 of The Core Strategy (2012) and DM01 of the Development Management Policies (2012).

8. The development shall be carried out in accordance with the approved Landscape Management and Maintenance Plan and the management and maintenance regimes shall be reviewed on an annual basis for a minimum period of 5 years as set out in the approved document.

Reason: To secure opportunities for the enhancement of nature conservation value of the site in accordance with policy CS7 of the Core Strategy (2012) and DM16 of the Development Management Policies

(2012).

9. This permission does not extend to destroy, fell, lop or top the existing trees and which are shown on the approved plans to be retained. These trees shall be protected during the implementation of the development in accordance with recommendations set out in BS 5837 (2012) and any supplementary protection requested by the Local Planning Authority. Before excavation can commence, drawings shall be submitted to the Local Planning Authority giving details of the method of excavation, type of foundation proposed for the buildings and indicating how the roots of these trees shall be protected. In addition, drawings shall be submitted showing the layout of underground services. No construction works shall commence until such drawings have been approved in writing by the Local Planning Authority.

Reason: To safeguard the health of existing trees which represent an important amenity feature in accordance with policies CS5 and CS7 of The Core Strategy (2012) and DM01 of the Development Management Policies (2012).

10. In this condition “retained tree” means an existing tree which is to be retained in accordance with approved plan and particulars; and paragraphs (a) and (b) below shall have effect until the expiration of 5 years from the date of the occupation of the building for its permitted use.
- (a) no retained tree shall be cut down, uprooted or destroyed, nor shall any retained tree be topped or lopped other than in accordance with the approved plans and particulars, without the written approval of the Local Planning Authority. Any topping or lopping approved shall be carried out in accordance with British Standard 3998 (Tree Work);
- (b) if any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted at the same place and the tree shall be of such size and species, and shall be planted at such time as may be specified in writing by the Local Planning Authority;
- (c) the erection of fencing for the protection of any retained tree shall be undertaken in accordance with the recommendations set out in BS 5837 (2012) and the approved plans and particulars before demolition or any equipment, machinery or materials are brought on to the site for the purposes of the development, and shall be maintained until all equipment, machinery and surplus materials have been removed from the site. Nothing shall be stored or placed in any area fenced in accordance with this condition and the ground levels within those areas shall not be altered, nor shall any excavation be made, without the written consent of the Local Planning Authority.

Reason: To safeguard the health of existing trees which represent an

important amenity feature in accordance with policies CS5 and CS7 of The Core Strategy (2012) and DM01 of the Development Management Policies (2012).

11. An arboricultural consultant shall be employed to supervise and advise tree protection during the course of development including demolition and construction phases.

Reason: To protect existing trees during the construction works in order to ensure that the character and amenity of the area are not impaired in accordance with policies CS5 and CS7 of The Core Strategy (2012) and DM01 of the Development Management Policies (2012).

12. Notwithstanding the details submitted in support of the application. No development or site works shall take place on site until a 'Demolition & Construction Method Statement' has been submitted to and approved in writing by, the Local Planning Authority.

The Statement shall provide for: access to the site; the parking of vehicles for site operatives and visitors; hours of construction, including deliveries, loading and unloading of plant and materials; the storage of plant and materials used in the construction of the development; the erection of any means of temporary enclosure or security hoarding and measures to prevent mud and debris being carried on to the public highway and ways to minimise pollution including dust.

b) The development shall thereafter be implemented in accordance with the measures detailed within the statement.

Reason: In the interests of highway safety and good air quality in accordance with Policies DM04 and DM17 of the Development Management Policies DPD (adopted September 2012), the Sustainable Design and Construction SPD (adopted April 2013) and Policy 5.21 of the London Plan (2015).

13. Notwithstanding the details shown on plans otherwise hereby approved and prior to development commencing a detailed external lighting scheme including siting of lighting columns and a site plan with lux lines shall be submitted to and approved in writing by the Local Planning Authority. The development shall be implemented in accordance with such details as approved.

Reason: To safeguard the visual amenities of the locality and prevent disturbance to existing and future occupants thereof and to ensure the free flow of vehicular and pedestrian traffic and security of the site in accordance

with Policies CS9 of the Core Strategy (2012) and DM01, DM04 and DM17 of the Development Management Policies (2012).

14. The development shall not be occupied until details of all mechanical plant to be installed as part of the development have been submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure a satisfactory appearance for the development and satisfactory accessibility; and to protect the amenities of the area in accordance with Policy DM01 of the Development Management Policies DPD (adopted September 2012) and Policy CS14 of the Local Plan Core Strategy (adopted September 2012).

15. The mitigation measures outlined in the noise report by ACA Acoustics Ltd, Report Number 150405-002B, dated May 2015 shall be implemented in their entirety prior to the commencement of the use of the development and retained as such thereafter.

Reason: To ensure that the proposed development does not prejudice the amenities of occupiers of neighbouring properties in accordance with Policy DM04 of the Development Management Policies DPD (adopted September 2012), the Sustainable Design and Construction SPD (adopted April 2013) and Policy 7.15 of the London Plan 2011

INFORMATIVE(S):

- 1 The applicant is reminded that the conditions and planning controls in the outline permission H/04017/09 dated 22/09/2011 are still relevant and must be complied with. There are also conditions that require to be discharged prior to the occupation of the development.
- 2 The costs of any associated works to public highway, including reinstatement works, will be borne by the applicants and will require the Applicant to enter into a 278 Agreement under the Highways Act 1980.
3. The applicant is also advised that the development is located in close vicinity of Transport for London Road Network (TLRN) and a Strategic Road Network (SRN) and is likely to cause disruption. The Traffic Management Act (2004) requires the Council to notify Transport for London (TfL) for implementation of construction works. The developer is expected to work with the Council to mitigate any adverse impact on public highway and would require TfL's approval before works can commence.

1. BACKGROUND TO THE CURRENT APPLICATION

1.1 The Mill Hill East Area Action Plan

Mill Hill East is designated as an Area of Intensification in the London Plan (2015) and as a key growth area in the Barnet Core Strategy (2012). The area covered by this designation includes the former Inglis Barracks; Mill Hill East station; International Bible Students Association (IBSA House); the Council Depot and recycling centre; Bittacy Court; the Scout Camp and former Mill Hill Gas Works (the area now centred around Libbury Square).

The area was first highlighted as an area which could be redeveloped in the London Plan in 2004. This is primarily as a result of Project MoDEL (Ministry of Defence Estates London) which involves the consolidation and sale of surplus MoD properties around London. The activities from Inglis Barracks were transferred to RAF Northolt and the base vacated in 2008 thereby providing an opportunity for redevelopment. The Council recognised that Mill Hill East was an area where more detailed policies were required to guide future development and in 2009 adopted an Area Action Plan (AAP) which covered an area of 48 hectares focused primarily on the former Inglis Barracks site. The aim of the AAP is to seek to ensure that development takes place in a balanced and coordinated manner by setting out a comprehensive framework to guide the delivery of housing, employment, leisure and associated community facilities, infrastructure, transport initiatives and environmental protection and enhancement.

A partnership comprising of a number of the key landowners and developers including Barnet Council (the Inglis Consortium) prepared and submitted the outline application in 2009 for the comprehensive redevelopment of most of the area covered by the AAP.

1.2 The outline planning permission

In September 2011 outline planning permission was granted for the redevelopment of Mill Hill East regeneration site (now also known as Millbrook Park). This site covers an area of approximately 33.6 hectares (83 acres) and is located within the Mill Hill ward. The site is bounded to the east by Frith Lane, to the north by Partingdale Lane and to the west by Bittacy Hill (B552). Bittacy Business Park is immediately to the south of the site and Mill Hill East Underground station (Northern Line) lies to the south west.

The site is divided into a number of Development Land Parcels (DLP) otherwise known as phases. The approved phasing plan has altered several times subsequent to the grant of outline planning permission and currently consists of 19 separate phases, 8 of which currently have either reserved matters or full planning approval. Following approval of the site wide pre-commencement requirements, reserved matters applications will be brought forward for all detailed elements of the

development, which would deal with all matters not fully addressed within the outline consent – the ‘reserved matters’ (layout, design, appearance and landscaping). This is controlled by Condition 5 of the outline permission (ref H/04017/09, dated 22nd September 2011).

In addition to the plan drawings submitted, the following information was also submitted in support of the application and forms the supporting information:

- Design and Access Statement;
- Planning Statement
- Soft Landscape Specification and Landscape Scheme;
- Arboricultural Impact Assessment, Survey, Protection Plans and Details;
- Sustainability/Energy Statement;
- Phase 1 Environmental Assessment;
- Highways Design Capacity Statement;
- Drainage and Utilities Design Capacity Statement;
- Construction Management Plan;
- Extended Phase 1 Habitat Survey; and
- Archaeological Written Scheme of Investigation.

2. MATERIAL CONSIDERATIONS

2.1 Key Relevant Planning Policy

National Planning Policy Guidance / Statements: The National Planning Policy Framework (NPPF)

On March 27th 2012 the Government published the National Planning Policy Framework (NPPF). The NPPF sets out the Government’s planning policies for England and how these are expected to be applied. The NPPF replaces 44 planning documents, primarily Planning Policy Statements (PPS’s) and Planning Policy Guidance (PPG’s), which previously formed Government policy towards planning.

The NPPF states that the purpose of the planning system is to contribute to the achievement of sustainable development. The document includes a ‘presumption in favour of sustainable development’. This is taken to mean approving applications, such as this proposal, which are considered to accord with the development plan.

The Mayor’s London Plan: March 2015

4.1 (Developing London’s Economy), 4.4 (Managing Industrial Land and Premises) 5.2 (Minimising carbon dioxide emissions), 5.3 (Sustainable design and construction), 5.7 (Renewable energy), 5.11 (Green roofs and development site environs), 5.12 (Flood risk management), 5.13 (Sustainable drainage), 5.14 (Water quality and wastewater infrastructure), 5.21 (Contaminated land), 6.3 (Assessing effects of development on transport capacity), 6.9 (Cycling), 6.10 (Walking), 6.13

(Parking), 7.15 (Reducing noise and enhancing soundscapes), 7.19 (Biodiversity and Access to Nature), 7.21 (Trees and Woodlands).

Core Strategy (Adoption version) 2012

Development Management Policies (Adoption version) 2012

Barnet's Local Plan is made up of a suite of documents including the Core Strategy and Development Management Policies Development Plan Documents (DPD).

Relevant Core Strategy Policies: CS NPPF (National Planning Policy Framework – Presumption in Favour of Sustainable Development), Choice in Barnet), CS5 (Protecting and Enhancing Barnet's Character to Create High Quality Places), CS8 (Promoting a strong and prosperous Barnet) CS9 (Providing safe, effective and efficient travel), CS12 (Making Barnet a Safer Place), CS13 (Ensuring the Efficient Use of Natural Resources), CS14 (Dealing with Waste).

The Development Management Policies document provides the borough wide planning policies that implement the Core Strategy. These policies will be used for day-to-day decision making.

Relevant Development Management DPD Policies: DM01 (Protecting Barnet's Character and Amenity), DM02 (Development Standards), DM03 (Accessibility and Inclusive Design), DM04 (Environmental Considerations), DM14 (New and existing Employment Space), DM16 (Biodiversity), DM17 (Travel Impact and Parking Standards).

Mill Hill East Area Action Plan (AAP) 2009

The Mill Hill East Area Action Plan (AAP) was adopted by the Council in 2009 and forms part of Barnet's Local Plan containing policies relevant to the determination of planning applications in the area. The AAP forms a material consideration in the determination of Planning Applications in this area.

The relevant policies for the consideration of this application are: MHE3 (Employment), MHE10 (Making the Right Connections), MHE12 (Sustainable Transport), MHE13 (Parking), MHE14 (Creating a Sustainable Development), MHE15 (Design), MHE16 (Delivering Design Quality), MHE18 (Delivering the AAP).

Approved Design Code

The approved Design Code pursuant to Condition 4 of the outline consent (ref H/04565/11, 21st Dec 2011) also sets out the guidelines for how the site, its neighbourhoods, open spaces and key amenities could be designed and built. It informs the formulation of individual reserved matter applications related to specific phases of development. Site-wide or phase related reserved matters must be in compliance with the agreed Design Code unless satisfactorily justified and this will be assessed in detail below.

2.2 Relevant Planning History

Application Reference:	H/04017/09
Case Officer:	Jo Dowling
Proposal:	Outline application for the comprehensive redevelopment of the site for residential led mixed use development involving the demolition of all existing buildings (excluding the former officers mess) and ground re-profiling works, to provide 2,174 dwellings, a primary school, GP Surgery, 1,100sqm of 'High Street' (A1/2/3/4/5) uses, 3,470sqm of employment (B1) uses, a district energy centre (Sui Generis) and associated open space, means of access, car parking and infrastructure (with all matters reserved other than access). Full application for the change of use of former officers' mess to residential (C3) and health (D1) uses.
Stat Start Date	30/10/2009
Application Type	Outline Application
Decision	Granted
Decision Date	22/09/2011

Application Reference:	H/04655/11
Case Officer:	Colin Leadbeatter
Proposal:	Demolition of existing buildings within the curtilage of the Millbrook Park development (formerly Inglis Barracks) as approved under outline application reference H/04017/09 (Approved September 2011)
Stat Start Date	12/11/2011
Application Type	Prior Notification (Demolition)
Decision	Granted
Decision Date	20/12/2011

Application Reference:	H/00480/12
Case Officer:	Colin Leadbeatter
Proposal:	Reserved matters application seeking approval for advance infrastructure works in relation to Phase 1A of Millbrook Park (Mill Hill East) pursuant to outline planning permission reference H/04017/09 dated:

	22/9/2011
Stat Start Date	07/02/2012
Application Type	Reserved Matters
Decision	Granted
Decision Date	19/04/2012

Application Reference:	H/00642/12
Case Officer:	Colin Leadbeatter
Proposal:	Reserved matters application seeking approval for advance infrastructure works in relation to Phase 2 of Millbrook Park (Mill Hill East), pursuant to outline planning permission H/04017/09 dated 22/09/2011
Stat Start Date	20/02/2012
Application Type	Reserved Matters
Decision	Granted
Decision Date	20/04/2012

Application Reference:	H/03057/12
Case Officer:	Wing Lau
Proposal:	Reserved matters application seeking approval for landscaping works to Officers' Mess Gardens (including associated infrastructure works) for Phase 2 (public open space OSI) of Mill Hill East development, pursuant to Condition 5 of Outline planning permission reference H/04017/09 dated: 22/9/2011, together with details to discharge the requirements of conditions 26 (Pedestrian and Vehicular Access Points), 48 (Design of Open Space) and 52 (Children's Play Space)
Stat Start Date	13/08/2012
Application Type	Reserved Matters
Decision	APC
Decision Date	23/10/2012

Application Reference:	H/03548/12
Case Officer:	Thomas Wyld
Proposal:	Reserved matters application seeking approval of Appearance, Landscaping, Layout and Scale in relation to Phase 1a for the erection of 58 houses comprising 39 x 3 bed houses and 19 x 4 bed houses at 'Millbrook Park' (Inglis Barracks) submitted to meet the requirements of Condition 5 of outline planning

	application H/04017/09 dated 22 September 2011.
Stat Start Date	17/09/2012
Application Type	Reserved Matters
Decision	Granted
Decision Date	31/01/2013

Application Reference:	H/03904/12
Case Officer:	Wing Lau
Proposal:	Reserved matters application seeking approval of appearance, landscaping, layout and scale for Phase 2 of Mill Hill East development pursuant to Condition 5 of Outline planning permission reference H/04017/09 dated: 22/9/2011 involving the erection of 103 dwellings comprising 3 x one bed flats, 20 x two bed flats, 45 x 3 bed houses, 25 x four bed houses and 10 x five bed houses. Approval of layout and landscaping works to Phase 2 public open space (OS2), together with details to discharge the requirements of: Conditions 12 (relating to Plot L only); 57 (relating to plots within Phase 2 only); and 8,26, 27, 29, 48, 52, 70, 80, 83, 85 and 91 all in relation to Phase 2 only.
Stat Start Date	08/10/2012
Application Type	Reserved Matters
Decision	Granted
Decision Date	28/03/2013

Application Reference:	H/04080/12
Case Officer:	Wing Lau
Proposal:	Reserved matters application seeking approval of appearance, landscaping, layout and scale for Phase 1 of Mill Hill East development (Millbrook Park) pursuant to Condition 5 of Outline planning permission reference H/04017/09 dated: 22/9/2011 involving the erection of 133 residential dwellings comprising 31 x one bed flats, 61 x two bed flats, 14 x three bed houses and 27 x four bed houses, including associated infrastructure, access roads, car parking, landscaping and approval of layout and landscaping works to Phase 1 public open space OS5, together with details to discharge the requirements of

	conditions 8, 13, 26, 27, 29, 32, 48, 52, 70, 80 and 85.
Stat Start Date	29/10/2012
Application Type	Reserved Matters
Decision	Granted
Decision Date	29/04/2013

Application Reference:	H/04386/12
Case Officer:	Kevin Waters
Proposal:	Construction of a three storey primary school (3 forms of entry) with nursery, community facilities and associated works and landscaping, including staff parking, hard play and sports games area, retaining walls, drainage attenuation measures and provision of a temporary drop off car park
Stat Start Date	10/12/2012
Application Type	Full Planning Application
Decision	Granted
Decision Date	22/04/2013

Application Reference:	H/00668/13
Case Officer:	Wing Lau
Proposal:	Reserved matters application seeking approval for construction of associated advanced infrastructure works and landscaping associated with Phase 3 (public open space OS4) of Mill Hill East development to create 'Panoramic Park' and advance infrastructure works in relation to Phases 3, 4a, 4b and 5, pursuant to Conditions 5 and 5b of Outline planning permission reference H/04017/09 dated: 22/9/2011, together with details to discharge the requirements of conditions 26 (Pedestrian and Vehicular Access Points), 48 (Design of Open Space) and 52 (Children's Play Space). Ground re-profiling works to Phases 10, 11 and part of Phase 7.
Stat Start Date	12/02/2013
Application Type	Reserved Matters
Decision	Granted
Decision Date	22/04/2013

Application Reference:	H/03441/13
Case Officer:	Andrew Dillon
Proposal:	Reserved matters application seeking approval for

	Phase 3a (Central Community Park) and advanced infrastructure works in relation to phases 3a, 8, 9, 10 and 11 of Millbrook Park (Mill Hill East) pursuant to Outline planning permission reference H/04017/09 dated: 22/9/2011, together with details to discharge the requirement of condition 5 (Reserved matter details), 25 (East-West and North-South links), 26 (Pedestrian and vehicular access points), 48 (Design of open spaces) and 52 (Children's play spaces).
Stat Start Date	06/08/2013
Application Type	Reserved Matters
Decision	Granted
Decision Date	13/02/2014

Application Reference:	H/03860/13
Case Officer:	Andrew Dillon
Proposal:	Reserved matters application seeking approval of appearance, landscaping, layout and scale for Phase 3 of Mill Hill East development pursuant to Outline planning permission reference H/04017/09 dated: 22/9/2011 involving the erection of 138 units 7 x 5 bedroom houses, 41 x 4 bedroom houses, 47 x 3 bedroom houses, 26 x 2 bedroom apartments and 17 x 1 bedroom apartments together with details to discharge the requirements of: Conditions 5 (Reserved matter details), 8 (Affordable housing), 12 (Noise Survey along Boundary with IBSA House), 26(Access points), 27 (Details of Estate Roads), 29(Internal access roads), 35 (Petrol/oil interceptors), 48(Open space), 52 (Children's playing space), 57 (Boundary treatment/buffer), 70 (Home standards), 80 (Sustainable homes), 83 (Grey water/rainwater recycling),85 (Green/brown roofs).
Stat Start Date	28/08/2013
Application Type	Reserved Matters
Decision	Granted
Decision Date	12/11/2013

Application Reference:	H/00065/14
Case Officer:	Andrew Dillon
Proposal:	Reserved matters application seeking approval of appearance, landscaping, layout and scale for Phase 3

	Reserved matters application seeking approval of appearance, landscaping, layout and scale for Phase 3a of Mill Hill East development involving the erection of 92 dwellings comprising 27 x 1 bed flats, 42 x 2 bed flats, 13 x 3 bed houses and 10 x 4 bed houses to meet the requirements of Condition 5 of outline planning application H/04017/09 dated 22 September 2011.
Stat Start Date	23/01/2014
Application Type	Reserved Matters
Decision	Granted
Decision Date	20/03/2014

Application Reference:	15/01546/RMA
Case Officer:	Andrew Dillon
Proposal:	Reserved matters application seeking approval of appearance, landscaping, layout and scale for Phase 4a of the Mill Hill East development pursuant to Outline planning permission reference H/04017/09 dated: 22/9/2011, involving the erection of 114 units 6 x 5 bedroom houses, 37 x 4 bedroom houses, 12 x 3 bedroom houses, 52 x 2 bedroom apartments and 7 x 1 bedroom apartments together with details to discharge the requirements of: Conditions 5 (Reserved matter details), 8 (Affordable housing), 26 (Access points), 27 (Details of Estate Roads), 29 (Internal access roads), 32 (Shared Footways/ Cycleways), 35 (Petrol/oil interceptors), 48 (Open space), 52 (Children's playing space), 69 (Noise from Plant), 70 (Home standards), 80 (Sustainable homes), 83 (Grey water/rainwater recycling) and 85 (Green/brown roofs).
Stat Start Date	20/03/2015
Application Type	Reserved Matters
Decision	Granted
Decision Date	25/06/2015

2.3 Consultations and Views Expressed

Public Consultation

Neighbours Consulted: 37

Replies: 0

Neighbours Wishing 0

To Speak

Residents Associations and Amenity Groups.

No Comments Received.

Internal /external and Other Consultations:

Highways

The Council's Highway Officer has confirmed no objection to the proposal subject to the attachment of appropriate conditions. Detailed comments are incorporated into officer report below.

Environmental Health

The Council's Environmental Health Team have not raised any objections to the proposal subject to the attachment of suitable conditions.

Trees and Landscape

Clarification required regarding tree planting holes and trees to be removed.

Officer Comment:

The applicant has provided additional information clarifying the proposed tree pits. Suitable Safeguarding Conditions are also attached to ensure appropriate mitigation.

3. DESCRIPTION OF THE SITE, SURROUNDINGS AND PROPOSAL

3.1 Site Description and Surroundings

Millbrook Park

In September 2011 outline planning permission was granted for the redevelopment of a site known as Mill Hill East (now also known as Millbrook Park). This site covers an area of approximately 33.6 hectares (83 acres) and is located within the Mill Hill ward. The site is bounded to the east by Frith Lane, to the north by Partingdale Lane and to the west by Bittacy Hill (B552). Bittacy Business Park is immediately to the south of the site and Mill Hill East Underground station (Northern Line) lies to the south west.

Outline consent was granted for a residential-led mixed use development, involving the demolition of all existing buildings (excluding the Officers' Mess building) and ground re-profiling works, to provide 2,174 dwellings, a primary school, GP surgery, 1,100sqm of 'High Street' (A1/2/3/4/5) uses, 3,470sqm of employment (B1) uses, a district energy centre and associated open space, means of access, car parking and infrastructure in September 2011 (ref H/04017/09).

Site in relation to the outline consent:

The outline planning consent allows the Millbrook Park development to be implemented in a series of phases (split into Development Land Parcels). The approved phasing plan has altered several times subsequent to the grant of outline planning permission and currently consists of 19 separate phases, 8 of which currently have either reserved matters or full planning approval. This application relates to Phase 6a of the outline consent, which is in the south eastern part of the Mill Hill East regeneration area (also known as Millbrook Park).

The site covers an area of 0.37 hectares measuring 61 m deep and 61 m wide including the access road. The site generally slopes North to South 5 m to the current access road and West to East by approximately 2 m. To the north is the Scout Club and to the South Bittacy Business Centre. The site is currently vacant and covered in hardstanding. Prior to 2015 the site formed part of the London Borough of Barnet Waste and Recycling Operations.

The site is located within the employment character zone. The form of development within this zone is not specified in the approved Design Code although the approved scale parameter plan advises that development should not exceed three storeys, which this scheme of development is with compliance with.

The site will be accessible via the future internal road network to Frith Lane which lies to the east. The site has a public transport accessibility level (PTAL) of 2. Mill Hill East underground station is approximately 240m to the south-west of the site. The 221 and 240 bus routes run along Bittacy Hill with the closest stop located approximately 230m from the application site.

3.2 Description of Proposal

The proposal is to seek approval of matters reserved under outline planning consent ref H/04017/09 (layout, scale, appearance and landscaping) to redevelop the site involving the erection of a three storey B1 light industrial building providing 2,935m² of gross internal floor area together with associated access, car parking and landscaping.

Discharging of conditions

This application also involves the partial discharging of a number of planning conditions attached to the outline consent that require information to be submitted for each phase of the development. Those conditions that are to be approved in relation to Phase 6a are as follows:

- 5 – Reserved Matters Details

Sets out the submission requirements for submission accompanying each reserved matters application.

- 5b – Advanced Infrastructure Works

Sets out the submission requirements for submission accompanying each reserved matters application for Advanced Infrastructure Works, which relates to advance site preparation, decontamination and infrastructure works including roads and lighting.

- Condition 26 – Pedestrian and Vehicular Access Points

This requires details of access points, estate roads and footways to be submitted and approved.

- Condition 27 – Details of Estate Roads

This requires details of lighting, pedestrian facilities, crossing points, cycle facilities, signing, bus stops/shelters, bus standing/layover facility, bus driver facilities, highway improvements and estate road layout and gradient.

- Condition 30 – Existing Adopted Highway

This requires details of any works to existing public highways affected by the proposed works.

- Condition 35 – Petrol/ oil interceptors

This requires details of petrol/ oil interceptors or justification concerning why this is not required.

- Condition 83 – Greywater/Rainwater Recycling Provision

This requires details demonstrating the incorporation of either rainwater or grey water recycling facilities into each of the buildings to be submitted and approved.

- Condition 85 – Green/Brown Roofs Provision

This requires details to demonstrate the provision of Green or Brown roofs into each of the buildings to be submitted. Details shall also include a reconciliation plan or table showing how the proposed provision complies with the 10% target fixed by condition 85 for the wider Millbrook Park redevelopment.

4. PLANNING CONSIDERATIONS

4.1 The Principle of Development

The principle of constructing a commercial employment building established by the outline planning consent. Condition 5 (Reserved Matters Details) seeks details (layout, scale, landscaping and appearance) to be submitted to and approved by the Local Planning Authority (LPA) prior to the commencement of development.

The reserved matters currently under consideration are:

Scale – the height, width and length of each building proposed in relation to its surroundings.

Layout – the way in which buildings, routes and open spaces are provided within the development and their relationship to buildings and spaces outside the development.

Appearance – the aspects of a building or place which determine the visual impression it makes, excluding the external built form of the development.

Landscaping – this is the treatment of private and public space to enhance or protect the site’s amenity through hard and soft measures, for example, through planting of trees or hedges or screening by fences or walls.

Access – The internal road layout within this phase was established at outline stage.

This current application alters the location of the proposed access to utilise the existing access road from Frith Lane rather than creating a new access to the north of the plot. This change is considered acceptable as it reduces unnecessary highway works and the need for some tree removal. Improvements to the existing access are proposed including the provision of increased visibility splays at the junction with Frith Lane. Details of internal circulation and routes for pedestrians and vehicles are considered as part of the overall scheme and for the discharge of conditions 26, 27 and 30.

The outline planning permission consists of a number of parameter plans which establish a series of parameters and principles to create a clear framework of planning control and fix the quantum of development, land uses, levels and access arrangements.

The key parameter plans of relevance to the consideration of this application are:

- **Parameter Plan 1: Access and Movement**
Establishes the main vehicular and pedestrian access points and vehicular movement hierarchy.
- **Parameter Plan 2: Landscape**
Establishes the location and extent of areas of public open space.
- **Parameter Plan 3: Land use**
Establishes the location and distribution of land uses and open spaces.
- **Parameter Plan 4: Scale**
Establishes the maximum height permissible across the whole Millbrook Park site.
- **Parameter Plan 5: Character Areas**
Establishes the extent and disposition of the strategic character areas.
- **Parameter Plan 6: Levels Strategy**
Establishes the proposed spot levels at street junctions and maximum permissible gradients along each of the streets.

In order to support the detail contained within the parameter plans the outline consent has a number of additional documents that form a ‘strategic development

framework' in accordance with the requirements of Policy MHE18 of the AAP. The 'framework' establishes a series of development principles that will be used to guide detailed elements and the preparation of reserved matter applications. Of relevance to the consideration of this application are the following documents:

- Design Principles Document;
- Phasing and Delivery Strategy
- Technical/Infrastructure Strategy
- Revised Public Realm and Open Space Strategy
- Technical and Infrastructure Strategy
- Revised Phasing and Delivery Strategy

Design Code

In addition to the above a site wide design code was approved in 2011 pursuant to the clearance of condition 4 of the outline application and forms the guide to the assessment of reserved matters applications. However in relation to the employment Zone which incorporates Phase 6a the Design Code does not provide any specific guidance advising that '*all mixed use, education and employment uses are subject to separate development briefs*'. Notwithstanding this

This reserved matters application for Phase 6a is therefore considered within the framework of established broad development principles and approved Parameter Plans, as well as the broad parameters of the approved design code.

Deviations from approved parameters

The applicant has submitted a statement of compliance with this application to describe the proposed development and demonstrates general compliance with the outline planning permission. There are some areas where the application does not conform and the applicant has provided justification for these deviations. These are explained in the sections below.

4.2 Amount of Development

Employment

Phase 6a comprises employment (B1) use only. Condition 14 of the outline planning permission sets a maximum quantity of 3470 sq.m of employment floor spaces within the wider Millbrook Park development. The outline permission does not control the quantity of employment space to be provided within each phase although identified areas include the current site at 6a, the adjoining plot at 6b which is also identified as the location of the CHP plant and around the future proposed public square opposite Mill Hill East station, although other commercial uses such as retail are also proposed in this location.

The current application proposes 2935 sq.m within this phase leaving a balance of 535 sq.m to be provided elsewhere within the Millbrook Park development. The quantity of Floorspace which is proposed is based on the operational needs of the applicant 'Barry M Cosmetics' who is currently located in the Bittacy Hill Business Centre to the south.

While the proposal reduces the quantity of employment floor space available to future phases there is no contradiction with the outline planning approval in regards to the quantity of floor space proposed. It is also considered that enabling this successful local business to expand to a 'fit for purpose' premises within in the local area is a positive aspect of the scheme in keeping with local, regional and national policy.

Density

The amount of development and minimum/maximum building dimensions have already been approved at the outline stage which the current application accords to and the proposal is therefore considered acceptable in this regard.

Public Open Space

Condition 15 (Level of Open Space) of the outline consent sets out the level/target of open space to be provided across the whole Millbrook Park site. It stipulates that not less than 5.95 Hectares of open space shall be provided in the development which will consist of a target provision in a number of areas across the development site. In relation to Phase 6a no formal areas of public open space are proposed although the landscaped buffer adjoining Frith Lane will be open and publically accessible.

The application also proposes associated hard and soft landscaping works across the site. The landscape included along the Frith Lane frontage will include the introduction of new tree and shrub planting to reinforce the Green Edge. The proposal is therefore considered in accordance with Condition 15.

4.3 Scale

Parameter Plan 4 (Scale) indicates a 3 storey maximum permissible height for this phase with a height range ranging from between 6-15 metres. Minimum and maximum dimensions are also set out within this parameter plan.

The proposed building accords with the maximum height parameters with the building measuring 3 storeys in height and with the height of the building measuring 13m along the front and side of the building fronting Frith Lane with the setback stacking area increasing to 15m in height.

In relation to the dimensions of the building, the approved parameters specify a minimum/ maximum width of between 12-30m and length of between 30-50m.

The proposed building measures between 22.3m and 34.2m in width and between 38.56m and 48.05m in length. As such there is a breach in relation to the proposed width of the building by approximately 4.2m at the widest point.

The justification provided by the applicant concerns the dimensions of the plot and the need to provide adequate space for the loading and unloading of vehicles, which is proposed on the western elevation of the building, along with the packing and racking requirements connected with the business. The application maintains a gap of between 8-10m to Frith Lane and it is not considered that the minor breach would result in any significantly increased visual impact or the ability of the proposal to maintain an effective landscaped screen. On balance it is therefore considered that the proposed departure from the approved parameters is considered acceptable in this instance.

4.4 Layout

Policies CS5 and DM01 require development to be of a high quality design and should ensure attractive, safe and vibrant streets which provide visual interest. Proposals should also create safe and secure environments, reduce opportunities for crime and minimise fear of crime. The proposals demonstrate, with the exceptions outlined above provides for an internal road and building layout in broad accordance with the Illustrative Masterplan and the Access and Movement Parameter Plan 1.

Site Boundaries

Within Phase 6a there is one key primary site boundary, this being the relationship with Frith Lane to the West.

The relationship with Frith Lane is considered to be of paramount importance to the overall acceptability of the scheme, marking the transition from the development to the rural green belt located on the opposite side of Frith Lane to the East. The proposed factory is set back 10m from the road with existing trees retained where possible and augmented where necessary with additional planting.

Parking

Parking for the phases is provided on the southern edge of the factory building fronting the new access road. Vehicular loading / unloading and cycle parking facilities are also provided.

The level of parking is discussed in the highways section of this report.

Access

The access road from Bittacy Hill is illustrated on Parameter Plan 1 (Access and Movement) as providing vehicle access through to the adjoining Phase at Phase 6b with no through vehicular connection to the wider street network running through the rest of the Millbrook Park development. The access road which is shown on the submitted drawings is in accordance with this plan, being capable of being easily extended through to the adjoining phase. The proposed layout is therefore considered in broad compliance with the parameter plans and the Illustrative Masterplan.

Levels

Levels through the site slope from North to South by 5 m from the boundary with the scout camp to the current access road and from West to East by approximately 2 m. The proposed building is sunk down into the existing ground level by up to 2.5m at the rear of the building, with a further 2m drop down via stairs and ramp to the access road at the front of the site. These levels are in compliance with the approved levels parameters for this plot.

4.5 Appearance

The National Planning Policy Framework 2012 makes it clear that good design is indivisible from good planning and a key element in achieving sustainable development. This document states that permission should be refused for development which is of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions.

As discussed, Phase 6a is located within the employment character zones and as such the design and appearance of any building will by necessity differ in appearance from the predominately housing development which characterises the majority of the Millbrook Park Site.

The proposed factory building is predominately three storeys in height with a large single storey loading bay element on the western elevation of the building. The production rooms and nail varnish barrels are located along the southern edge of the building, with the entrance and staff facilities located on the eastern façade. The warehouse stacking area consisting of a single 15m high storey is located on the northern section of the building.

The building is designed to provide a corner feature around the entrance on the south eastern corner, incorporating the Barry M signage, large scale windows, sky blue steel cladding, with feature external entrance staircase. The treatment of the rest of the façade facing the south incorporates different colour panelling (Silver grey) with vertical panels and punched horizontal windows. All of the building is built over a brick plinth at ground level. The full height stacking area is set back towards

the rear of the building to limit the visual impact and perceived height of the building when viewed from the south and east.

The North elevation to the Scout Hut has also been “broken up” into vertical panels and a horizontal strip of blue to help break up the façade when viewed from the scout camp through the trees to the north.

Overall it is considered that the proposal represents a high quality and satisfactory visual appearance for a building which is in effect an industrial shed. The proposal would be contemporary in appearance while softening its appearance when viewed from adjoining vantage points due to the setbacks, colours and contours of the proposed building.

4.6 Landscaping

The ‘Revised Public Realm and Open Space Strategy’ approved at outline stage sets out the principles for a landscape and open space strategy for Millbrook Park and provides detailed design guidance for reserved matters applications. The approved Design Code adds another layer of detail and requires a number of landscaping features in Phase 6a.

Frith Lane frontage

The Design Code indicates that along the Frith Lane South frontage which includes the application site, existing hedgerow should be retained, with new planting where necessary to reinforce existing planting. The landscaped buffer should measure between 5-8m in width. The application proposes the removal of several trees to facilitate the improvement of the existing access and to erect the building. These trees are to be compensated by replacement tree and shrub planting along the Frith Lane boundary along a 10m wide landscaped buffer. This is considered to accord with the principles outlined in the Design Code and is considered acceptable.

Trees

This Phase contains a large number of existing trees that have been identified to be retained along the boundary of the site with Frith Lane to the east and the scout camp to the north.

The Outline Planning Consent envisaged that some trees would need to be removed in order to facilitate the development of this phase. This is illustrated in Parameter Plan 2 (Landscape) and also in regards to the approval of Condition 53 (Protective Fencing Around Trees). A further application for the demolition of existing buildings (Ref: H/04655/11) was granted in December 2011 which included details of tree retention, felling and protection measures.

The application proposes the removal of four trees which already benefit from planning consent to be removed these consist of two common ashes, a Lombardy poplar and sycamore. In addition to this, the application proposes the removal of an ash, a sycamore and group of saplings (oak, sycamore, elm, hawthorn and elder) not previously surveyed to allow for the widening of the existing access road and the erection of the building. Dead and dying elms (Dutch Elm Disease) are also proposed to be removed fronting Frith Lane. All of the trees proposed for removal are Grade C2.

In compensation for these losses replacement planting in the form of six 'extra heavy standard' and 1423 shrubs are proposed to be planted to reinforce existing gaps along the boundaries with Frith Lane and the Scout Camp, as well as to frame the building along the south of the site.

Protection of existing trees

The applicant has submitted an Arboricultural Method Statement and accompanying plans to indicate the protection measures for the retained trees. The RPA encroachments to retained trees are mainly attributable to proposed roads, footpaths and other hardstanding. The method statement proposes to utilise tree protection measures including protective fencing, a trenchless drainage installation method and the provision of a Geocellular confinement system within Root Protection Area. The Council's Tree Officer has examined the submitted details and has raised some concerns regarding discrepancies between the submitted landscape drawings and proposed replacement species but has not raised any in principle objections.

It is noted that the broad position of the proposed factory building has already been established in the Outline Consent and the trees in question are not subject to any statutory protection, although they provide substantial group amenity value and screening to the boundaries of the site.

It is therefore recommended that appropriate conditions are attached requiring the submission of a foundation method statement, the supervision of relevant works by a qualified arboriculturalist and details of hard and soft landscaping including the requirement for replacement planting in the event of future loss.

Maintenance

The application submission provides limited details of post planting management of landscaped areas. Suitable conditions are attached requiring the submission and approval of a landscape management plan including specifications for replacement planting if necessary and identifying site specific management requirements for establishment period (years 1-5) and management beyond.

Hard areas

The application has not specified the materials to be used for hard surfaced areas, a suitable condition requiring details to be submitted and agreed has been attached.

Lighting

Positions of street lights have not been indicated on the submitted plan, although bollard lighting is shown in the proposed car parking layout. A condition is also suggested requiring the provision of a detailed lighting plan prior to the commencement of development for this phase.

Conclusion for Landscaping

The landscaping approach is considered to be in accordance with design principles set in the Design Code and parameter plans. It will preserve the Frith Lane frontage and rural nature of the scout camp and will frame and complement the contemporary architectural approach to the factory building whilst increasing the overall biodiversity of the site's environment. It complies with Policies CS5 and DM16.

4.8 Impacts on amenities of neighbouring and surrounding occupiers and users

Properties on Frith Lane

The closest properties are located either 400m to the north or 137m to the south on the opposite side of the railway track, due to this distance separation it is not considered that the proposal would adversely affect the amenities of neighbouring residents in these locations.

Scout Camp

The site adjoins the scout camp which is located immediately to the north. The building will be screened by existing mature trees along the boundary and would be of limited visibility when viewed from the scout camp, particularly during the summer months when this facility is likely to be in greater use. The building does not contain windows on this elevation and as such no loss of privacy would also occur. In relation to noise and disturbance, the applicant has advised that the normal hours of operation are Monday to Friday 9 – 5, given that the scout camp is more likely to be in use in the evening or weekends, it is not considered that the relationship would result in significant noise and disturbance problems. The application is also accompanied by a noise impact assessment which the Environmental Health Officer has examined and confirmed that anticipated noise levels would be within permitted levels.

Bittacy Hill Business Park

Other commercial uses included the current site used by Barry M are located immediately to the south of the site, due to the nature of the use of these buildings which is similar to that proposed under this planning application it is not considered that the proposal would adversely impact upon the occupiers of these premises.

Future Phases

The site adjoins Phase 6b which is identified as future employment land and for the location of the CHP plant. Pre application discussions have taken place regarding the potential of incorporating a residential element within this phase. However no application has been submitted. Any new planning application needs to be assessed in the context of the planning situation which exists at the time an application is submitted. In assessing any future scheme for the adjoining site, one of the considerations will be to ensure that the layout and design of a scheme does not prejudice any existing land uses and incorporates appropriate mitigations measures. On this basis it is not considered that the proposal would adversely impact upon the amenities of occupiers of units in future phases.

4.9 Transport, parking and highways matters:

Access

The site fronts Frith Lane. It is proposed that a new access road will incorporate the existing service road adjacent to the South boundary and will also serve the adjacent LBB Refuse and Recycling yard pending the future redevelopment of the wider depot

Proposed Frith Lane / Business Area Junction Works:

The proposed junction will be in a form of a simple priority junction. The proposed access carriageway width will be a minimum of 6m throughout to allow two HGVs to pass.

The kerb radii for the proposed Phase 6a access junction will be designed to accommodate the movement of HGVs into and out of the access. The swept path analysis indicates a junction design to accommodate the movement of 16.5m long Articulated HGV associated with the operational phase of the Phase 6a development and also accommodating the likely HGV movements associated with the construction phase. The design of this junction follows extensive discussions between the applicant and the Council's Highway Team and is considered acceptable.

Highway Capacity:

The preliminary junction design of the Phase 6a prepared by Halcrow and included in the Transport Assessment (TA) as part of the outline planning application demonstrated that the proposed form of junction would operate within capacity with minimal delay and queuing in the 2023 design year with all Millbrook Park development traffic on the highway network. Therefore the junction design is acceptable.

The proposed highway works will need to be carried out under S278 of the Highways Act. Although these works are shown on the submitted application drawings for S278 works, a formal application will need to be submitted to Highways Department for technical approval before any construction works can take place. An informative to this affect is attached.

Drawings submitted with the application shows the swept path analysis for Heavy Goods Vehicles accessing and egressing from the site access which in principle is acceptable but would be subject to technical approval under S278 Agreement.

The applicant has confirmed that an agreement is in place to ensure that the access road remains open at all times for LBB access. It is proposed that this will be ensured by carrying out the construction in phases. Deliveries to and from the site are proposed via the unloading /checking area.

Parking

London Plan parking standards for B1 use is 1 space for 100m²-600m². This is a range of parking that would need to be provided which equates to provision of between 30 parking spaces for a site within PTAL rating of 1 for and 5 parking spaces for a site within a high PTAL rating of 6 for the proposed 2935m².

The PATL rating for the site is assessed as 2 therefore would require a parking provision of approximately 15 parking spaces. A total of 12 parking spaces are being provided including 2 disabled parking spaces, accessed directly from the main access road. The parking spaces fall short by 3 parking spaces. However, it is proposed that a full Commercial Travel Plan is provided to address the shortfall in parking provision. This is an obligation secured under the approved S106 agreement pursuant to the outline consent and as such the level of parking provision is considered acceptable in this instance.

Accessibility and Inclusivity

The submitted planning documents advise that level access by means of a ramp will be provided from street/ car park levels to the entrance of the building. Lifts are also contained within the building providing level access to all storeys which is considered acceptable.

Sustainable travel

The submitted car parking plans show the provision of 2 active EVCPs and 1 passive Electrical Vehicle Charging Points which is in compliance with Condition 68 of the outline consent.

The submitted drawings show the provision of a secure cycle storage building located on the eastern portion of the building. London Plan Cycle Parking standards require 1 space per 250 sq.m equating to 11 spaces, along with short stay provision of 1 spaces per 100 sq.m equivalent to 2 spaces. A condition is attached requiring the provision of such spaces.

The site is also located a short distance from Mill Hill East Underground Station 250m and local bus services running along Bittacy Hill and Frith Lane and as such is accessible by means other than private motor vehicle.

Construction Traffic Management Plan (CTMP):

The applicant has submitted a report to provide a Construction Traffic Management Plan (CTMP) for the Highway Infrastructure associated with Phase 6a of the redevelopment of the Inglis Barracks site (Millbrook Park) supporting the reserved matters application for the infrastructure associated with Phase 6a. The Highway Infrastructure associated with Phase 6a includes the new proposed access located to the south east of the development site onto Frith Lane.

It is proposed that a more detailed CTMP will be submitted, for approval prior to the commencement of any works, once the identity of the Contractor is known and further details relating to the proposed construction process are known.

Street lighting

Positions of street lights have not been indicated on the submitted plan, although bollard lighting is shown in the proposed car parking layout. A condition is including requiring the provision of a detailed lighting plan prior to the occupation of the development for this phase.

Conclusion for Transport, Parking and Highways

In summary, the application provides for adequate parking without harming the local highway network and promotes sustainable modes of travel and complies with Policies CS9 and DM17.

4.10 Environmental issues

Construction management

A Construction Management Plan for the whole of Millbrook Park was approved pursuant to Condition 17 of the outline consent (ref H/04183/11). The document incorporates the view that succinct method statements will be required for each reserved matters application.

A Construction Management Plan has been submitted in support of this reserved matters application which sets out the approved construction routes to and from the site together with arrangements that will be implemented to ensure the environmental issues are managed and the impact on the surrounding environment by this development is kept to a minimum with regards to noise disturbances, vibration, dust, smoke, plant emissions and traffic.

Contamination

A contamination strategy for the whole site has been dealt with under Condition 63 of the outline consent (ref H/00643/12, approved April 2012). This condition is split into 4 parts and parts i) and ii) which includes desk top studies and site investigation have been approved. Part iii) of the condition requires the approval of a remediation strategy on a phase by phase basis and Part iv) requires a verification to be submitted for each phase.

This reserved matters application is accompanied by a contaminated land report by Soils Limited. This study has been examined by the Council's scientific services team who advise that this is a comprehensive Phase 1 Study. The study recommends further intrusive investigation during the course of construction which is secured by Condition 63 of the Outline Consent.

Noise

The noise report supplied with the application (ACA Acoustics Ltd, Report Number 150405-002B, dated May 2015) is comprehensive. It assesses the potential noise impact from plant on nearest residential and office receptors, and demonstrates that Barnet's standard noise level requirements will be met. A condition to ensure that the noise mitigation measures outlined in the plant is incorporated. It is also recommended that a condition is attached requiring the exact details of the plant to be installed, along with the standard noise level condition in order to ensure adequate amenity.

4.11 Energy, climate change, biodiversity and sustainable construction matters:

Sustainable design and construction

The application is accompanied by an energy and sustainability statement which advises the development will achieve a 'Very Good' Building Research

Establishment Environment Assessment Method (BREEAM) rating and achieve a minimum carbon emissions reduction of 40%. The submitted information is considered sufficient to meet the requirements of Condition 80 which can be discharged in relation to Phase 4a.

An overarching energy strategy for the whole of Millbrook Park was submitted and approved pursuant to Condition 79 of the outline consent (ref H/00560/12). The approved strategy outlines how a centralised energy supply to the south of the site will be delivered, and a decentralised supply to the north. The south of the site will be served by a District Heating Network provided by a single Energy Centre while the north of the site will adhere to the Mayor's Energy Hierarchy by utilising an energy efficient building fabric and where applicable photovoltaic panels (PV).

The Mayor's Energy Hierarchy sets out three methods for achieving a minimum 35% reduction in carbon emissions:

- 1 Be lean: use less energy (fabric efficiency standards)
- 2 Be clean: supply energy efficiently
- 3 Be green: use renewable energy

The submitted energy and sustainability strategy advises that a total carbon reduction can be achieved by utilising a suitably sized photovoltaic array (circa 288m², 45kWp) in conjunction with the carbon reductions achieved as a result of building fabric and services efficiencies would be 42.7% which is in accordance with the approved energy strategy for the Millbrook Park development.

Water resource

The drainage strategy for Phase 6a is developed from the principles identified in the approved site wide drainage strategy produced by Halcrow pursuant to Condition 43 (Drainage Strategy, H/04340/11, April 2012) of the outline consent.

Condition 82 (Greywater/Rainwater Recycling Target) of the outline consent requires a minimum of 10% of rainwater to be collected on site and used to provide for irrigation needs of the development. Condition 83 (Greywater/Rainwater Recycling Provision) requires the submission and approval of details demonstrating the incorporation of either rainwater or grey water recycling facilities into each of the buildings of the phase and this should include a reconciliation plan or table showing how the provision complies with the 10% target fixed by Condition 82.

The submitted drainage strategy advises that surface water drainage network will be installed within the highway to drain surface water runoff from the paved surfaces and adjacent commercial or residential development plots, i.e. Phase 6A and 6B. Proposed connection points into the commercial or residential development plots are shown on the drainage drawing submitted with this application. The existing surface water drainage is not available at the Frith Lane adjacent to the Phase 6A junction.

Therefore, the drainage system at the Phase 6A junction and the estate road will be drained toward London Borough of Barnet (LBB) Depot in Phase 6, away from the Frith Lane.

The networks outfall from the site via a new connection into the existing Thames Water surface water sewer present in LBB Depot in Phase 6 and then into the network in Bittacy Hill.

The surface water discharge limit up to the 1 in 100 year plus 30% for climate change design storm, as described within the Drainage Strategy, is 5l/s and 5l/s for Phase 6A and 6B respectively at Outfall J. The impermeable area is 0.338 hectares for each phase.

Rainwater harvesting/brown/green roofs will be provided by the Phase 6A Plot Developer as part of the site wide drainage strategy.

The above information is considered satisfactory in accordance with the approved site wide drainage strategy. It is also recommended that condition 82 is discharged.

Biodiversity and Ecology

The AAP encourages the planting of native species to encourage biodiversity. The Environmental Statement at outline stage concluded that there are no overriding concerns with respect to ecology and nature conservation preventing redevelopment taking place.

A site wide Ecological Mitigation and Management Plan (EMMP) was submitted and approved (H/04184/11, November 2011) pursuant to Condition 60 of the outline consent. It was considered that the document as approved demonstrated a comprehensive overall management plan for ecological assets on the application site.

An extended Phase 1 habitat survey has been submitted in support of the application. The survey found no record of protected species on the site, although some of the trees which are identified as to be retained could potentially contain bat roosts. Otherwise the site is identified as of low ecological value. The survey recommends various ecological enhancement measures including the installation of bat tubes and nest boxes. An identified invasive species 'Butterfly Bush' was also found on the site, which in accordance with legislation is proposed to be removed.

Green roofs

Condition 84 (Green/Brown Roofs Target) of the outline consent requires a minimum of 10% of green or brown roofs across the whole of Millbrook Park site. Condition 85 (Green/Brown Roofs Provision) requires details to be submitted and approved demonstrating this provision across the whole site including a reconciliation plan or table showing how it meets the 10% target fixed by Condition 84.

The application does not propose any green/ brown roofs within this phase. The requirements of Condition 84 and 85 are on a site wide basis and do not mandate the provision of such roof on every phase as such it is considered that the submission has complied with the requirements of Conditions 85 and it is considered that these details can be discharged as so far as they relate to this phase.

5. EQUALITIES AND DIVERSITY ISSUES

Section 149 of the Equality Act 2010, which came into force on 5th April 2011, imposes important duties on public authorities in the exercise of their functions, including a duty to have regard to the need to:

- “(a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;*
- (b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;*
- (c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.”*

For the purposes of this obligation the term “protected characteristic” includes:

- age;
- disability;
- gender reassignment;
- pregnancy and maternity;
- race;
- religion or belief;
- sex;
- sexual orientation.

Officers have in considering this application and preparing this report had regard to the requirements of this section and have concluded that a decision to grant planning permission for this proposed development will comply with the Council’s statutory duty under the above legislation. Issues concerning level access and disable parking are addressed in sections 4.4 and 4.9 of this report.

6. CONCLUSION

As conditioned the proposal would not compromise the outline planning permission (H/04017/09) for the redevelopment of the wider site. It accords with the relevant development plan policies, conforms to the design principles and the parameters established in both the approved outline application for the former Inglis Barracks site and the Design Code.

The proposal is acceptable on land use, visual amenity, access, highways, biodiversity, and drainage grounds. The proposal would not affect the amenities of neighbouring residential properties. It would provide for much needed quality employment space for a successful local business.

The design of the building is appropriate to its context and use, the proposal has been designed to provide an attractive form to the public realm, and limiting the height of the building as far as is possible without prejudicing the future usability of the building.

The proposed new trees and the landscaping scheme would mitigate the additional loss of some of the existing trees, contributing to the 'green edge' fronting Bittacy Hill and the Scout Camp in this part of Millbrook Park site.

The application also satisfies the requirements of Conditions 5, 5b, 26, 27, 30, 35, 83 and 85 of the outline consent.

Condition 5 (Reserved matter details), 5b (Advanced infrastructure works), 26 (Access points), 27 (Details of estate roads), 30 (Existing adopted highway), 35 (Petrol/oil interceptors), 83 (Grey water/rainwater recycling) and 85 (Green/brown roofs).

It is recommended that the application be **approved** subject to the attached conditions.

SITE LOCATION PLAN: Phase a, Millbrook Park (former Inglis Barracks), Mill Hill East, London, NW7 1PX

REFERENCE: 15/03305/RMA



Reproduced by permission of Ordnance Survey on behalf of HMSO. © Crown copyright and database right 2013. All rights reserved. Ordnance Survey Licence number LA100017674.

This page is intentionally left blank

Location **Monkfrith Primary School Knoll Drive London N14 5NG**

Reference: **15/01725/FUL**

Received: 17th March 2015

AGENDA ITEM 13

Accepted: 25th March 2015

Ward: Brunswick Park

Expiry 20th May 2015

Applicant: Alison Dawes

Proposal: The erection of four single storey classroom extensions and further alterations to the existing school building including the provision of 14 car parking spaces, new pedestrian entrance on Brookside South and associated landscaping to increase the capacity of the school from one to two forms of entry

Recommendation: Approve subject to conditions

- 1 The development hereby permitted shall be carried out in accordance with the following approved plans: MK-CS-ZZ-ZZZ-DSC-A-061002 REV P1, MK-CS-ZZ-ZZZ-DSC-A-062001 REV P1, MK-CS-ZZ-ZZZ-DSC-A-062002 REV P1, MK-CS-ZZ-ZZZ-DSC-A-063000 REV P1, MK-CS-ZZ-ZZZ-DSC-A-063001 REV P1, MK-CS-ZZ-ZZZ-DSC-A-063001 REV P1, EBC-CS-B1-LGF-GA-L-G100-001 REV P01, EBC-CS-B1-LGF-GA-L-G100-001 REV P1, MK-CS-ZZ-ZZZ-DSC-A-061001 REV P1, MK-CS-ZZ-ZZZ-DSC-A-063002 REV P1, MK-CS-ZZ-ZZZ-DSC-A-063003 REV P1, MK-CS-ZZ-ZZZ-DSC-A-063004 REV P1, MK-CS-ZZ-ZZZ-DSC-A-063005 REV P1, MK-CS-ZZ-ZZZ-DSC-A-063001 REV P1, Site Plan, Design and Access Statement, Planning Statement.

Reason: For the avoidance of doubt and in the interests of proper planning and so as to ensure that the development is carried out fully in accordance with the plans as assessed in accordance with Policies CS NPPF and CS1 of the Local Plan Core Strategy DPD (adopted September 2012) and Policy DM01 of the Local Plan Development Management Policies DPD (adopted September 2012).

- 2 This development must be begun within three years from the date of this permission.

Reason: To comply with Section 51 of the Planning and Compulsory Purchase Act 2004.

- 3 a) No development (Excluding demolition or site preparation works) shall take place until details of the levels of the building(s), road(s) and footpath(s) in relation to the adjoining land and highway(s) and any other changes proposed in the levels of the site have been submitted to and approved in writing by the Local Planning Authority.

b) The development shall thereafter be implemented in accordance with the details as approved under this condition and retained as such thereafter.

Reason: To ensure that the development is carried out at suitable levels in relation to the highway and adjoining land having regard to drainage, gradient of access, the safety and amenities of users of the site, the amenities of the area and the health of

any trees or vegetation in accordance with policies CS NPPF, CS1, CS5 and CS7 of the Local Plan Core Strategy (adopted September 2012), Policies DM01, DM04 and DM17 of the Development Management Policies DPD (adopted September 2012), and Policies 7.4, 7.5, 7.6 and 7.21 of the London Plan 2011.

4 a) No development (Excluding demolition or site preparation works) shall take place until details of the materials to be used for the external surfaces of the building(s) and hard surfaced areas hereby approved have been submitted to and approved in writing by the Local Planning Authority.

b) The development shall thereafter be implemented in accordance with the materials as approved under this condition.

Reason: To safeguard the character and visual amenities of the site and wider area and to ensure that the building is constructed in accordance with Policies CS NPPF and CS1 of the Local Plan Core Strategy (adopted September 2012), Policy DM01 of the Development Management Policies DPD (adopted September 2012) and Policies 1.1, 7.4, 7.5 and 7.6 of the London Plan 2011.

5 The development shall be implemented in accordance with Ecological Survey report, and the proposed ecological mitigation and enhancements outlined shall be provided prior to occupation of the building. These measures shall be permanently maintained in accordance with these details thereafter.

Reason: To ensure that the proposed ecological enhancements are provided.

6 No site works or works on this development including demolition or construction work shall commence until a Demolition, Construction and Traffic Management Plan has been submitted to and approved in writing by the Local Planning Authority. All works must be carried out in full accordance with the approved details unless previously agreed in writing by the Local Planning Authority.

Reason: In the interests of highway safety in accordance with London Borough of Barnet's Local Plan Policy CS9 of Core Strategy (Adopted) September 2012 and Policy DM17 of Development Management Policies (Adopted) September 2012.

7 The development shall be implemented in full accordance with the details of refuse storage and collection shown on the approved plans and retained as such thereafter.

Reason: To ensure a satisfactory appearance for the development and satisfactory accessibility; and to protect the amenities of the area in accordance with policies DM01 of the Adopted Barnet Development Management Policies DPD (2012) and CS14 of the Adopted Barnet Core Strategy DPD (2012).

8 a) None of the approved development shall be occupied until details of all extraction and ventilation equipment to be installed as part of the development have been submitted to and approved in writing by the Local Planning Authority. The report shall include all calculations and baseline data, and be set out so that the Local Planning Authority can fully audit the report and critically analyse the content and recommendations.

b) The development shall be implemented in accordance with details approved under this condition before first occupation or the use is commenced and retained as such thereafter.

Reason: To ensure a satisfactory appearance for the development and satisfactory accessibility; and to protect the amenities of the area in accordance with Policy DM04 of the Development Management Policies DPD (adopted September 2012) and Policy CS13 of the Local Plan Core Strategy (adopted September 2012).

9 a) No site works (including any temporary enabling works, site clearance and demolition) or development shall take place until details of temporary tree protection have been submitted to and approved in writing by the Local Planning Authority.

b) No site works (including any temporary enabling works, site clearance and demolition) or development shall take place until the scheme of temporary tree protection as approved under this condition has been erected around existing trees on site. This protection shall remain in position until after the development works are completed and no material or soil shall be stored within these fenced areas at any time.

Reason: To safeguard the health of existing tree(s) which represent an important amenity feature in accordance with Policy DM01 of the Development Management Policies DPD (adopted September 2012), Policies CS5 and CS7 of the Local Plan Core Strategy (adopted September 2012) and Policy 7.21 of the London Plan 2011.

10 a) No development other than demolition work shall take place unless and until a Drainage Strategy detailing all drainage works to be carried out in respect of the development hereby approved and all Sustainable Urban Drainage System features to be included in the scheme has been submitted to and approved in writing by the Local Planning Authority.

b) The development hereby approved shall not be first occupied or brought into use until the drainage works and Sustainable Urban Drainage System features approved under this condition have been implemented in their entirety.

Reason: To ensure that the development provides appropriate drainage infrastructure and to comply with Policy CS13 of the Local Plan Core Strategy (adopted September 2012), the Sustainable Design and Construction SPD (adopted April 2013) and Policies 5.13 and 5.14 of the London Plan 2011.

11 a) Before the development hereby permitted is first occupied or brought into use, a School Travel Plan incorporating measures to reduce trips to the school by the private car and encourage non-car modes such as walking, cycling and public transport shall be submitted to and approved by the Local Planning Authority. This should include reference to the changes made to the school building/s and the impact this will have on travel and access, the contact details of the School Travel Plan Champion and appropriate actions to ensure that the STP will meet at least Bronze level in the Transport for London STARS (Sustainable Travel Active Responsible Safe) accreditation scheme for the following 3 years.

The School Travel Plan shall include SMART targets and a clear action plan for implementing the measures. The School Travel Plan shall be monitored, reviewed

and resubmitted in writing annually, for approval by the local planning authority, in accordance with the targets set out in the Plan.

b) The measures set out in the Travel Plan approved under this condition shall be implemented and retained until such time as the site is no longer in use or occupied.

Reason: To encourage the use of sustainable forms of transport to the site in accordance with Policy CS9 of the Local Plan Core Strategy (adopted September 2012) and Policy DM17 of the Development Management Policies DPD (adopted September 2012).

12 a) The site shall not be brought into use or first occupied until details of the means of enclosure, including boundary treatments, have been submitted to and approved in writing by the Local Planning Authority.

b) The development shall be implemented in accordance with the details approved as part of this condition before first occupation or the use is commenced and retained as such thereafter.

Reason: To ensure that the proposed development does not prejudice the appearance of the locality and/or the amenities of occupiers of adjoining residential properties and to confine access to the permitted points in the interest of the flow of traffic and conditions of general safety on the adjoining highway in accordance with Policies DM01, DM03, DM17 of the Development Management Policies DPD (adopted September 2012), and Policies CS NPPF and CS1 of the Local Plan Core Strategy (adopted September 2012).

13 No construction work resulting from the planning permission shall be carried out on the premises at any time on Sundays, Bank or Public Holidays, before 8.00 am or after 1.00 pm on Saturdays, or before 8.00 am or after 6.00pm pm on other days.

Reason: To ensure that the proposed development does not prejudice the amenities of occupiers of adjoining residential properties in accordance with policy DM04 of the Development Management Policies DPD (adopted September 2012).

14 Provisions shall be made within the site to ensure that all vehicles associated with the construction of the development hereby approved are properly washed and cleaned to prevent the passage of mud and dirt onto the adjoining highway.

Reason: To ensure that the development does not cause danger and inconvenience to users of the adjoining pavement and highway.

15 None of the approved development shall be occupied until waiting restrictions to mitigate adverse impact on roads in the vicinity of the school have been implemented in accordance with details to be submitted to and approved in writing by the Local Planning Authority.

Reason: To safeguard highway and pedestrian safety.

16 None of the approved development shall be occupied until the proposed Zebra Crossing for the increase in number of pupils has been implemented in accordance

with details to be submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure that the development makes satisfactory provision for highway and pedestrian safety.

- 17 Before the development hereby permitted is occupied full details of the electric vehicle charging points to be installed in the development shall have been submitted to the Local Planning Authority and approved in writing. These details shall include provision for not less than 20% of the approved parking spaces to be provided with active and 20% passive electric vehicle charging facilities. The development shall be implemented in full accordance with the approved details prior to first occupation and thereafter be maintained as such.

Reason:

To ensure that the development makes adequate provision for electric vehicle charging points to encourage the use of electric vehicles in accordance with policy 6.13 of the London Plan.

- 18 Before the permitted development is occupied a full Delivery and Servicing Plan (DSP) shall be submitted to and agreed by the Local Planning Authority.

Reason: In the interest of highway safety in accordance with London Borough of Barnet's Local Plan Policy CS9 of Core Strategy (Adopted) September 2012 and Policy DM17 of Development Management Policies (Adopted) September 2012.

- 19 Before the development hereby permitted is occupied, shall be provided in accordance with London Plan cycle parking standards and that area shall not thereafter be used for any purpose other than for the parking of cycles associated with the development.

Reason: To ensure and promote easier access for disabled persons to the approved building in accordance with London Borough of Barnet's Local Plan Policy CS9 of Core Strategy (Adopted) September 2012 and Policy DM17 of Development Management Policies (Adopted) September 2012 which in turn refers to London Plan Parking Standards.

- 20 Before the development hereby permitted is occupied, parking spaces shall be provided in accordance with Drawing Ref. No MK CS ZZ G00 DFP A 061001 Rev. P1. Thereafter, the parking spaces shall be used only as agreed and not be used for any purpose other than the parking and turning of vehicles in connection with approved development.

Reason: To ensure that adequate and satisfactory provision is made for the parking of vehicles in the interests of pedestrian and highway safety and the free flow of traffic in accordance with London Borough of Barnet's Local Plan Policy CS9 of Core Strategy (Adopted) September 2012 and Policy DM17 of Development Management Policies (Adopted) September 2012.

- 21 Before the development is brought into use, details of any external lighting shall be submitted to and approved in writing by the Local Planning Authority. The development shall be implemented in accordance with these details.

Reason: To safeguard neighbouring amenity.

Informative(s):

- 1 In accordance with paragraphs 186 and 187 of the NPPF, the Local Planning Authority (LPA) takes a positive and proactive approach to development proposals, focused on solutions. The LPA has produced planning policies and written guidance to assist applicants when submitting applications. These are all available on the Council's website. A pre-application advice service is also offered and the Applicant engaged with this prior to the submissions of this application. The LPA has negotiated with the applicant/agent where necessary during the application process to ensure that the proposed development is in accordance with the Development Plan.
- 2 Applicants and agents are encouraged to sign up to the Considerate Contractors Scheme (www.ccscheme.org.uk) whereby general standards of work are raised and the condition and safety of the Borough's streets and pavements are improved.
- 3 The applicant is advised that due to the large number of schools and school children in the area, site deliveries during the construction period should not take place between 0800hrs - 0900hrs and 1500hrs - 1600hrs. Careful consideration must also be given to the optimum routes for construction traffic and the Traffic and Development section should be consulted in this respect.
- 4 The applicant is advised that if any modification is required or proposed to access from the public highway, such works would be subject to a detailed investigation by the Traffic and Development section. Heavy duty access may need to be provided to cater for a heavy duty use and may involve relocation of existing street furniture. The works would be undertaken by the Highway Authority at the applicant's expense. You may obtain an estimate for this and any associated work on the public highway, and further information, from the Traffic and Development Section - Development and Regulatory Services, Building 4 North London Business Park, Oakleigh Road South, London N11 1NP, by telephone on 020 8359 3018, or via crossovers@barnet.gov.uk.
- 5 Any and all works carried out in pursuance of this consent / notice will be subject to the duties, obligations and criminal offences contained in the Wildlife and Countryside Act 1981 (as amended). Failure to comply with the provisions of the Wildlife and Countryside Act 1981 (as amended) may result in a criminal prosecution.

Officer's Assessment

1. Site Description

The site is Monkfrith School, an existing single form entry primary school. The site is bounded by Brookside South to the west, Knoll Drive to the east, and Monkfrith Way to the south. To the north is Oakhill Theological College which contains woodland. Part of Oak Hill Park is a site of Borough interest for Nature Conservation.

The site is 1.2 hectares in area.

2. Site History

N/03728A - Covered Way to Classroom - Approved - 14/08/1974

N/03728C/00 - Single storey extension to provide 2 additional classrooms - Approved - 02/01/2001

N/03728D/08 - Removal of entrance lobby. New reception area and school office. - Approved - 25/03/2008

15/03870/FUL - Installation of new Porta cabins to provide classrooms for a period of 52 weeks. - Approved - 14/08/15

3. Proposal

The proposals are for the erection of four single storey classroom extensions and further alterations to the existing school building including the provision of 14 car parking spaces, new pedestrian entrance on Brookside South and associated landscaping to increase the capacity of the school from one to two forms of entry

The proposals involve the following individual elements:

4no. Single storey extensions to north and south wings

Extension to front entrance foyer

Infill of courtyard

Additional cycle storage to Monkfrith Way

New car park to Monkfrith Way

Relocation of play equipment

New external play space

New pedestrian access from Brookside South

The proposed classroom buildings will be approximately 4m high.

The proposed design features rendered elevation panels, extensive glazing, aluminium framed doors and windows, and curtain wall framing.

The proposals involve the creation of a level access from the reception class to the external play area, and timber frame canopy.

The proposed front foyer extension would be glazed.

4. Public Consultation

Consultation letters were sent to 114 neighbouring properties.

5 responses have been received, comprising 4 letters of objection, and 1 letters of comment.

The objections received can be summarised as follows:

-Requirements for class places are reaching a crisis because we haven't planned for it. We continue to build new homes but don't 'build in' the required infrastructure - which is why we now have a knee jerk approach to finding places.

-Doubling the numbers at Monkfrith is a short term solution and not a sustainable one.

-Doubling numbers will increase traffic levels (autumn to late spring car journeys higher than the summer term). This area is already congested in relation to school day journeys. Residents have problems with parents dropping off children blocking their drives! If possible I avoid travelling just before 8.45 -9.15 a.m. and from 3.30-4 p.m. because getting through Monkfrith Way is so slow.

-This plan reduces, almost by half, the already limited play space for children. Apart from the buildings the car parking area takes out a large area of play area. Lack of exercise is causing issues with our children's health & contributes to rising obesity. Providing less area for play for double the numbers of children to run around is counter productive & all because the increase in spaces (known for several years) has been ignored. Do we value children's health or are we just looking for a quick fix?

-Ecological surveys for bats & GCNs still to be carried out. Have recorded bats in Oakhill Park adjacent to the school.

-No attention to the increase in impervious surfaces and the slope at the corner with Brookfield South. Already pond like and suggestions for decking to increase play space there is ridiculous - it will be lost space as will be under water with potential flooding onto Brookfield South -unless drainage addressed (expensive). Requires calculation of changes to substrate and how that will impact on the run-off coefficients. The area needs a new school - not an expansion here.

-Traffic congestion and pollution at the start and end of the school day, to a road which is already used by motorist as cut through. There are problems with parents parking across residents drives and not to mention an increase in litter around the area.

-We support the development plans for the school, however, we OBJECT to the potential increase in road traffic and congestion on a route that is already very dangerous on Parkside Gardens.

-The roads around Monkfrith are used as a short cut to Chase side / southgate from East Barnet. It is extremely dangerous around the school. We have been personally witness several car incidents where they ended across the pavements and in to trees.

-As part of planning, we would kindly request that Parkside Gardens road is slowed down by using few road humps or other suitable measures. The electronic displays already in place do not work, physical traffic slowing measure are needed. I know that there is a national campaign to have 20mph speed limits around all primary school... this is sensible and we should take the opportunity to implement this.

The representations received can be summarised as follows:

- As it stands the traffic has increased over the years. This seems to have developed as a cut through road, drivers often speeding. The road is not wide enough for two cars to pass when cars are parked. Drivers often pass at ridiculous speeds and can be very impatient.
- Drivers can be inconsiderate, sometimes parking over the angled parts of the drop curves, making it hazardous when reversing out as views are obscured and leaving not much room for our cars to pass. Although this may only be for 15-20 minutes, it is inconvenient and dangerous when you need to leave at these times.
- It is a concern for pedestrians as it is dangerous to cross.

Sport England – No comments received.

Highways – Comments addressed in main report

Environmental Health - Comments will be reported to the meeting

Children's Service – Comments will be reported to the meeting

5. Planning Considerations

5.1 Policy Context

National Planning Policy Framework and National Planning Practice Guidance

The determination of planning applications is made mindful of Central Government advice and the Local Plan for the area. It is recognised that Local Planning Authorities must determine applications in accordance with the statutory Development Plan, unless material considerations indicate otherwise, and that the planning system does not exist to protect the private interests of one person against another.

The National Planning Policy Framework (NPPF) was published on 27 March 2012. This is a key part of the Governments reforms to make the planning system less complex and more accessible, and to promote sustainable growth.

The NPPF states that 'good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people'. The NPPF retains a presumption in favour of sustainable development. This applies unless any adverse impacts of a development would 'significantly and demonstrably' outweigh the benefits.

The Mayor's London Plan 2015

The London Development Plan is the overall strategic plan for London, and it sets out a fully integrated economic, environmental, transport and social framework for the development of the capital to 2050. It forms part of the development plan for Greater London and is recognised in the NPPF as part of the development plan.

The London Plan provides a unified framework for strategies that are designed to ensure that all Londoners benefit from sustainable improvements to their quality of life.

Barnet's Local Plan (2012)

Barnet's Local Plan is made up of a suite of documents including the Core Strategy and Development Management Policies Development Plan Documents. Both were adopted in September 2012.

- Relevant Core Strategy Policies: CS NPPF, CS1, CS5.
- Relevant Development Management Policies: DM01, DM02, DM03, DM04, DM13, DM16, DM17

The Council's approach to development as set out in Policy DM01 is to minimise the impact on the local environment and to ensure that occupiers of new developments as well as neighbouring occupiers enjoy a high standard of amenity. Policy DM01 states that all development should represent high quality design and should be designed to allow for adequate daylight, sunlight, privacy and outlook for adjoining occupiers. Policy DM02 states that where appropriate, development will be expected to demonstrate compliance to minimum amenity standards and make a positive contribution to the Borough. The development standards set out in Policy DM02 are regarded as key for Barnet to deliver the highest standards of urban design.

Supplementary Planning Documents

Sustainable Design and Construction SPD (adopted April 2013)

- Provides detailed guidance that supplements policies in the adopted Local Plan, and sets out how sustainable development will be delivered in Barnet.

5.2 Main issues for consideration

The main issues for consideration in this case are:

- Principle of development
- Impact on trees of special amenity value
- Impact on ecology/biodiversity
- Impact on neighbouring amenity
- Impact on the character of the area
- Sustainability/Environmental Issues
- Highways Issues

5.3 Assessment of proposals

Principle of development

The proposals seek to extend an existing Local Authority School from single form to two from entry school.

Policy DM13 of the Adopted Barnet Development Management Policies 2012 states that:

New community or educational uses should be located where they are accessible by public transport, walking and cycling, preferably in town centres or local centres. New community or educational uses should ensure that there is no significant impact on the free flow of traffic and road safety. New community or educational uses will be expected to protect the amenity of residential properties.

Core Strategy policy CS10 is generally supportive of new educational development to meet demand.

The Government issued the 'Planning for schools development' policy statement in August 2011. In this statement the Government pledged its support for the development of schools stating that the creation and development of state funded schools is strongly in the national interest and that planning-decision makers can and should support the objective, in a manner consistent with their statutory obligations.

The statement outlines the Government's belief that the planning system should operate in a positive manner to schools. The statement requires Local Authorities to apply a presumption in favour of development of state-funded schools. This is further reinforced by para 72 of the NPPF which states that the Government attaches great importance to ensuring that a sufficient choice of school places is available to meet the needs of existing and new communities. It tasks Local Planning Authorities to take a proactive, positive and collaborative approach to meeting this requirement and to development that will widen the choice in education. Specifically it states that Local Authorities should:

Give great weight to the need to create, expand or alter schools; and work with schools promoters to identify and resolve key planning issues before applications are submitted.

The NPPF outlines that suitable infrastructure and services are needed in order to support new and existing economic development and to promote strong, stable and productive economies. In addition both the London Plan and the Local Plan recognise that the provision of education facilities are required in order to accommodate growth in a sustainable manner.

The London Plan advocates that the Mayor is committed to ensuring equal life chances for all Londoner's. Meeting the needs and expanding opportunities for all Londoner's - and where appropriate addressing the barriers to meeting the needs of particular groups and communities (Policy 3.1). Furthermore Policy 3.18 states that the Mayor strongly supports the establishment of new schools and steps to enable local people and communities to do this. The London Plan highlights the need to safeguard sports facilities and that new sites need to be identified to meet additional demands and changes in provision for education.

Policy 3.18 of the London Plan states:

Development proposals which enhance education and skills provision will be supported, including new build, expansion of existing facilities or changes of use to educational purposes. In particular, proposals for new schools should be given positive consideration and should only be refused where there are demonstrable negative local impacts which substantially outweigh the desirability of establishing a new school and which cannot be addressed through the appropriate use of planning conditions or obligations. Development proposals which maximise the extended or multiple use of educational facilities for community or recreational use should be encouraged.

A key guiding principle of the adopted London Plan and the Boroughs corporate plan is to sustain the boroughs communities. Policy CS10 of the Local plan states that 'the council will work with our partners to ensure that community facilities including schools and community meeting places and facilities for younger and older people are provided for Barnet's communities'. The proposals therefore would result in the provision of new education facilities to cater for the needs of current and future populations which is consistent with adopted policy which seeks to ensure that an adequate supply of buildings are available for educational facilities to meet the needs of the residents in the borough.

Whilst the proposals would result in the loss of some open space around the building, Sport England have not objected to the proposals.

The benefits of the additional school accommodation would need to be given substantial weight in the consideration of this planning application.

Impact on trees of special amenity value

A tree survey accompanies the proposals. The trees on the site are not subject to Tree Preservation Order. The proposals involve the removal of a mature tree to Monkfrith Way and a tree to Knoll Drive. The site currently is surrounded by a perimeter hedge and grassed areas. The trees are of some local amenity value, though are not subject to Tree Preservation Order, and as such could be removed as such without any requirement for consent from the Local Planning Authority.

Nevertheless, the wider loss of some of this screening is acknowledged and would have some impact in making the development appear somewhat more stark as viewed from Monkfrith Way and Knoll Drive (though the principal impact would be from the hardstanding for the car parking area). However tree protective fencing is proposed and the majority of trees are to be retained. Officers are satisfied that the retention of these trees is likely to be feasible.

The site to the north at Oak Lodge Theological College has Tree Preservation Orders on existing woodland. The proposed development would be 16-22m from the boundary with the neighbouring site. Subject to tree protective fencing and ground protection it is not considered that damage or harm to these trees is likely to result.

Impact on ecology/biodiversity

The site is located in close proximity to the site for Borough Nature Conservation at Oak Hill Park. Furthermore, there are large amounts of woodland at Oakhill Theological College to the north immediately adjoining the site.

The applicant has provided a Bat Survey and this included:

Dusk survey on 13th May 2015.

Dawn Survey on 12th June 2015.

Dawn Survey and block up features on 30th June 2015.

No bats were found within the buildings on the site itself.

Common pipistrelle bats were found to be using the woodland corridor/mature tree line along the north of the site and hedgerow along the eastern boundary for commuting and foraging purposes.

A condition is suggested to ensure that the suggested benefits in the ecological survey are provided. However, subject to this it is not considered that there would be likely harm to protected species under the Wildlife and Countryside Act 1981.

It is considered that the proposals would have an acceptable impact on local ecology and biodiversity.

Impact on neighbouring amenity

Visual Amenities

The site is enclosed within a plot of land and the nearest residential properties are located opposite on Knoll Drive and Brookside South. The proposed buildings are all single storey.

Given the distance of the proposed extensions from neighbouring buildings, it is considered unlikely that these would harm neighbouring visual amenity.

Residential Amenities

The proposals are for a significant increase to the size of the school, from 270 to 420 pupils. This has the potential to cause increased amounts of noise and disturbance, general activity, highway and pedestrian movements. The proposed buildings have been sited in locations that are generally well set away from neighbouring windows and it is considered that the increased number of residents would be unlikely to cause serious harm to neighbouring residential amenity.

The applicant has provided a planning statement which makes reference to the increased noise and disturbance from the proposals. This advises that playtimes and lunchtimes will be staggered to manage the numbers of children outside the school building at any one time. The site is surrounded by roads on three sides this creates a buffer between the main playground and the closest residential properties.

The proposals also involve a new pedestrian access from Brookside South, in addition to the existing pedestrian access to the property. This would help disperse the activity created and ensure that any impacts are not concentrated around one point of access. It is considered that whilst there might be some perceptible increase in terms of activity and noise from residences on Brookside South it is not considered that this would materially harm the residential amenities of neighbouring occupiers.

In terms of the use of the school, the school currently has a breakfast club, from 8.00 a.m. The school hall is hired from 8.00 a.m.-9.00 a.m. Various extra- curricular activities take place immediately following the school's afternoon teaching. These last until to 4.45 p.m. The school also has an After School Club which finishes at 5.45 p.m. on Monday to Friday.

The hall and dining room area are available for letting in the evenings and at weekends for local residents and community groups.

The proposals would represent something of an increase above that existing on the site. It should be noted that the proposals would make provision for 14no. new car parking spaces to serve the development.

Policy CS10 states that the Council will '*promote the role of schools as 'community hubs providing a wide range of educational, advice, leisure and support services to children, families and the wider community...and support the enhancement and inclusive design of community facilities ensuring their efficient use, and the provision of multi-purpose community hubs that can provide a range of services to the community at a single accessible location'*

In this way the proposals are encouraged by policy CS10.

Overall, it is considered that whilst the proposals would result in some increase in terms of noise and disturbance as perceived by neighbouring residents, it is not considered that the proposals would cause material harm, and in any event the benefits of the increased community use of the facility need to be considered.

Impact on the character of the area

The proposals involve single storey extensions to the building, albeit relatively tall single storey buildings given their height. The existing building is single storey in scale and the proposals would respect this. Furthermore, given levels differences across the site, all extensions won't be fully visible from one location.

It should be noted that a hedge runs along the perimeter boundary of the site and along with the trees on the site, this contributes positively to local green infrastructure and the appearance of the general locality. In order to provide staff parking, areas of proposed hardstanding have been proposed for 14 parking spaces accessible from Monkfrith Way. Given the hedging and trees on site, there would be some loss of greenery. This would have a minor detrimental impact on the character and appearance of the streetscene.

Sustainability/Environmental Issues

The proposals would involve the additional of 925 square metres of additional floorspace. Conditions are attached to ensure that the development would comply with policies 5.2, and 5.3 of the Mayor's London Plan.

Bin storage enclosures are proposed to Knoll Drive and this is considered an acceptable storage location. The development would make use of the existing bin collection point and this is considered acceptable.

Highways Issues

The public transport accessibility for the existing Monkfrith Primary School site was assessed as 2 and is considered as poor.

The existing Monkfrith Primary School is a one form entry school. The school currently accommodates 270 pupils (age range 4 to 11 years old) and 49 staff. The school has a provision for 30 Reception children starting each September.

The main pedestrian and cyclist entrance and exit to the school for pupils is located on Monkfrith Way. This entrance is supervised by the head teacher from 08:45 hours until start of lessons.

There is another access located on Knoll Drive which is used by key stage 1 (reception, year 1 and year 2) pupils exiting the school at the end of the school day.

The main teachers and visitors entrance is located on Knoll Drive consisting of 2 accesses, one ramped and the other a stepped access. This access leads to the school office and reception.

Parking

There is existing cycle storage for 20 cycles or scooters on the existing site for use by pupils located near the entrance located on Monkfrith Way. There is no cycle storage for use by staff. There are no shower facilities within the existing school for use by pupils or staff.

There is currently no on site car parking at the existing school. Staff and visitors currently park on the surrounding residential streets.

The proposal is for the expansion of the existing primary school to provide 2 form entry school. This would result increase the pupil intake of 420 pupils (150 additional pupils) and an additional 41 staff (11 teaching staff and 30 non-teaching/support staff). It is proposed that the school expansion will begin in 2015 with an additional 30 pupils and 7 staff (2 teaching staff and 5 non-teaching/support staff) increasing in size year on year until capacity is reached in 2022.

As part of the proposed expansion 14 new parking spaces are proposed on site which will be accessed from Monkfrith Way.

No disabled parking provision has been indicated within the proposed new car park due to the steps between the proposed car park and the main reception building within the school. It is suggested that any disabled parking requirements would need to be located adjacent to the existing access for staff and visitors to the school on Knoll Drive. This would provide direct access to the main reception via the existing pedestrian ramps. Disabled parking provision should be reviewed by the school once staff members are known to ensure the needs of the school are met and it would be recommended 1 disabled space is provided for each disabled member of staff.

In order to assess the parking demand on roads in the vicinity of the existing school, parking surveys were undertaken on Wednesday 26th November 2014. The parking survey indicates there are parking spaces available on the surrounding streets adjacent to the school.

48 cycle stands and shower facilities will be provided.

There is no on-site car parking available at the existing school. Staff and visitors currently park on the surrounding residential streets. 14 new parking spaces are proposed as part of the planning application to minimise the impact of the additional staff intake.

It is also proposed that outside normal school hours, e.g. after school clubs, these car parking spaces could be made available to be used for pick up and drop off of children by parents, reducing the existing parking stress on the adjacent residential streets. This would need to be managed by the school but would result in an improvement compared to the existing situation, as there is currently no car parking available at the school site.

Travel Plan

As part of the school Travel Plan measures are proposed to be implemented and managed by the school in order to reduce parking and traffic generated by parents dropping off and picking up children at start and end of school.

Pedestrian/cyclist infrastructure will be improved with the introduction of a new cycle storage facility, pedestrian ramps within the school site and a new pedestrian access provided at the site on Brookside South to further encourage sustainable travel.

It is anticipated that with the introduction of a staff and visitor car park combined with a comprehensive range of measures to promote sustainable transport, the impact on the local highway network due to the school expansion will be minimised.

Emergency Access

The applicant has confirmed that the delivery, service and emergency vehicles for the school park on Knoll Drive at present. Existing access arrangements at the school for delivery, service and emergency vehicles will remain unchanged.

Safety

No accident trend has been identified near to the existing school. There were no reported accidents involving children aged 11 years old or under in close proximity to the existing school.

Therefore, the additional traffic associated with the expansion of the school is unlikely to have a significant impact on the safety of the local highway network.

It is however considered necessary to provide additional waiting restrictions and a pedestrian crossing in order to address the above impacts.

The application is recommend for approval on highway grounds subject to the following conditions, improvements towards proposed pedestrian improvements and highway safety.

{b 5.4 Response to Public Consultation}

Requirements for class places are reaching a crisis because we haven't planned for it. We continue to build new homes but don't 'build in' the required infrastructure - which is why we now have a knee jerk approach to finding places. – *It is acknowledged that there is high demand for school place within Barnet. It is suggested that it would make more efficient use of an existing school site, rather than developing a new site.*

Doubling the numbers at Monkfrith is a short term solution and not a sustainable one. – *It is considered that the additional accommodation proposed would not materially harm neighbouring amenity and would make provision for highways improvements that are necessary.*

Doubling numbers will increase traffic levels (autumn to late spring car journeys higher than the summer term). This area is already congested in relation to school day journeys. Residents have problems with parents dropping off children blocking their drives! If possible I avoid travelling just before 8.45 -9.15 a.m. and from 3.30-4 p.m. because getting through Monkfrith Way is so slow. – *It is acknowledged that the existing surrounding roads do suffer from parking stress. The proposals would make provision of highways improvements as well as staff parking and it is considered that the proposals would not harmfully increase existing issues in this regard.*

This plan reduces, almost by half, the already limited play space for children. Apart from the buildings the car parking area takes out a large area of play area. Lack of exercise is causing issues with our children's health & contributes to rising obesity. Providing less area for play for double the numbers of children to run around is counter productive & all because the increase in spaces (known for several years) has been ignored. Do we value

children's health or are we just looking for a quick fix? - *The proposals are considered to make adequate provision for external play areas. Sport England have been consulted on the proposals and do not object. The proposals would involve an increase of 183sq m in hard informal play areas, and a reduction in 1245sq m soft informal play area, principally to the north-west of the site.*

Ecological surveys for bats & GCNs still to be carried out. Have recorded bats in Oakhill Park adjacent to the school. – *Bat surveys were carried out and consultation has taken place. Conditions are also suggested to ensure that adequate mitigation has been provided. It is considered that the site is unlikely to provide a habitat for Great Crested Newts however conditions are suggested to ensure that mitigation is provided. Furthermore, an initial survey of Great Crested Newts was undertaken by the applicant and found that the locations on the neighbouring site that are likely to be suitable for Great Crested Newts are a considerable distance from the site.*

No attention to the increase in impervious surfaces and the slop at the corner with Brookfield South. Already pond like and suggestions for decking to increase play space there is ridiculous - it will be lost space as will be under water with potential flooding onto Brookfield South -unless drainage addressed (expensive). Requires calculation of changes to substrate and how that will impact on the run-off coefficients. – The site is not located in a Flood Zone Area, and any impacts are likely to be localised. The application makes provision for drainage including two attenuation tanks. It is considered that the proposals make adequate provision for drainage.

- Increase in litter around the area. – *Litter outside the site is outside the applicants control and it is not considered that this be a planning justification for withholding permission.*

6. Equality and Diversity Issues

The proposal does not conflict with either Barnet Council's Equalities Policy or the commitments set in the Equality Scheme and supports the Council in meeting its statutory equality responsibilities.

7. Conclusion

Taking the above issues into consideration, whilst there would be some impact from the additional parking area proposed on the character and appearance of the streetscene, it is considered that any limited harm would be outweighed by the benefits of providing additional school accommodation within the borough.

The proposal is considered to accord with the requirements of the Development Plan and is therefore recommended for approval.



Reproduced by permission of Ordnance Survey on behalf of HMSO. © Crown copyright and database right 2013. All rights reserved. Ordnance Survey Licence number LA100017674.

AGENDA ITEM 14

Document is Restricted

This page is intentionally left blank